

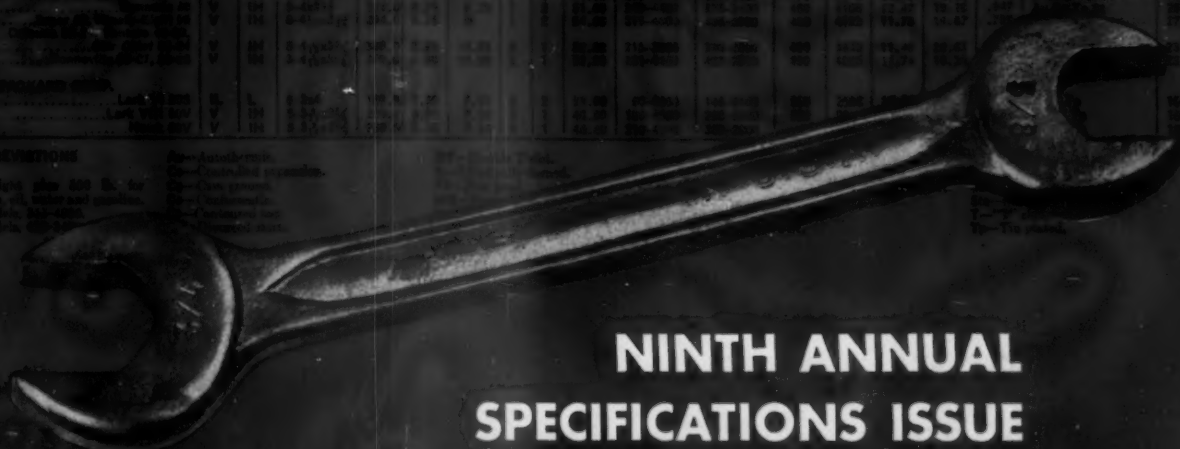
PUNCH-OUT-HANG UP FOR REFERENCE

MOTOR AGE

A CHILTON PUBLICATION

APRIL 1960

FIX FAX



**NINTH ANNUAL
SPECIFICATIONS ISSUE**

PRICE \$100



PERFECT CIRCLE PISTON RINGS ARE BUILT TO TAKE IT



HOTTER THAN A BLOWTORCH

The searing heat that's created inside engine cylinders causes inferior piston rings to lose their strength and resilience, and wear out far before their time. That's why Perfect Circle employs special metallurgical skills to produce ring materials that have the high heat stability needed for long life.

Extra-thick, solid chrome plating adds greater protection against scuffing. And, special alloys and heat treating deliver extra-high heat resistance for critical applications.

Whatever the job, Perfect Circle rings are built to take it. Insist on Perfect Circles—first choice of leading engine manufacturers and mechanics everywhere.



PERFECT CIRCLE

PISTON RINGS • PRECISION CASTINGS • POWER SERVICE PRODUCTS • SPEEDOSTAT
HAGERSTOWN, INDIANA • DON MILLS, ONTARIO, CANADA

ANOTHER BLUE STREAK
SECRET SERVICE TIP FOR YOU.

SHERLOCK McKANICK and MIKE

"The case of the
SWITCH THAT ALMOST BURNED"



MECHANICS:
THERE'S NO EXCUSE
FOR A BURNT-OUT
NEUTRAL SAFETY
SWITCH! LET
BLUE STREAK
HELP YOU!

WRITE NOW FOR **BLUE STREAK**
SERVICE BULLETIN #90, "HINT FOR
BETTER ELECTRIC SERVICE"
STANDARD MOTORS PRODUCTS, INC.,
37-18 NORTHERN BLVD.,
LONG ISLAND CITY, NEW YORK.



WORLD'S FOREMOST
HEAVY-DUTY
IGNITION LINE

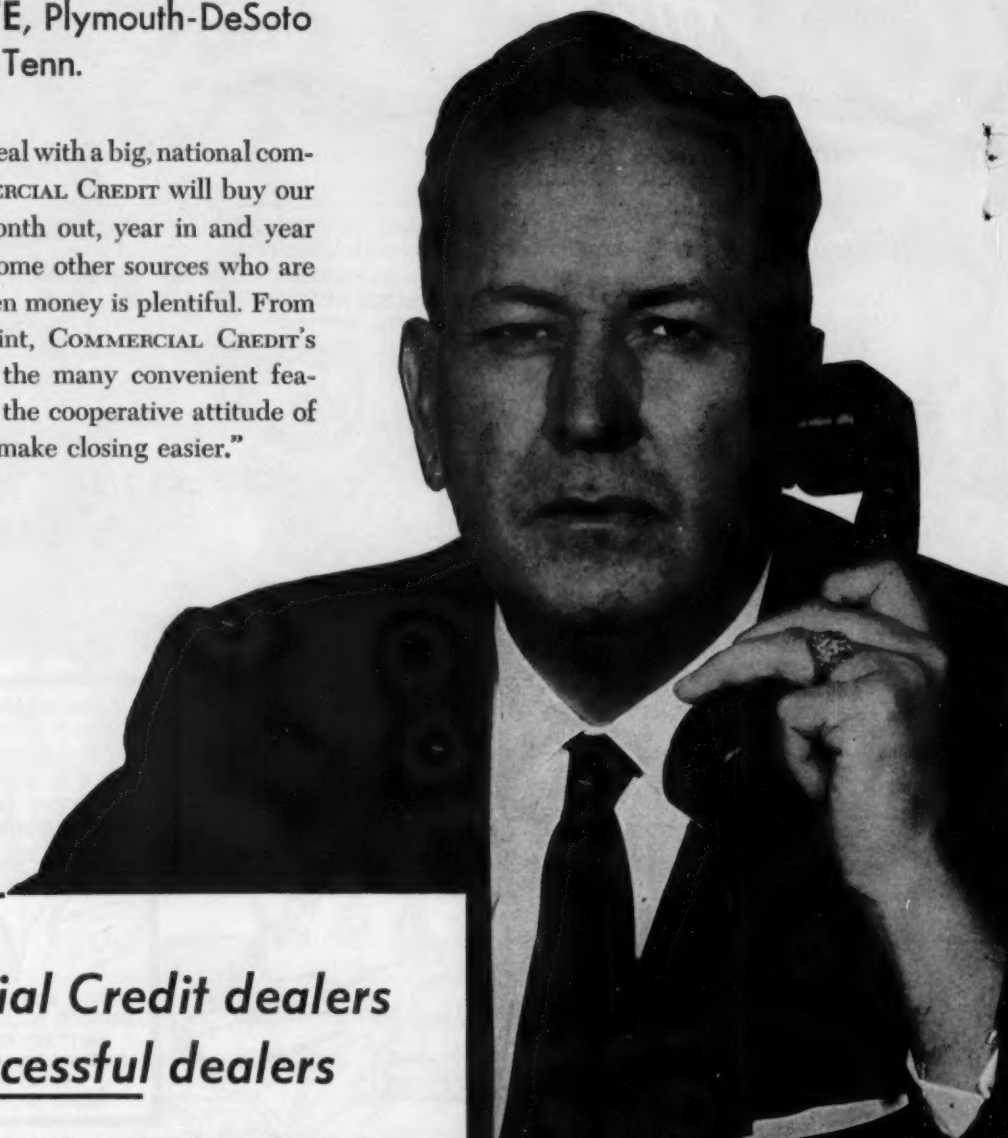


REGULATORS • SWITCHES • COILS • CONDENSERS • CONTACT POINTS • WIRE AND CABLE

"...will buy our paper month in and month out..."

says **J. PRIVETTE**, Plymouth-DeSoto dealer, Millington, Tenn.

"We've found it pays to deal with a big, national company. We know COMMERCIAL CREDIT will buy our paper month in and month out, year in and year out. They are not like some other sources who are just interested in us when money is plentiful. From the customer's standpoint, COMMERCIAL CREDIT's nationwide reputation, the many convenient features of their Plan, and the cooperative attitude of their local people help make closing easier."



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News from the Chemical Workbench

SERVICE INFORMATION FOR THE PROFESSIONAL FROM PERMATEx

New PERMATEx Degreaser • Cleaner Catching On Fast



Since its recent introduction, this new Permatex Product has started to gain wide acceptance. That's because it is specially compounded to do a fast and efficient job for you in degreasing and cleaning auto engines, outboard motors, tools—all metal surfaces soiled with grease, oil and grime.

Available Two Ways

Concentrate (mix with kerosene or light fuel oil) or "Ready-to-Use" (use right out of the can) in Aerosol or special applicator spout can.

Easy To Use

Apply! Let stand! Wash off! The perfect complement to Permatex Cold Parts Cleaner—order some today.



PERMATEx Anti-Seize Compound (Graphite Grease) Best For Aluminum Engines

To prevent seizure of the bolts when disassembling an all-aluminum engine, Permatex Anti-Seize Compound (Graphite Grease) is a must. One major car manufacturer is now carrying this advice in dealer service manuals. This Permatex Product should be used whenever you reassemble an aluminum engine—it'll save you time and trouble next time you work on it. It comes in one pound cans and sells to you for \$1.16 a can (less than dozen cans).



Tips on Repairs with PERMATEx Form-A-Gaskets*

1. Warped Cylinder Heads. Use Form-A-Gasket No. 1 on head gasket to build block flush.
2. Battery Corrosion. Clean the connections and coat them with Aviation Form-A-Gasket No. 3 to eliminate corrosion of battery terminals.
3. Emergency Plug for Gas Tanks. Use a sheet metal screw larger than the hole. Push screw through a piece of gasket material. Apply Form-A-Gasket No. 1 to gasket and insert. Screw into hole.
4. Trim screws cannot work loose when dipped in No. 1 before assembling.
5. Repair Cracked Batteries. Simply wash off the acid with water and apply Form-A-Gasket No. 1 for strong, permanent repair job.
6. Stop oil filter leaks. A coating of Form-A-Gasket No. 2 on the top level gasket before seating gives you a sure leak tight seal.

The Right Sealant For Every Job
The smart mechanic, the professional, is the man who has just the right sealant for the job... handy at all times. Famous Permatex Form-A-Gaskets Nos. 1, 2, and 3 and Super "300" Form-A-Gasket make life easier in dealing with any sealant problem around your shop. You'll want to stock all the Form-A-Gaskets. And be sure and write us for special sealants chart.



Share your
chemical
service tips with
other dealers...and
get \$5 for it when
it's published.
PERMATEx PETE



Car Care: Shoeshine For Autos

with PERMATEx Polishes and Cleaners

Just like a shoeshine gives the average person that special, well-groomed feeling, so too, a clean, sparkling car gives the owner a special, proud-of-his-car feeling.

Car manufacturers recommend a regular wash and wax procedure in their owner manuals—it keeps the resale value of the car higher. That's why you should tie-in an outside Car Care special whenever you have a lube or tune-up special. Usually with a lube or tune-up it means the car is around your station a good part of the day—so you have the opportunity to wash, clean and wax the car.

Simplify The Job

Permatex makes a complete line of Polishes and Cleaners that are easy to use and meet every need: Perma-Lustre®—cleaner-polish for enamel and lacquer body finishes; Chromium Polish—cleans and protects chrome parts; Fabric Cleaner—for all stains, spots and soiled areas. Removes tar, lipstick, gum, grease; Blue-Blazes®—removes dirt, grime from finishes. Leaves long-lasting silicone wax coating; Car Wash—powerful detergent action. Won't streak

or hurt finish; Windshield Cleaner Concentrate—for automotive windshield washer reservoirs and spray bottles; Acrylic Cleaner and Polish—use on all acrylic finishes. Tar Remover—removes all tars, gums; safe for all acrylics.

Car Care Profitable Business

You can make money with a planned Permatex Car Care program because *anyone*, even your newest employee, can earn his way by "shoe-shining" your customers' cars. And don't overlook the fact that YOU... the professional—the man responsible for your customers' safety and driving comfort—can show through this sales program that you recommend keeping the car's appearance at its peak—to match the care you give the engine.

Try It And See For Yourself

Order the Permatex Polishes and Cleaners you need. Check first hand how well this idea works. And don't overlook the extra profit you make when you sell these quality Permatex Products to the car owner for his own use. You can't miss!

PERMATEx COMPANY, INC.

300 Broadway, Huntington Station, L. I., N. Y.
Factories: Brooklyn 35, N. Y. Kansas City 15, Kan.

don't divide your income with
lost time



protect your service profits with a

Lathem PAYROLL and JOB TIME RECORDER

Consider the ways that time on the job can be wasted . . . can run up even higher your cost of doing business. For instance . . . too much delay between jobs . . . too much time on each job . . . excessive tardiness.

To correct these abuses, you need to know where they occur. And that's where a Lathem Time Recorder can do a job for you. It gives you an accurate record of the time each job is started and completed . . . when each employee arrives and leaves work. Every loss of time stands out . . . ready for you to eliminate for a better profit picture.

Mail the coupon today for complete information, prices and sample payroll and job cards.

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66 Third Street, N. W., Atlanta, Georgia

Gentlemen: Without obligation, please send me complete information, including prices, about the Lathem Payroll and Job Time Recorder.

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IN THIS ISSUE

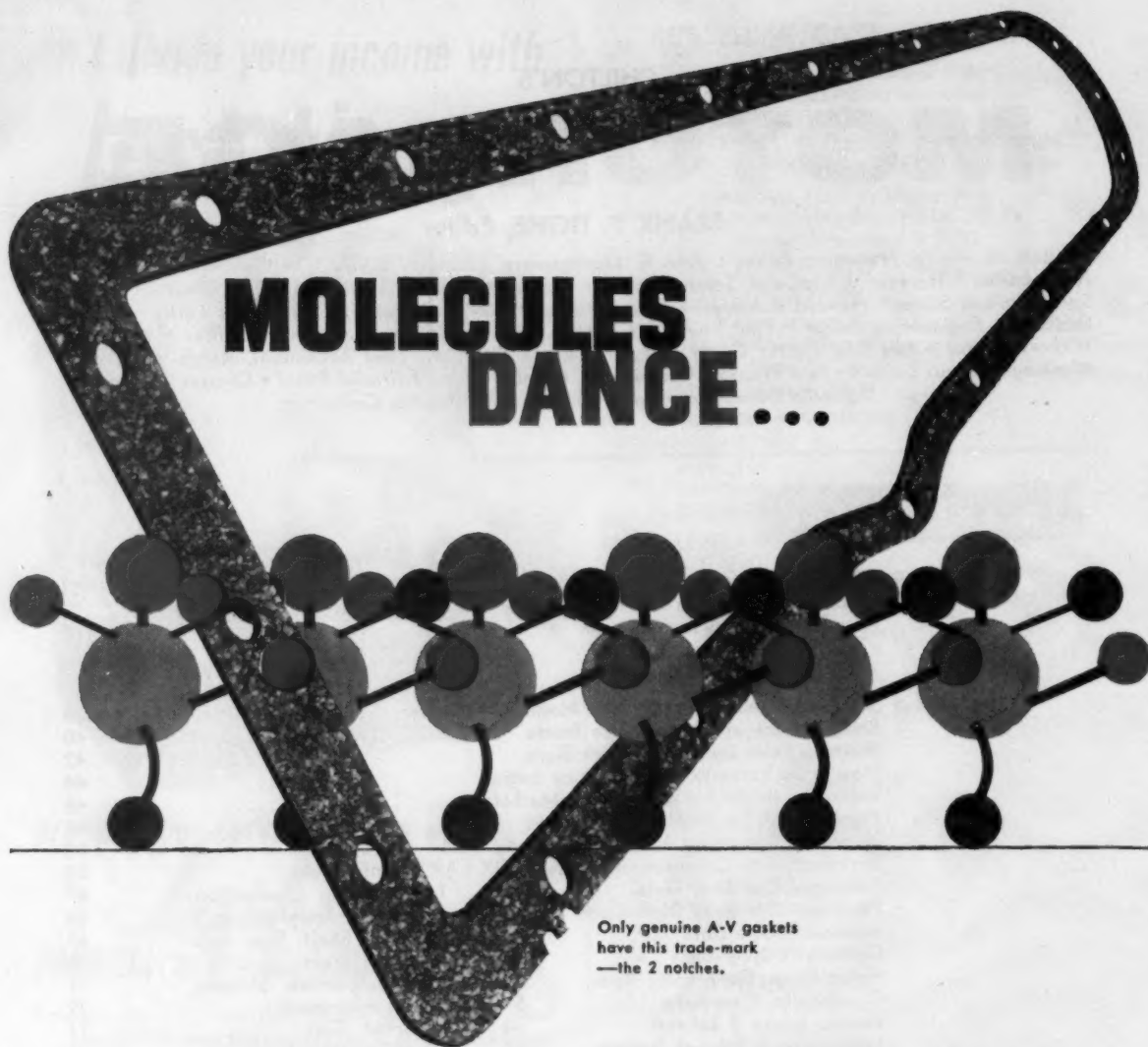
NEWS NEWSCOOP	33
Circle of Safety Aids Safety Checks	37
New Products News—begins on	118

ARTICLES		
Service Techniques on the New Holley Carburetor		38
Brake Service on Medium-Size Trucks		40
Promote Sales and Service with Signs		42
How to Do Valve Work on a Simca Engine		44
Report on the Motor Boat Engine Market		46
Planning Tips for Employee Vacations		48
Hydraulic Equipment Aids Unitized Body Work		50
Introduction to Consecutive Tables of FIX FAX Specifications		53
Passenger Car Body Data	54	Transmissions, Conventional
Passenger Car Body Dimensions	55	Automatic Transmissions
General Engine Data	56	Propeller Shaft, Rear Axle
Optional Engine Data	57	Tires and Brakes
Piston Rings, Connecting Rods	58	Front Suspension, Steering
Crankshafts, Camshafts	59	Wheel Alignment
Valves, Intake & Exhaust	60	Imported Cars
Lubrication & Exhaust Systems	61	Small Gasoline Engines
Fuel, Battery, Electrical	62	Outboard Motors
Fan, Battery, Electrical	63	Tune-up Data, '55-'60
Starting & Ignition	64	Small Engine Tune-up
Ignition Timing, Spark Plugs	65	Service, Imports
Lamp Bulbs, Fuses	66	Light Trucks

DEPARTMENTS		
Letters to the Editors		31
Shop Kinks for Cars and Trucks		52
Readers' Clearing House (Troubleshooting Problems)		96
Calendar of Coming Events		162
Body Shop Tips		167
Advertisers' Index		173
Last Laff		174



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Only genuine A-V gaskets
have this trade-mark
—the 2 notches.

TO MAKE A-V CORK GASKETS BETTER

It's crazy, man! But important! We mean the "dance" cork composition molecules do when they are baked electronically—to make the cork fit to become Armstrong-Victor cork gaskets. High-frequency baking produces a denser, stronger cork without robbing it of resiliency. The cork in A-V gaskets is never over- or under-done, which is often true of steam-cured cork.

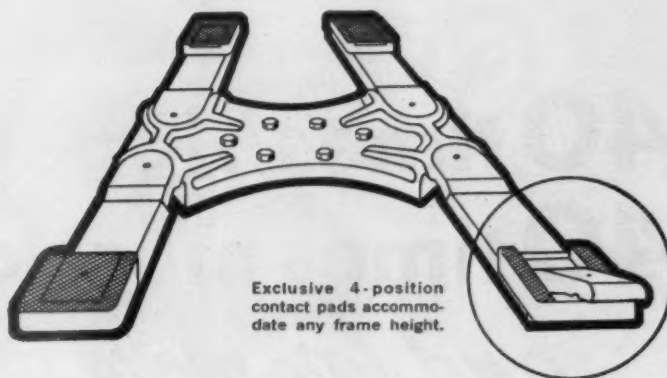
Users note these differences: A-V electronically baked cork gaskets are stronger, more flexible, more resistant to breakage. Cut and punched accurately, cleanly, they fit perfectly. So, they seal tighter, last longer. No wonder more A-V cork gaskets are used than any other brand! Your Victor Jobber carries complete stocks—in sets and individual parts.

Victor Mfg. & Gasket Co., P.O. Box 1333, Chicago 90, Ill. Canadian Plant: St. Thomas, Ont.

Armstrong-VICTOR
CORK GASKETS

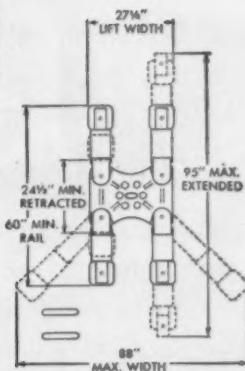
The 100% Coverage Line—for Cars, Trucks, Tractors, Stationary Engines

**LIFTS ANY CAR
SAFELY...
WITHOUT
ADAPTERS!**



Wayne UNIVERSAL Frame Lift

You can adjust the Wayne UNIVERSAL to make it longer, shorter, narrower or wider than any other lift available. Height above floor—4½ in.—is lowest of any lift.



From the largest domestic model to the smallest 3-wheel import, no car has a frame too wide, a track too narrow, or an underbody too low to be handled by the Wayne UNIVERSAL. Spot the car on the wheel stops... position the swivel arms with a handy hook (no crawling under)... raise the lift. No adapters ever needed.

Safe—Load is supported by a large underlip on each arm, eliminating stress on the swivel bolts. New 6-position safety leg releases by foot treadle with operator standing clear. A Wayne UNIVERSAL—rated to 8000 lb. capacity—has been successfully tested at loads far beyond specifications of commercial Standard C.S. 142-58.

Saves Service Time—With the Wayne UNIVERSAL Lift you get a clear working area, ample headroom, and an unobstructed view. All parts of the car underbody are easily accessible, saving time in lubrication, tire servicing, muffler replacement, or other repairs. New minimum-friction packing on main cylinder assures lift follow-through to floor after tires touch down.

Write for new Bulletin W-114 giving complete data on latest model Wayne UNIVERSAL Lift.



**WORLD'S LARGEST
MANUFACTURER OF
GASOLINE PUMPS AND
SERVICE STATION EQUIPMENT**

THE WAYNE PUMP COMPANY • Division of Symington Wayne Corporation • Salisbury, Md. • WAYNE PUMP CANADA, LTD. • Toronto, Ontario

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40 times bigger picture **40 times more detail...** *and far more profitable*

THIS IS THE BASIC REASON WHY AN ALLEN SCOPE IS YOUR BEST BUY!

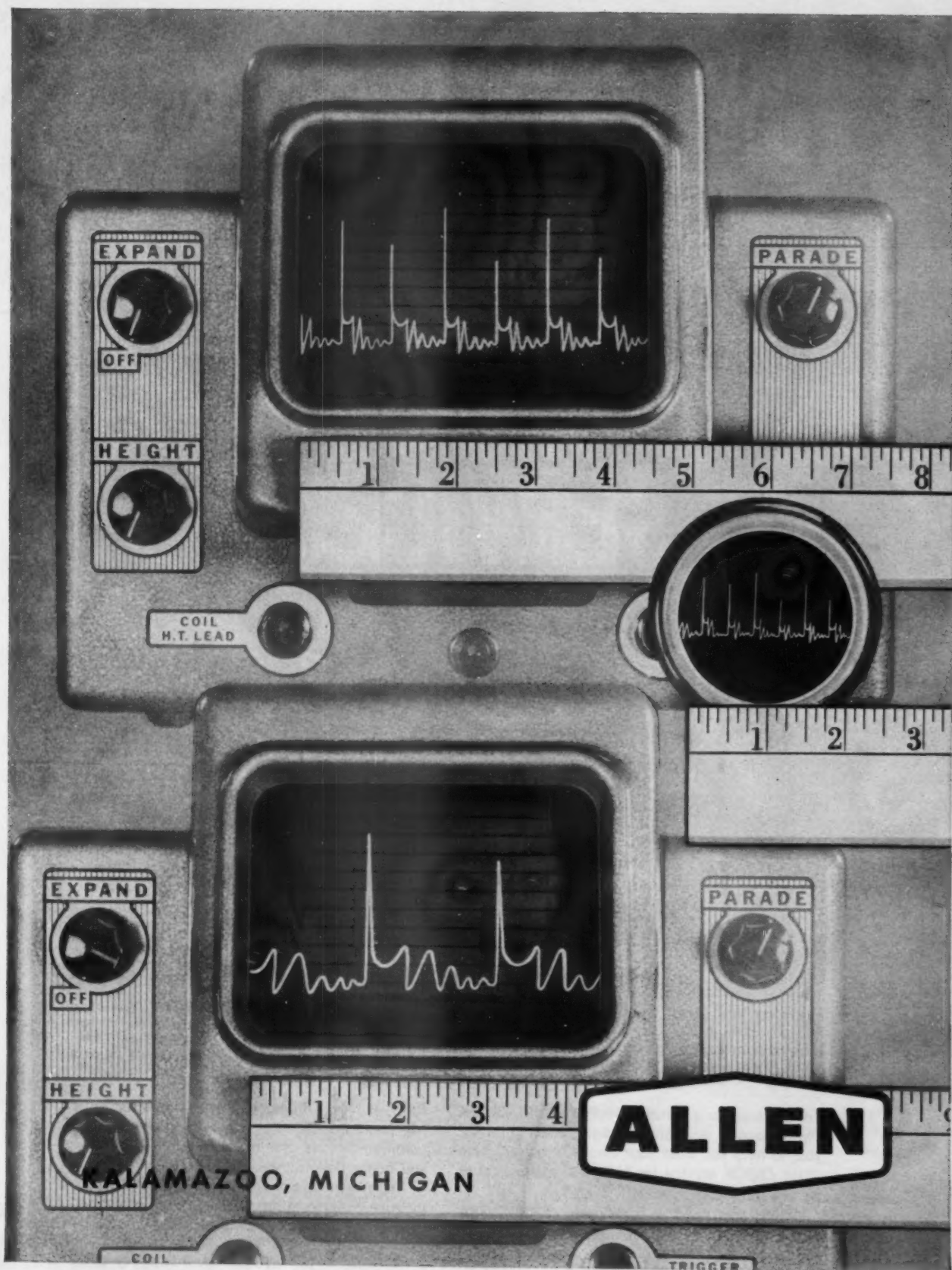
With all the midget scopes on the market, you might wonder why they can cost only a fifth as much as an Allen Scope. The fact is, they don't cost less. *They cost more.*

Here's why.

You can compare all 4, 6, or 8 cylinders on an Allen Scope screen. Then, you expand the pattern to look at the details on each individual cylinder. This single cylinder pattern is 40 times larger, 40 times more detailed than the non-expanding pattern on a 2" midget scope. The Allen Scope tells you, in detail, what is wrong with an ignition system. The midget scope only tells you something is wrong. You have to guess what. And guessing is no way to make a profit in tune-up.

So, when you think about a scope, consider what you want a scope to do. If a profitable tune-up business is your goal, visit your Allen Jobber. See why you sell more, more profitably with an Allen Scope.

ALLEN ELECTRIC AND EQUIPMENT COMPANY



KALAMAZOO, MICHIGAN

ALLEN

NOW EVEN FASTER! USE MOPAR'S *Dubl-Quick* SERVICE...



Here's how DUBL-QUICK Parts

1 **THE PART YOU WANT, WHEN YOU WANT IT!** And, remember, they're genuine MoPar parts, precision-engineered by Chrysler Corporation for Dodge Trucks—designed to install fast, fit right, last longer, to keep your Dodge Truck rolling.

For one truck or a hundred, MoPar's DUBL-QUICK Parts Service is geared to fight down time for you, to help you keep your trucks in action. Use DUBL-QUICK service for ALL your parts requirements, routine or emergency.

When you need a truck part or accessory, phone your Dodge Truck Dealer or MoPar Whole-

2 **NATION-WIDE PARTS AVAILABILITY.** Your phone call to your Dodge Truck Dealer or MoPar Parts Wholesaler connects you to a parts distribution and availability network that's second to none, plus five strategically spotted parts plants!

saler. If it isn't in stock, a new, simplified ordering procedure and a coast-to-coast teletype communications system let him get the item to you faster now than ever.

When you own a Dodge, you've got the best in truck transportation. Protect your investment (and your profit) with genuine MoPar parts.

DUBL-QUICK service plus MOPAR quality will keep your

TO KEEP YOUR DODGE TRUCKS IN ACTION!



Service works for you:

3

SPECIAL "TROUBLE-SHOOTER" FIELD FORCE. MoPar truck experts know what it means to have a truck sidelined for lack of repair parts. When you need a scarce item or there's an emergency, these experts move in to help you—fast!

4

"HOT WIRE" ORDERING SERVICE. When parts are not in stock locally, your Dodge Truck Dealer or MoPar Wholesaler will hit the "hot wire." Shipments are made "round the clock" from MoPar's parts plants—including Saturday and Sunday!

**TO GET PARTS—DUBL-QUICK—
PHONE YOUR DODGE TRUCK
DEALER OR MOPAR WHOLESALER**



MoPar Division
Chrysler Motors Corporation
Detroit 31, Michigan

DODGE TRUCKS in action!

PARTS AND ACCESSORIES

New Du Pont **Telar** anti-freeze and

PATENTED FORMULA:

- Prevents freezing in winter!
- Maintains perfect cooling in summer!
- Rust-proofs all cooling system metals all year round!
- Contains Color Check which changes the solution's color if cooling system trouble contaminates it!

Climaxing years of Du Pont research and 5 years of car tests, Du Pont presents "Telar"—the first and only anti-freeze and anti-rust coolant you can safely leave in year after year as long as the cooling system is functioning properly. You never need to drain "Telar"—you merely add to it.

NEVER NEEDS DRAINING

Why doesn't "Telar" need draining? Although some motorists already leave their anti-freeze in, all authorities have advised against it. The corrosion inhibitors in most anti-freezes gradually break down through high mileage, air suction,



New contribution of Du Pont chemistry!

exhaust leaks, hot spots, etc. Then rust clogging starts unless the anti-freeze is drained. The U. S. Bureau of Standards reports that in up to 50% of a series of tests to determine inhibitor life, inhibitors were exhausted at 10,000 miles.

But "Telar" is a patented new formula, exclusive with Du Pont, that stands up and stays fresh where others break down.

"Telar" also prevents galvanic action

(electrical currents), chief cause of rust and corrosion. It completely protects every cooling system metal, even aluminum.

"Telar" doesn't just coat the metal with a film that can wear off. It reacts with the metal surfaces, changing their chemistry to form a *chemical armor* against rust and corrosion. It doesn't harm hoses.

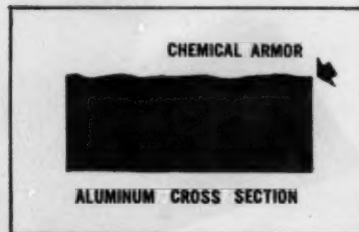


Du Pont "Telar" has been proved. Auto expert Tom McCahill reports: "I've been testing 'Telar' in my own cars for 3 years, from 120° above to 20° below zero. 'Telar' never broke down, so I never had to drain—and it keeps the entire cooling system clean as a tiger's tooth. This 'Telar' is for keeps!"

And "Telar" maintains perfect cooling summer and winter by preventing rust.

"TELAR" IS FULL STRENGTH

"Telar" is a full-strength coolant, not diluted, that works in *any* water. You install "Telar" like ordinary anti-freeze—just add it to water to give the degree of anti-freeze protection desired.



"Telar" protects against rust by reacting with metal surfaces to form a chemical armor, as photomicrograph shows!

For the past 5 years, following years of laboratory tests, "Telar" anti-freeze and anti-rust coolant has been tested in actual use in year-round driving in Du Pont's test-car fleet. A test run was even made to the Arctic Circle! Year after year, solutions of "Telar" have stayed fresh and new-car clean! Positive proof that you can leave "Telar" in your customers' cars with complete confidence.

CONTAINS COLOR CHECK

To guarantee peace of mind, Du Pont has added Color Check to "Telar". If any-



Patented Color Check watches cooling system for you! This demonstration shows how Color Check works. Plastic bag, left, contains solution of "Telar" and a small capsule of the same kind of acid that could be formed in a faulty cooling system. When capsule is broken, releasing acid, "Telar" immediately turns from red to yellow, as at right!

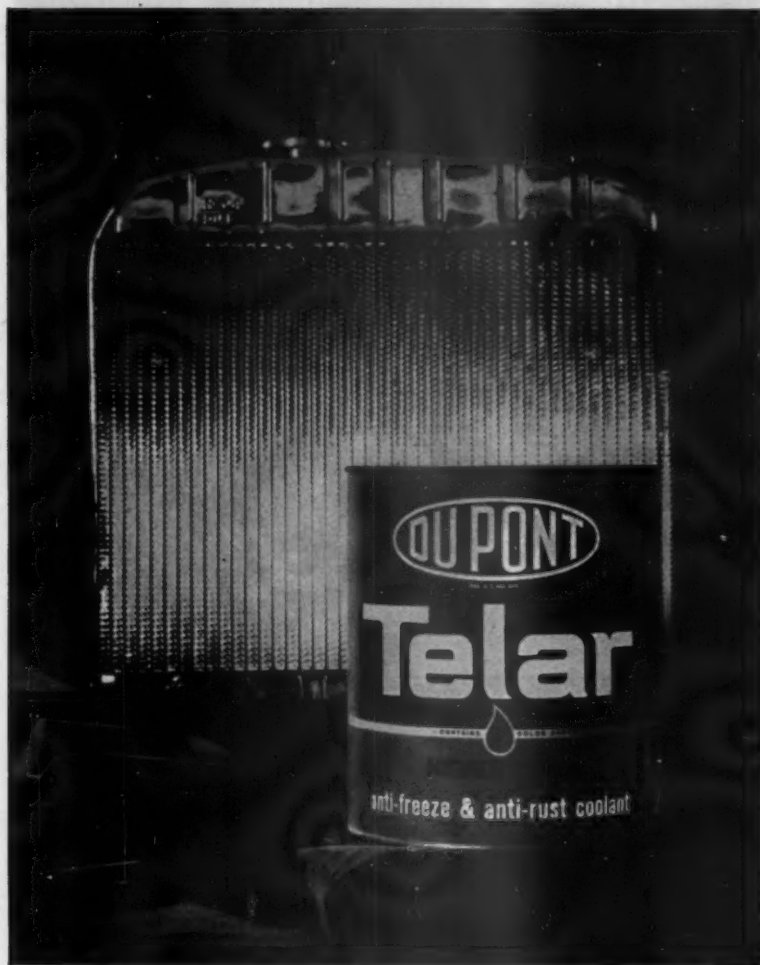
Commands installed price of \$5⁰⁰ per gallon!
Pays premium profit on every sale!
Sold exclusively through servicing dealers!



BETTER THINGS FOR BETTER LIVING
... THROUGH CHEMISTRY

Chilton's MOTOR AGE • April 1960

is the first NEVER DRAIN anti-rust coolant!



Introducing new "Telar"—for the protection car owners want—the profits you want!

thing should go wrong with the cooling system to contaminate the coolant and cause rust, like a leaky water pump or gasket, the solution's color changes from red to yellow to warn you *before* further trouble develops. As long as the "Telar"

solution stays red, you *know* it's giving complete protection against rust and corrosion. If the solution turns yellow, it of course should be drained and discarded and the system repaired before costly damage develops.

Telar

TRADEMARK

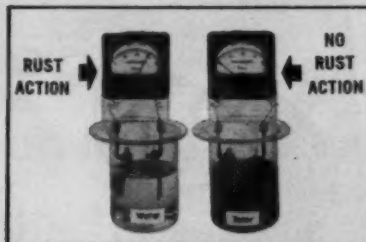
... the ultimate anti-freeze and anti-rust coolant

Chilton's MOTOR AGE • April 1960

MORE SALES, MORE PROFITS

You make \$1.65 on every gallon you sell, and every "Telar" sale builds repeat business. Inevitable mechanical losses mean make-up is necessary from time to time to maintain anti-freeze protection. Only "Telar" can be added to "Telar"—the customer comes back to you! "Telar" will be sold only through servicing dealers because of the importance of proper servicing of the cooling system at the time of installation.

Nobody wants to drain anti-freeze. At best, it's a messy job. That's why all your customers are prospects for "Telar", not just a select few. Because "Telar" protects in all 4 seasons, not just in winter,



Ammeters show how "Telar" stops galvanic action (electrical current) that causes corrosion and rust in cooling system. Ammeter, right, shows no current flow in "Telar", compared with strong current flow in water, left.

the customer gets more protection for his money. And he saves money in the long run—actually in just 2 years of use. Anyone with a radiator is a prospect for "Telar".



"Telar" will be sold only through servicing dealers because of the importance of proper servicing at the time of installation.

DELCO MORaine
BRAKE PART
REPAIR KIT
5459082

DIVISION OF GENERAL MOTORS CORP., DAYTON, OHIO

- **REPAIR KITS**—all the parts generally needed to restore peak operating efficiency.
- ▲ **MAJOR COMPONENTS**—individual replacements are available when extensive rebuilding is required.
- **COMPLETE ASSEMBLIES**—full unit for immediate restoration of original equipment performance.



It's easy to service Delco Moraine POWER BRAKES

Kits, parts and complete assemblies are readily available through car dealers and U.M.S. outlets. Proper diagnosis will determine which is the best way to restore dependable performance.


Ask your dealer for copies of the Delco Moraine power brake service manuals. They will help you perform all repairs surely and efficiently, as recommended by the original manufacturer.

DELCO MORaine


DEPENDABLY MADE

Division of General Motors, Dayton, Ohio

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IT'S NEW

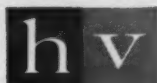
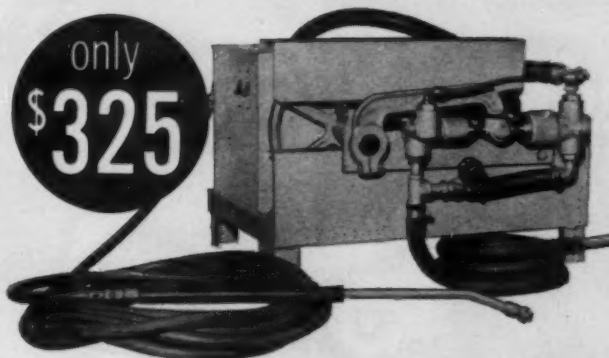


Heat makes the difference! Cars washed sparkling clean in 7 to 10 minutes. Motors washed really clean for \$3 to \$4 extra profit.

New Homestead Multi-Job Washer washes motors and cars...saves you \$1,000 in the bargain!

Homestead's revolutionary, new MULTI-JOB WASHER does the work of a \$1,000 semi-automatic car washer *plus* the job of a \$400 engine cleaner, *yet is priced lower than the cost of the engine cleaner alone!*

Makes \$6 to \$12 extra cash per hour. Washes cars, cleans motors, back-flushes cooling systems . . . all in one stall. The Multi-Job Washer is not a steam cleaner. The only pressure washer with controlled heat, it heats solution electrically, and is electrically powered, too. No flame. No fumes. It is comfortable and convenient for year around use indoors. Takes no more floor space than a 55-gallon drum. Makes you more money . . . yet costs as little as \$325! Order from your authorized Jenny® Distributor.



HOMESTEAD VALVE MANUFACTURING COMPANY
 Hypressure Jenny Division — Coraopolis, Pa.
 (In Canada: Hypressure Jenny Sales & Service, Ltd.,
 517 Jarvis St., Toronto 5, Ont.)

Please send me full information on the extra profits I can enjoy with the new Homestead Multi-Job Washer.

Name.....Title.....

Company.....

Address.....

City.....Zone.....State.....

Chevrolet Dealers Have the Largest Reservoir of Customers in the Business!

With Chevrolet cars and trucks outnumbering any other make on the road by nearly 3,000,000, Chevrolet dealers can draw on a sales potential enjoyed by few businessmen. And, thanks to customer-keyed Chevrolet programs, this potential promises to become even greater in the future.

But sales potential is only part of the story of what makes a Chevrolet dealer happy. There's Chevy's wonderful range of products, wide enough to meet virtually every automotive need: a full line of conventional cars; the revolutionary Corvair; Corvette, America's only true sports

car; a complete choice of Sturdi-Bilt trucks.

And how about Chevrolet's vigorous new quality control manufacturing program? . . . an important step in seeing that these products reach the dealer with a minimum of preparation necessary for prompt delivery. Also, parts pose no problem at all to the Chevrolet dealer. He can keep his shelves well stocked from the 41 parts depots Chevrolet maintains at strategic locations across the country. Replacements can be had without delay.

The newest element in the picture is the forward-looking Owner Rela-

tions Program created by Chevrolet in cooperation with Chevrolet dealers. Its single purpose is to provide a constant interchange of ideas on sales, service, customer preferences—all the things that mean greater customer satisfaction and a steady Chevrolet market.

We think these (and there are many more) are pretty convincing reasons why Chevrolet dealers enjoy a decided advantage over any other dealers in the industry. . . . Chevrolet Division of General Motors, Detroit 2, Michigan.



Chevrolet dealers are No. 1 with customers because customers are No. 1 with Chevrolet dealers!

ANNOUNCING

a revolutionary new
hydro-pneumatic suspension assist—

THE

GOLDEN GLIDE Load-Absorber

PAT. APPL'D. FOR

Improves load-carrying capacity without impairing
riding comfort—even under normal load conditions

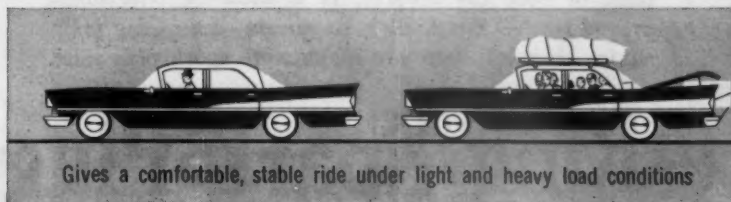
Golden Glide announces a great new advance in the field of ride control—the self-compensating, adjustable, hydro-pneumatic load absorber.

Installed in place of shock absorbers in the rear of the car, Load-Absorbers do everything any combination of steel helper springs and shocks can do—and much more.

Golden Glide Load-Absorbers carry the load on a cushion of air at any riding height the car owner desires. They automatically sense a change in the car load, automatically compensate by hydro-pneumatic pressures to maintain level and stable riding qualities. Each Load-Absorber is a completely sealed unit containing a flexible-wall air cushion and an oversize new design hydraulic shock absorber.

Golden Glide Load-Absorbers are pre-set at the factory for light, normal, or heavy loads and driving conditions. To adjust for *extremely* heavy loads or trailers of any kind, an external valve on the Load-Absorber is provided for increasing air pressure, thereby raising the rear of the car to the desired riding height.

Under all road and load conditions Golden Glide Load-Absorbers will improve load carrying capacity, stability and riding qualities. Call, wire or write for further details.



Gives a comfortable, stable ride under light and heavy load conditions

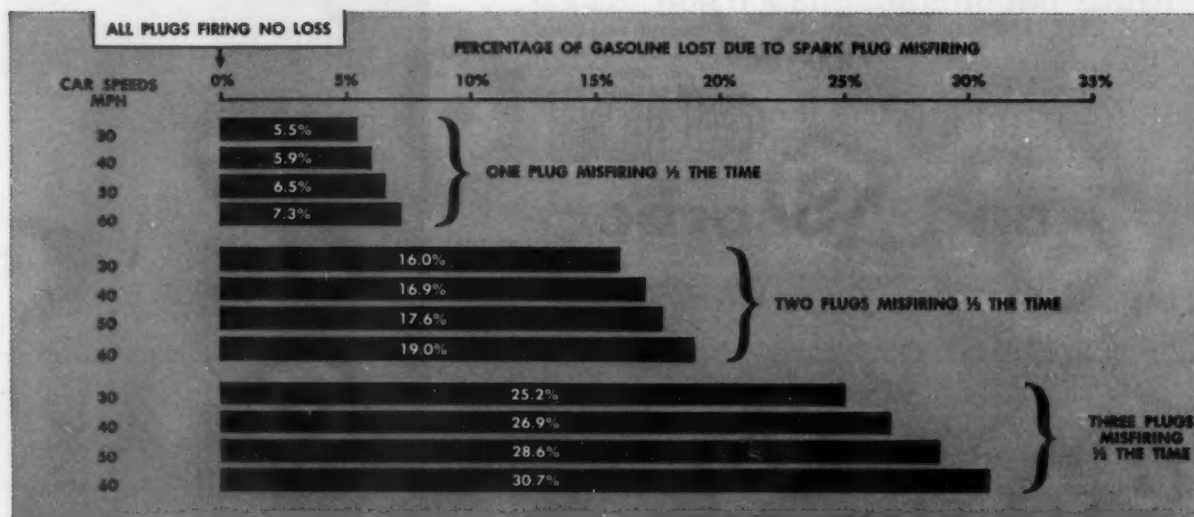
GOLDEN GLIDE

Load-Absorber

PAT. APPL'D. FOR

THE GOLDEN GLIDE SHOCK ABSORBER COMPANY, Cleveland 15, Ohio

Show them, sell them,



Show your customers how much they lose when Spark Plugs misfire!



Show Spark Plug Misfiring with the ACilloscope

You can prove to your customers whether or not their spark plugs are misfiring, by use of AC's new portable electronic spark plug analyzer—the ACilloscope. Sell more spark plugs, make more money, with this profit-building tool.

Modern high horsepower engines make it difficult for a driver to tell when a spark plug is misfiring. Yet spark plugs tend to misfire intermittently when worn, and countless drivers daily operate their cars unaware of the power they are losing and the gasoline they are wasting due to spark plugs that are working only part of the time.

The chart above shows what happens to gasoline mileage when one, two or three spark plugs misfire half the time. And a little arithmetic will show how much they can reduce mileage at various speeds. For example, in a car that gets 16 miles per gallon at 60 miles per hour, one spark plug firing half the time can drop mileage to 14.8 miles per gallon—two can drop it to 13—and three can drop it to less than 12.

Clip this chart and show it to your customers who complain of poor gas mileage. It is *authentic information* proved by AC Spark Plug Engineers—*convincing information* that will help you make more spark plug sales.

New **AC** Fire-Ring Spark Plugs

the Spark Plugs they need !



Show them how much they gain
with these AC Fire-Ring features!

Greater Power and Mileage with AC's exclusive Hot Tip that heats faster to stay clean longer, cools faster to prevent engine-damaging pre-ignition and power loss.

Sure Firing provided by the AC Butress Top Insulator that lengthens the flash-over path and reduces misfiring caused by the spark going down the outside of the insulator rather than through the center electrode.

Longer Peak Performance through the patented AC Isovolt electrodes that increase spark plug life and resist misfiring by decreasing the rate of electrode gap wear.

Smoother Operation with the exclusive AC copper-glass seal that bonds center electrode and terminal screw in an unyielding bond, preventing gas leakage that can alter heat range, cause spark plug and engine damage.

AC SPARK PLUG ❁ THE ELECTRONICS DIVISION OF GENERAL MOTORS

AC Presents The Art Carney Show, NBC-TV, May 6, 1960

— THEY MUST BE THE BEST!



Patents on chisels applied for

AP

1. 15-Minute Installation

Now you can install new mufflers in only 15 minutes. AP dealers can get this exclusive, heavy-duty muf-fler tool and three exclusive chisels FREE from their wholesalers with the purchase of only 5 fastest-selling AP mufflers at retail price.



2. Finest Quality

AP gives you more quality features at no extra cost than you can get in any other muffler. And it gives them to you because they are needed to provide your customers with the long-lasting mufflers they want.



3. Dealer Identification

The now famous and much-imitated AP station signs are available in two sizes and at two prices to suit your needs. There are more than 45,000 of these signs now in use from coast to coast, making the AP name known to millions of motorists.

provides you with all 5 basic needs to make more muffler profits



4



5

4. Complete Technical Information

This manual, free to dealers on request, is packed with profit-making information on how to make more money in the muffler business, how to get the most out of your exclusive AP pneumatic muffler gun.

5. Customer-Getting National Ads

AP really goes after customers for you with a consistent advertising program in TRUE, ARGOSY, POPULAR MECHANICS and MECHANIX ILLUSTRATED written by the famous Tom McCahill, (right, above), plus a spectacular promotion in LOOK and a barrage of steady sales messages over CBS radio network with Walter Cronkite (left, above).

These five basics are essential for the dealer who wants to make money in mufflers. Not only do you get them all from AP, but you get quality as well as quantity. Properly used . . . and used together . . . these five basics enable you to cash in to the fullest on muffler business.

**MORE FOR YOUR MONEY...
MORE MONEY FOR YOU**

with

AP

THE AP PARTS CORPORATION
3-Q AP BUILDING • TOLEDO 1, OHIO
WORLD'S LARGEST MANUFACTURER OF REPLACEMENT MUFFLERS



more
miles
per
lining

Trucks and buses get more miles per lining from American Brakeblok. Highest quality you can use. Riveted, bonded, heavy duty. All available through over 2700 NAPA jobbers coast to coast.

*American
Brakeblok®*



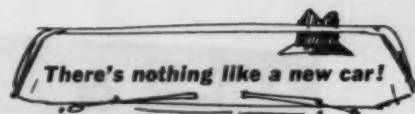
P. O. BOX 21, BIRMINGHAM, MICHIGAN
DIVISION OF AMERICAN BRAKE SHOE CO.

AMERICAN BRAKEBLOK

THERE'S NOTHING
LIKE A NEW CAR!



\$ 00.59 Mc
\$ 01.24 6r
\$ 00.39 6r
\$ 00.29 6r
\$ 00.39 6r
\$ 00.29 6r
\$ 00.39 6r
\$ 00.55 6r
\$ THERE'S
\$ NOTHING
\$ LIKE A
\$ NEW CAR!



It's spring—and folks are thinking about new cars.

Part of the reason is General Motors advertising that continually tells your prospects: "There's Nothing Like A New Car!" You've seen these big, exciting colorful ads in leading national publications. They've appeared ever since last fall to supplement sales efforts of dealers. Each stresses the satisfaction of new car ownership—and pounds home the point that nothing returns so much in family fun for so long a time as does that new car purchase. The millions of new car prospects who read these ads have more money to spend than ever before, and more spare time in which to spend it.

Yet authorities predict this unprecedented prosperity to be just the beginning! For the next few years they foresee more families with higher incomes, a greater swing to the suburbs, and many more multiple-car families—all contributing to increased car sales.

To help every GM dealer share in the prosperity of our expanding economy, General Motors is sponsoring this advertising effort—making it great to be a GM dealer in the Sixties!

GENERAL MOTORS

GO GM FOR '60

CHEVROLET • PONTIAC • OLDSMOBILE • BUICK • CADILLAC All work Only by Fisher

Here's why Champion is launching the biggest

IN MAGAZINES AND ON RADIO, CHAMPION URGES MOTORISTS TO GET PLUGS CHECKED



"Say, mister, why do worn spark plugs waste lots of gas?"

"Well, you see, son, a spark plug that's worn often misfires—that means it doesn't give *any* spark at all. When this happens, a lot of gas you paid good money for is never ignited. It's just pumped out the exhaust without even being burned."

"I get it. But how can I tell if my plugs are getting worn?"

"With your one-cylinder car you'll feel it right away!

But your Dad—or the average driver—with a powerful family car never knows it. He just keeps on wasting power and money!"

"Gee, how can a guy win?"

"Easy. Just have the plugs checked at 5,000 miles to see if they need servicing. And to play it safe, put in a new set of Champions every 10,000 miles."



CHAMPION

This full-page advertisement, in full color, will appear in May *READER'S DIGEST*, the April 16th *SATURDAY EVENING POST*, and the April 25th *LIFE*. These magazines reach the owners of 6 out of 10 cars—24 million motorists. In addition, Champion is using a heavy spot

radio campaign during April, May and June, plus spots on *NBC's MONITOR* on April weekends. And *every 10 days* throughout the year, a Champion ad in *LIFE*, *LOOK*, *POST* or *READER'S DIGEST* reminds motorists to get plugs checked.

"Check Your Plugs" program in history!

Most motorists just can't tell if their spark plugs are in good condition. 7 out of every 10 cars need spark plug service or replacement. So, this spring Champion is telling your customers why they need a plug check.

Because now—with your Champion Plug-Scope—spark plug checking is quick, easy and profitable—and a real service to your customers...

Your customers need to have their spark plugs checked. With today's powerful, "soundproofed" cars, the average driver just can't tell whether or not his plugs are firing full-time. And many plugs are not.

A recent survey by *MOTOR* magazine showed, as have other surveys, that *every other car* actually needs new spark plugs right now. The way to tell *which* cars need new plugs is to check plugs. But plug checking was a time-consuming chore—until Champion developed the Plug-Scope.

With the Plug-Scope, you check a set of plugs in seconds—*electronically*. It's now easy—as well as profitable—to give plug checks. And that's why Champion is now sponsoring the biggest "get your plugs checked" program in spark plug history—and paving the way for you to make extra sales and profits.

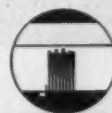
If you don't already have a Champion Plug-Scope, get one from your Champion supplier right now—so you can take full advantage of Champion's big promotion during Spring "car care" time.

DAYTONA FLASH: Champion sparks the winners at Daytona! The 500-mile Daytona Sweepstakes was won by Junior Johnson in a Champion-sparked '59 Chevy. Both Compact Car races were won by Marvin Panch in a Champion-equipped Valiant. The two 100-mile stock car races were won by Jack Smith and Fireball Roberts—both driving Champion-sparked '60 Pontiacs.

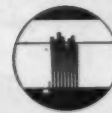
The Champion Plug-Scope checks spark plugs electronically, right in the engine—in less than 60 seconds!

With just four fast connections, the Champion Plug-Scope is hooked up and ready to go.

Accelerate the engine and release immediately, watching the red warning line on the Plug-Scope screen. If traces rise above the line, plugs need attention. It's that simple!



SATISFACTORY SPARK PLUG



UNSATISFACTORY SPARK PLUG

No guesswork, no "interpretation" required—you know at a glance if plugs need attention!

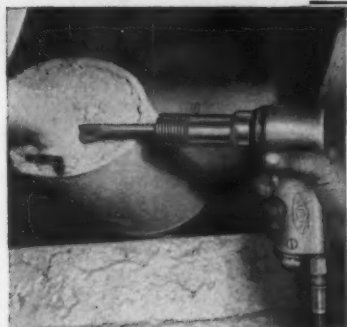


SPARK PLUGS



CHAMPION SPARK PLUG COMPANY • TOLEDO 1, OHIO

NEW **SIoux** air slugger



**PAYS FOR ITSELF IN
A HURRY ON MUFFLER
AND TAILPIPE JOBS . . .
A MONEY MAKER IN
THE BODY SHOP!**



SHEET METAL PUNCH—punches perfect holes for metal screws, faster than you can put them in place; aligns garnish rails; punches out small rivets.



Breaks spot welds quickly, easily and inexpensively.



QUICKLY SECTIONS BODY PANELS, DOORS, fenders, with a perfect overlap; follows curves easily; takes the warp out of section welding; makes a strong neat joint.

The SIoux Air Slugger is one of the most useful of air tools. It will not create the hazard of fire or explosion and will save cost of torches and gas. Design features include a one piece barrel with no parts to get misaligned or wear out; and a cast iron handle that will survive the roughest treatment. It's light, sturdy, and Sioux dependable.

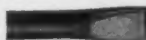


METAL CUTTER

REMOVES QUARTER PANELS, TURRET TOPS, rocker panels, lower body panels, door inner panels, etc. Cuts smooth and fast on any radius with excellent maneuverability.



CHISEL—breaks spot welds, splits nuts, shears bolts, pulls pins, removes floor boards.



RIVET BUSTER—shears rivets in grill, frame, bumper, brackets, wherever they are.



PUNCH—punches out wedged rivets, drives king pins, perch bolts, shock link studs, bearing races, and pressed bearings.



SCRAPER—removes underseal and deadener, without heat. Removes all soft deadener without gumming. Cleans the surface ready for work. Will not damage the metal.



EXTENSION

CHISEL gets at those "hard to reach" places; replace individual grill bars, remove rocker panels to be repaired and replaced; removes Ford front door posts without removing dash.



Look for **SIoux** Tools in the yellow pages under "TOOLS ELECTRIC"

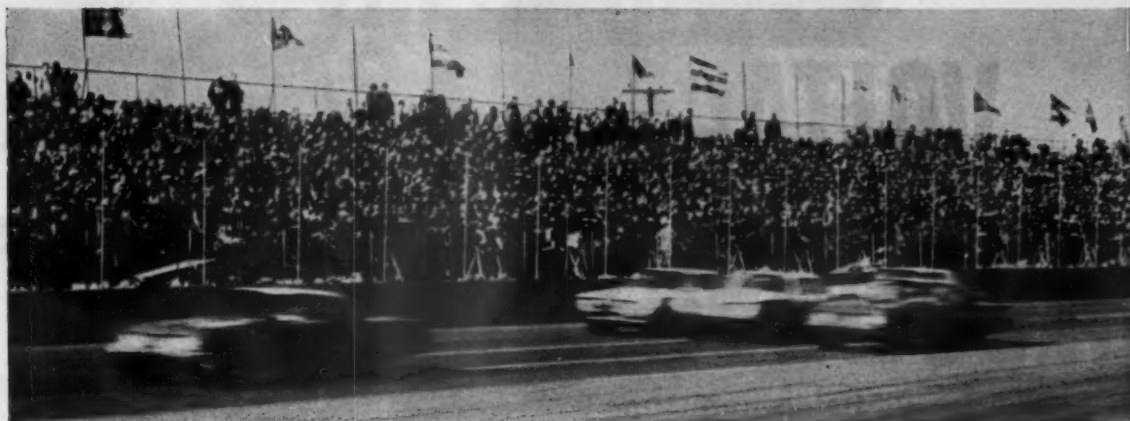
ALBERTSON & CO., INC.

SIoux CITY, IOWA, U.S.A.

AIR IMPACT WRENCHES • AIR SCREWDRIVERS • ELECTRIC IMPACT WRENCHES • DRILLS • SCREWDRIVERS
• GRINDERS • SANDERS • POLISHERS • FLEXIBLE SHAFTS • PORTABLE SAWS • VALVE GRINDING MACHINES
• ABRASIVE DISCS.

Grey-Rock quality proved again at Daytona

**Out of 60 qualified starters, 55 used
Grey-Rock brake linings**



The consistent preference of NASCAR drivers for Grey-Rock linings was evidenced again in the grueling 500-mile Sweepstakes at the Daytona, Fla., International Speedway, where speeds at times were in excess of 150 mph.

Proved quality and performance like this make your job of selling Grey-Rock easier. With its big, complete merchandising program and the most powerful consumer advertising ever,*

Grey-Rock is trying in every way to make your job still easier. You can help, too. Tie in by displaying the colorful Grey-Rock sign that brings in more customers and more profits. Ask your Grey-Rock jobber how you can get it as part of the 6-set "Sales-Maker." Grey-Rock Division of Raybestos-Manhattan, Inc., Manheim, Pa.

**Advised in the Saturday Evening Post and on ABC National Network Radio*

Only Grey-Rock makes BALANCED BRAKES LININGS
BALANCED BRAKES • TRUCKSETS • BRAKE BLOCKS • VEE LOK® CLUTCH FACINGS • AUTOMATIC TRANSMISSION PARTS

You Can't Buy a Better Brake Lining to Save Your customer's Life

DELCO-REMY VOLTAGE REGULATORS

[a part of the big Delco-Remy line]



ALL CARS?



ALL POPULAR CARS!

ARE MADE FOR ALL

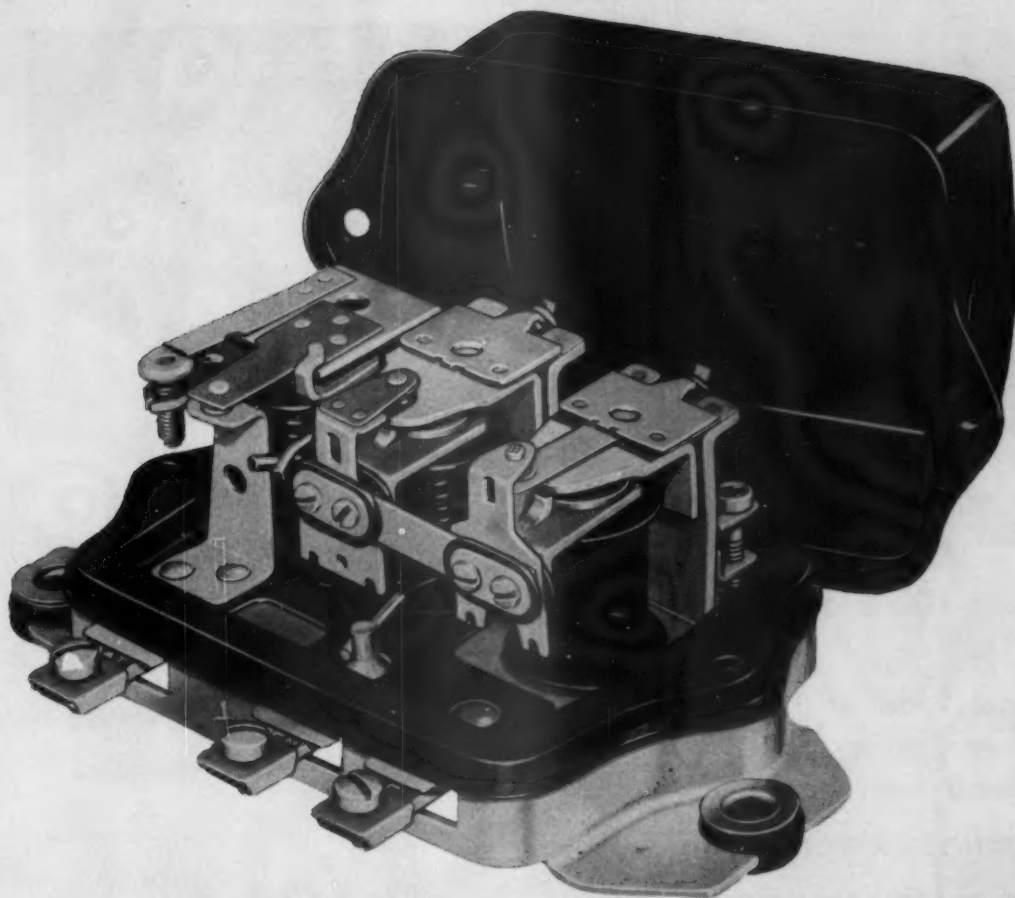
Simplify your stock—satisfy your customer! Now, with the big Delco-Remy line alone, you're set to service all popular American cars. And you offer the quality and reliability of parts made by the world's largest original equipment manufacturer of automotive electrical systems.

Ideal for replacement are Delco-Remy *waterproof* voltage regulators. What keeps them waterproof?

- New overhanging one-piece steel covers
- Molded nylon insulators
- Soft rubber gaskets.

What makes them top performers?

- Newly designed armature contact springs



POPULAR AMERICAN CARS

- Tungsten and precious metal contact points
- Welded electrical connections.

Adjustment of all three units is easy and highly accurate with special fine-thread screw-type controls.

Regulators and other Delco-Remy electrical parts are available from your car or truck dealer or through the United Motors System.

FROM THE HIGHWAY TO THE STARS

Delco-Remy
ELECTRICAL SYSTEMS

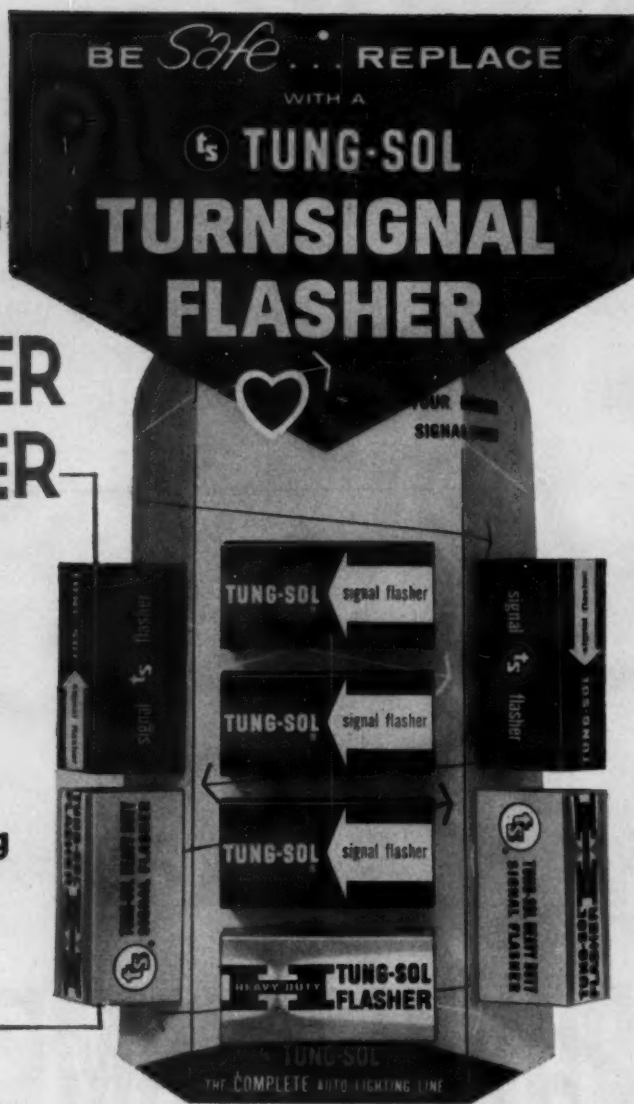


DIVISION OF GENERAL MOTORS • ANDERSON, INDIANA

NEW!

TUNG-SOL No. 8 SIGNAL FLASHER MERCHANDISER

- Services 100% of all vehicles*
- Reminds you and your customers to replace faulty flashers now
- Streamlines inventorying
- Builds extra sales and service profits



This sales-pulling card goes to work for you the minute you put it on your counter. It reminds you and your customers to replace faulty flashers... *extra* sales and service profits that are often missed... *extra* profits that take only minutes to make. Get your share. Order your fully stocked flasher counter card from your Tung-Sol supplier.

Automotive Products Division
TUNG-SOL ELECTRIC INC.
Newark 4, N.J.

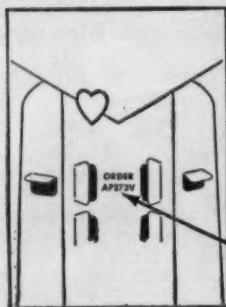
* Stocks one each:

A229S
AP273V

AP373V
P273D

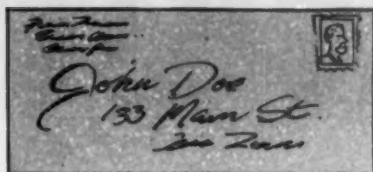
UP229D
534

535
536



TUNG-SOL		REPLACEMENT CHART	
Flasher	Part No.	Flasher	Part No.
AP273V	534	AP373V	535
UP229D	536	P273D	537
A229S	538	AP273V	539
AP373V	540	UP229D	541
P273D	542	A229S	543
AP273V	544	AP373V	545
UP229D	546	P273D	547
A229S	548	AP273V	549
AP373V	550	UP229D	551
P273D	552	A229S	553
AP273V	554	AP373V	555
UP229D	556	P273D	557
A229S	558	AP273V	559
AP373V	560	UP229D	561
P273D	562	A229S	563
AP273V	564	AP373V	565
UP229D	566	P273D	567
A229S	568	AP273V	569
AP373V	570	UP229D	571
P273D	572	A229S	573
AP273V	574	AP373V	575
UP229D	576	P273D	577
A229S	578	AP273V	579
AP373V	580	UP229D	581
P273D	582	A229S	583
AP273V	584	AP373V	585
UP229D	586	P273D	587
A229S	588	AP273V	589
AP373V	590	UP229D	591
P273D	592	A229S	593
AP273V	594	AP373V	595
UP229D	596	P273D	597
A229S	598	AP273V	599
AP373V	600	UP229D	601
P273D	602	A229S	603
AP273V	604	AP373V	605
UP229D	606	P273D	607
A229S	608	AP273V	609
AP373V	610	UP229D	611
P273D	612	A229S	613
AP273V	614	AP373V	615
UP229D	616	P273D	617
A229S	618	AP273V	619
AP373V	620	UP229D	621
P273D	622	A229S	623
AP273V	624	AP373V	625
UP229D	626	P273D	627
A229S	628	AP273V	629
AP373V	630	UP229D	631
P273D	632	A229S	633
AP273V	634	AP373V	635
UP229D	636	P273D	637
A229S	638	AP273V	639
AP373V	640	UP229D	641
P273D	642	A229S	643
AP273V	644	AP373V	645
UP229D	646	P273D	647
A229S	648	AP273V	649
AP373V	650	UP229D	651
P273D	652	A229S	653
AP273V	654	AP373V	655
UP229D	656	P273D	657
A229S	658	AP273V	659
AP373V	660	UP229D	661
P273D	662	A229S	663
AP273V	664	AP373V	665
UP229D	666	P273D	667
A229S	668	AP273V	669
AP373V	670	UP229D	671
P273D	672	A229S	673
AP273V	674	AP373V	675
UP229D	676	P273D	677
A229S	678	AP273V	679
AP373V	680	UP229D	681
P273D	682	A229S	683
AP273V	684	AP373V	685
UP229D	686	P273D	687
A229S	688	AP273V	689
AP373V	690	UP229D	691
P273D	692	A229S	693
AP273V	694	AP373V	695
UP229D	696	P273D	697
A229S	698	AP273V	699
AP373V	700	UP229D	701
P273D	702	A229S	703
AP273V	704	AP373V	705
UP229D	706	P273D	707
A229S	708	AP273V	709
AP373V	710	UP229D	711
P273D	712	A229S	713
AP273V	714	AP373V	715
UP229D	716	P273D	717
A229S	718	AP273V	719
AP373V	720	UP229D	721
P273D	722	A229S	723
AP273V	724	AP373V	725
UP229D	726	P273D	727
A229S	728	AP273V	729
AP373V	730	UP229D	731
P273D	732	A229S	733
AP273V	734	AP373V	735
UP229D	736	P273D	737
A229S	738	AP273V	739
AP373V	740	UP229D	741
P273D	742	A229S	743
AP273V	744	AP373V	745
UP229D	746	P273D	747
A229S	748	AP273V	749
AP373V	750	UP229D	751
P273D	752	A229S	753
AP273V	754	AP373V	755
UP229D	756	P273D	757
A229S	758	AP273V	759
AP373V	760	UP229D	761
P273D	762	A229S	763
AP273V	764	AP373V	765
UP229D	766	P273D	767
A229S	768	AP273V	769
AP373V	770	UP229D	771
P273D	772	A229S	773
AP273V	774	AP373V	775
UP229D	776	P273D	777
A229S	778	AP273V	779
AP373V	780	UP229D	781
P273D	782	A229S	783
AP273V	784	AP373V	785
UP229D	786	P273D	787
A229S	788	AP273V	789
AP373V	790	UP229D	791
P273D	792	A229S	793
AP273V	794	AP373V	795
UP229D	796	P273D	797
A229S	798	AP273V	799
AP373V	800	UP229D	801
P273D	802	A229S	803
AP273V	804	AP373V	805
UP229D	806	P273D	807
A229S	808	AP273V	809
AP373V	810	UP229D	811
P273D	812	A229S	813
AP273V	814	AP373V	815
UP229D	816	P273D	817
A229S	818	AP273V	819
AP373V	820	UP229D	821
P273D	822	A229S	823
AP273V	824	AP373V	825
UP229D	826	P273D	827
A229S	828	AP273V	829
AP373V	830	UP229D	831
P273D	832	A229S	833
AP273V	834	AP373V	835
UP229D	836	P273D	837
A229S	838	AP273V	839
AP373V	840	UP229D	841
P273D	842	A229S	843
AP273V	844	AP373V	845
UP229D	846	P273D	847
A229S	848	AP273V	849
AP373V	850	UP229D	851
P273D	852	A229S	853
AP273V	854	AP373V	855
UP229D	856	P273D	857
A229S	858	AP273V	859
AP373V	860	UP229D	861
P273D	862	A229S	863
AP273V	864	AP373V	865
UP229D	866	P273D	867
A229S	868	AP273V	869
AP373V	870	UP229D	871
P273D	872	A229S	873
AP273V	874	AP373V	875
UP229D	876	P273D	877
A229S	878	AP273V	879
AP373V	880	UP229D	881
P273D	882	A229S	883
AP273V	884	AP373V	885
UP229D	886	P273D	887
A229S	888	AP273V	889
AP373V	890	UP229D	891
P273D	892	A229S	893
AP273V	894	AP373V	895
UP229D	896	P273D	897
A229S	898	AP273V	899
AP373V	900	UP229D	901
P273D	902	A229S	903
AP273V	904	AP373V	905
UP229D	906	P273D	907
A229S	908	AP273V	909
AP373V	910	UP229D	911
P273D	912	A229S	913
AP273V	914	AP373V	915
UP229D	916	P273D	917
A229S	918	AP273V	919
AP373V	920	UP229D	921
P273D	922	A229S	923
AP273V	924	AP373V	925
UP229D	926	P273D	927
A229S	928	AP273V	929
AP373V	930	UP229D	931
P273D	932	A229S	933
AP273V	934	AP373V	935
UP229D	936	P273D	937
A229S	938	AP273V	939
AP373V	940	UP229D	941
P273D	942	A229S	943
AP273V	944	AP373V	945
UP229D	946	P273D	947
A229S	948	AP273V	949
AP373V	950	UP229D	951
P273D	952	A229S	953
AP273V	954	AP373V	955
UP229D	956	P273D	957
A229S	958	AP273V	959
AP373V	960	UP229D	961
P273D	962	A229S	963
AP273V	964	AP373V	965
UP229D	966	P273D	967
A229S	968	AP273V	969
AP373V	970	UP229D	971
P273D	972	A229S	973
AP273V	974	AP373V	975
UP229D	976	P273D	977
A229S	978	AP273V	979
AP373V	980	UP229D	981
P273D	982	A229S	983
AP273V	984	AP373V	985
UP229D	986	P273D	987
A229S	988	AP273V	989
AP373V	990	UP229D	991
P273D	992	A229S	993
AP273V	994	AP373V	995
UP229D	996	P273D	997
A229S	998	AP273V	999
AP373V	1000	UP229D	1001
P273D	1002	A229S	1003
AP273V	1004	AP373V	1005
UP229D	1006	P273D	1007
A229S	1008	AP273V	1009
AP373V	1010	UP229D	1011
P273D	1012	A229S	1013
AP273V	1014	AP373V	1015
UP229D	1016	P273D	1017
A229S	1018	AP273V	1019
AP373V	1020	UP229D	1021
P273D	1022	A229S	1023
AP273V	1024	AP373V	1025
UP229D	1026	P273D	1027
A229S	1028	AP273V	1029
AP373V	1030	UP229D	1031
P273D	1032	A229S	1033
AP273V	1034	AP373V	1035
UP229D	1036	P273D	1037
A229S	1038	AP273V	1039
AP373V	1040	UP229D	1041
P273D	1042	A229S	1043
AP273V	1044	AP373V	1045
UP229D	1046	P273D	1047
A229S	1048	AP273V	1049
AP373V	1050	UP229D	1051
P273D	1052	A229S	1053
AP273V	1054	AP373V	1055
UP229D	1056	P273D	1057
A229S	1058	AP273V	1059
AP373V	1060	UP229D	1061
P273D	1062	A229S	1063
AP273V	1064	AP373V	1065
UP229D	1066	P273D	1067
A229S	1068	AP273V	1069
AP373V	1070	UP229D	1071
P273D	1072	A229S	1073
AP273V	1074	AP373V	1075
UP229D	1076	P273D	1077
A229S	1078	AP273V	1079
AP373V	1080	UP229D	1081
P273D	1082	A229S	1083
AP273V	1084	AP373V	1085
UP229D	1086	P273D	1087
A229S	1088	AP273V	1089
AP373V	1090	UP229D	1091
P273D	1092	A229S	1093
AP273V	1094	AP373V	1095
UP229D	1096	P273D	1097
A229S	1098	AP273V	1099
AP373V	1100	UP229D	1101
P273D	1102	A229S	1103
AP273V	1104	AP373V	1105
UP229D	1106	P273D	1107
A229S	1108	AP273V	1109
AP373V	1110	UP229D	1111
P273D	1112	A229S	1113
AP273V	1114	AP373V	1115
UP229D	1116	P273D	1117
A229S	1118	AP273V	1119
AP373V	1120	UP229D	1121
P273D	1122	A229S	1123
AP273V	1124	AP373V	1125
UP229D	1126	P273D	1127
A229S	1128	AP273V	1129
AP373V	1130	UP229D	1131
P273D	1132	A229S	1133
AP273V	1134	AP373V	1135
UP229D	1136	P273D	1137
A229S	1138	AP273V	1139
AP373V	1140	UP229D	1141
P273D	1142	A229S	1143
AP273V	1144	AP373V	1145
UP229D	1146	P273D	1147
A229S	1148	AP273V	1149
AP373V	1150	UP229D	1151
P273D	1152	A229S	1153
AP273V	1		

Letters to the Editor



Floor Plans

Dear Editor:

In the December issue of your MOTOR AGE publication, your feature story on good shopkeeping was reviewed with much interest. We felt the article was very informative and contained much in the way of sound planning. Our prime objective here, in drawing dealership plans, is to provide maximum utilization of all potential productive space.

We noticed in one of your layouts that there is one productive space for each 560 square feet within the total service area. This is in excess of our maximum allowance in laying out service facilities.

We try to keep this figure down to under 500 square feet per potential productive space and this should include aisles and any parking area allotted.

D. H. Cunningham
Service Management Coordinator
Chrysler Corp.
Detroit, Mich.

Dear Editor:

We are interested in floor plans for a medium size garage. In the February issue of MOTOR AGE, "Letters to the Editor page," is a request for floor plans. The conditions outlined in that letter are about the same as ours. We would appreciate having sample floor plans if they are available.

L. H. Jackson & Son
L. H. Jackson
Portland, Conn.

Dear Editor:

I am a reader of MOTOR AGE, and like it. I am planning a new shop building, 60 feet by 80 feet. I specialize in front end alignment. I want to do body repairing and wreck work. I'm not sure how I want to plan the shop. I want to work about four men. I just wondered if you could help me on a space floor plan and door arrangement. Will appreciate any help you can give me.

J. H. Bennett
Bennett Body Shop
Tifton, Ga.

All Around Bumper

Dear Editor:

After sixty years of production experience the automobile manufacturers still leave the ornate vehicles



vulnerable to frontal, side and rear demolition by other cars. It would seem practicable to build in all-around bumpers of a substantial nature.

Stylewise, a fickle public that bought '59 models (The Year of the Grotesque Automobile) could be educated to want a car with built-in safety provisions.

F. P. Sherry
San Rafael, Calif.

Note of Appreciation

Dear Editor:

The Hough Chevrolet Company of Fort Myers, Florida, greatly appreciates the article and pictures in your February issue of MOTOR AGE, entitled "Motor Age's Shop of the Month."

We are very proud of this article and these pictures and would like several reprints.

Harold C. Case, Sls. Mgr.
Ernest C. Bowman, Ser. Mgr.
Hough Chevrolet Co.
Fort Myers, Fla.

Interested in Transistors

Dear Editor:

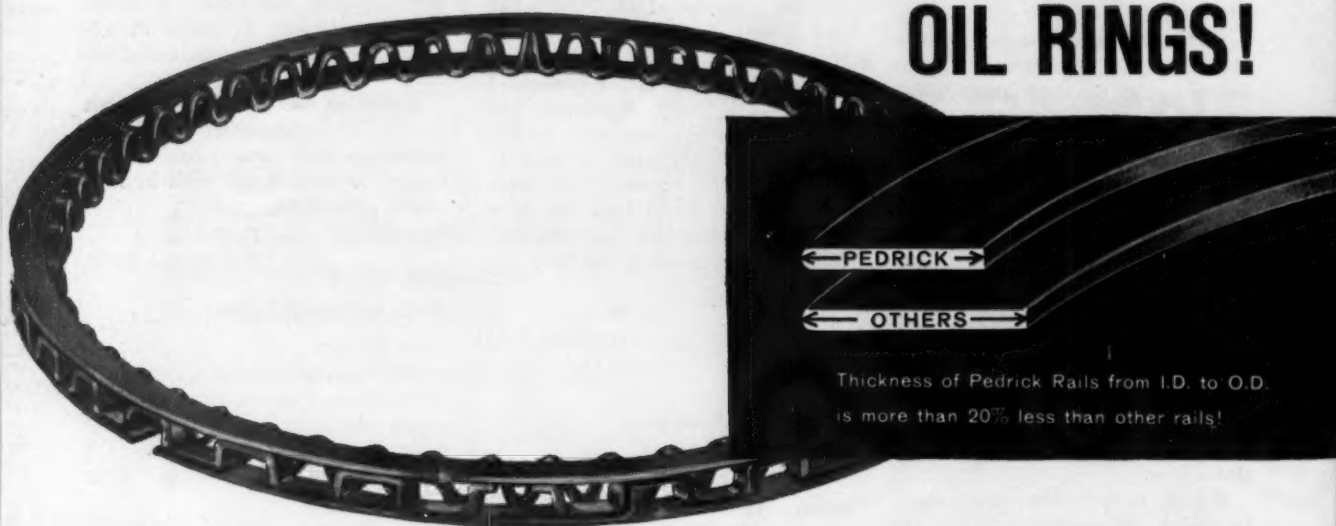
Many years have passed since I wrote the last letter for your magazine in 1938. However, I have kept in touch with your magazine and manuals since then and also thru a seven year tour of duty in the Ordnance Dept., World War II.

Your article in Jan., 1960 on transistors has aroused my deep interest.

Cal Crooks
Worcester, Mass.

Double the **CONFORMABILITY**

WITH
Pedrick FORMFLEX CHROME
OIL RINGS!



It is an engineering fact that *20% less radial thickness* delivers *double* the amount of flexibility. And flexibility determines the ability of a piston ring to conform to conditions of cylinder wear and distortion.

Pedrick's distinctive 4-piece construction, in which the "Equalizer" and Spacer are separate pieces, makes it possible to use rails at least 20% less in radial thickness and therefore much more flexible and conformable. Only by using Pedrick Formflex Chrome Rings do you get such exceptional conformability. The results are unequaled overall performance and piston ring life. Ask for them at your favorite jobber.

In addition, PEDRICK FORMFLEX CHROME RINGS are:

- Easy to Install.
- Effective in Side Sealing.
- Effective in Not Plugging.
- All Purpose—for any Kind of Service.
- Chrome-faced for Longer Life.

DEPEND ON

Pedrick

FOR THE BEST RING JOB

WILKENING MANUFACTURING CO., Philadelphia 42, Pa. • In Canada: Wilkening Manufacturing Co. (Canada) Ltd., Toronto 2

Automotive Refinish Institute to encourage auto refinish shops



L. J. O'Doherty, Chairman of ARI's Executive Committee & Director of Sales, Automotive Div., Acme Quality Paints, Inc.

A program to build awareness for the Automotive Refinish Institute was approved by the group at its annual meeting in Feb. The meeting was held during the International Automotive Service Industries Show. Chairman of the ARI executive committee is L. J. O'Doherty (Acme Quality Paints, Inc.).

Among matters discussed was the proposal by W. E. Marsden of Minnesota Mining and Mfg. Co. Mr. Marsden is vice chairman of the ARI executive committee. He suggested the formation of a committee to study ways and means of acquainting the automotive refinish industry with ARI's basic principles.

Five Types of Business

The committee would be com-

posed of one representative from each of the five types of business represented in ARI.

Named to head the ways and means committee was ARI secretary J. H. Gustafson (also 3M company). Other members, with their group and company affiliation, are: E. N. Grossman, paint manufacturers, Arco Paint Co., Cleveland; Homer Lange, spray equipment manufacturers, DeVilbiss Co., Toledo; Wes Kiley, tools and equipment manufacturers, Blackhawk Manufacturing Co., Milwaukee; Carl Porter, sandpaper and tape manufacturers, Behr-Manning Co., Troy, N.Y.; and Peter Putz, chemicals, Jaycee Chemical Co., Northford, Conn.

Gustafson noted that the committee will meet in May in Chicago. There it will formulate plans. Then it will report back to the executive committee by June 1.

Meanwhile, suggestions and recommendations will be sought from the membership at large.

Also offered at the general membership meeting was a re-

port that work is continuing on the ARI standards manual. Publication is to be made "at a time when the Association feels awareness of itself and its aims is sufficient within the industry" to make the manual—and the ARI—workable.

In another feature of the meeting, Mr. O'Doherty restated the basic objectives of the ARI as they relate to upgrading quality within the automotive refinish industry. He presented a 12-point plan for ARI action.

Specifically, O'Doherty singled out the ARI cause as one "to increase our own sales by upgrading our portion of the automotive aftermarket." He cited these points as basic aims:

To help or encourage refinish shops to—

utilize space to best advantage . . .

utilize lighting, and stress cleanliness . . .

update equipment and tools . . .

use equipment properly . . .

adopt reliable purchasing

Continued on page V

NEWSETTES

AWDA Creates Memorial Award

The Board of Governors, of the Automotive Warehouse Distributors Assn., Inc. has established the Ernest A. Tapp Memorial Award. This award was established in memory of Ernest A. Tapp, one of the founders of the Automotive Warehouse Distributors Association.

He served for many years on AWDA's Board of Governors and was first vice president, in line for the Association's presidency, when his death occurred. He was president of the Jobbers Supply Company, Inc., in Kansas City, Mo., one of the country's pioneer warehouse distributors.

The award will be given each year to someone who, in the opinion of the Association, has performed a valuable service, or a series of valuable services to the Association. AWDA's Committee on Industry Relations will serve as a selection committee for the award.

Bendix Adopts New Name

The stockholders of Bendix Aviation Corporation voted to adopt a new name—"The Bendix Corporation." It is planned that the name change will be put into effect about June 1. This will permit adequate preparation for the change.

Malcolm P. Ferguson, president of Bendix, said: "The corporation is engaged in the manufacture and sale of not only aviation products but of a great number of automotive, electronic, nuclear, missile and space, marine, machine tool and industrial products. . . .

"It has become increasingly important for our corporation not to convey the impression that its products and skills are limited to the field of aviation."



In January, MOTOR AGE reported on the Automotive Electrical Associations election results for 1960. Above is shown the composition of the 1960 Board of Directors for AEA: Top Row: M. F. Marsau (Marsau's, Sterling, Col.); E. R. Stroh (Electric Autolite); S. G. Puryear (555 Inc., Little Rock, Ark.); Ray B. Roberts (Roberts Brothers, Washington, D.C.); Paul A. Peck (Mobile Electric Garage, Mobile, Ala.); A. H. Knorr (Knorr-Maryland, Detroit, Mich.); Center Row: J. Howard Reed (AEA-Detroit, Mich.); R. S. Withers (United Motors Service); Raymond L. Jones (Automotive Service, Bakersfield, Calif.); Carl S. Johnson (Automotive Electric Associates, Charlotte, N.C.); H. A. Stiefvater (Stiefvater Electric, Utica, N.Y.); P. H. Neville (Leece-Neville); Bottom Row: E. N. Robinson (Stewart-Warner); J. E. Bickel, AEA president, (Monroe Auto Equipment); G. P. Robers (Carter Carburetor); Charles J. Boland (Boland Ignition, Cleveland, Ohio).



Movie cameras grind above on the set of Dow Chemical's new Jobber movie. The film tells the story of Dow's new automotive cooling system fluid, "Dowgard." Movie was shot recently at firm's headquarters in Midland, Mich.



This pretty young maid holds vinyl swim tube which is available with the Spring sales campaign promotion kit from AC Spark Plug Division of General Motors. A swim tube is included in each window trim kit for AC Spark Plugs.

Pacific Automotive Show Reports Fine Attendance

Another outstanding Automotive Trade Show, the 12th Pacific Automotive Show held in Denver, Colorado—March 10, 11, 12, 13, 1960—has been concluded.

Over 300 Manufacturers had the opportunity of displaying their merchandise to the Automotive Wholesalers and the Automotive Trades in the 13 Western States, Western Canada and Mexico. The newest products were displayed and the latest sales plans of manufacturers were unfolded.

In cooperation with Pacific Automotive Show the Automotive Service Industry Association—ASIA—sponsored a pre-show conference highlighted by top business and professional-key speakers in our industry. The conference was attended by a capacity audience. The Denver City Auditorium consisting of the Main Area Level, the Stage Area, the Lower Level and the Balcony Area—a total of over 460 booths—was completely occupied by many of the leading manufacturers of Automotive Products.

The region served by Pacific Automotive Show was well represented by over 650 Sponsoring Wholesalers, representing nearly 2000 Store Outlets plus Guest Wholesalers from the area outside of the 13 Western States, Western Canada and Mexico.

In addition to the wholesaler attendance, attendance of the automotive trades people consisting of garages, service stations, car dealers, fleets, vocational schools—and others—was extremely heavy from the Denver Area and from adjoining states.

The Annual P.A.S. Booster Kick-Off Banquet, which was held on the evening of March 9th at the Shirley Savoy Hotel, was a sellout. Facilities for handling the crowd had to be expanded.

The Annual Meeting of the Board of Directors of P.A.S. was held on March 8th at the Cosmopolitan Hotel in Denver. Officers for 1961 were elected.

New President

J. K. Wilkinson of Pomona Motor Parts, Pomona, California was elected President of the 1961 Pacific Automotive Show. It will be held in Los Angeles in the New Sports Arena of the Los Angeles Coliseum on February 16, 17, 18, 19, 1961. Wilkinson served as First Vice President of the 1960 P.A.S. in Denver.

Elected as First Vice President for 1961 was Harold Littrell, of Littrell Parts, Inc. Medford, Ore. William D. Henderson of Henderson Brothers, Sacramento, Calif. was elected Second Vice-President.

Pacific Automotive Show Officers re-elected were Andrew D. Shaw, Manufacturers' Representative of Los Angeles as Treasurer and S. B. Sturtevant of Sturtevant Motor Parts Company, Van Nuys, Calif. as Secretary.

J. Leonard Gibson will continue as Executive Manager of the Show and Mrs. Bess D. Rock as Office Manager.



Apr. 16-24—International Auto Show, Coliseum, New York, N.Y.

Apr. 23—Automotive Wholesalers of Okla. annual convention, Skirvin Hotel, Oklahoma City, Okla.

May 1-3—Michigan Automotive Wholesalers Assn. annual convention, Pantlind Hotel, Grand Rapids, Mich.

May 8-10—Fort Worth Auto Show, Will Rogers Exhibit Bldg., Fort Worth.

May 8-13—ASIA management institute, University of Illinois, Champaign.

May 20-22—New Jersey Automotive Jobbers Assn. convention, Colony Motel, Atlantic City, N.J.

May 22-27—ASIA sales management institute, Syracuse (N.Y.) University.

June 20-22—Pennsylvania Automotive Assn., Bedford Springs Hotel, Bedford, Pa.

July 7-9—Independent Garage Owners of America 5th annual convention, Dinkler Plaza Hotel, Atlanta, Ga.

July 21-23—Automotive Trade Assn. Managers summer meeting, Grand Hotel, Mackinac Island, Mich.

Oct. 10-12—Automotive Parts Rebuilders Assn. annual convention, Conrad Hilton Hotel, Chicago.

Oct. 15-23—National Automobile Show, Cobo Hall, Detroit.

Oct. 16-21—American Trucking Assn. annual convention, Waldorf Astoria Hotel, N.Y.C.

Oct. 23-25—New Jersey Automotive Trade Assn., Chalfonte-Haddon Hall, Atlantic City, N.J.

Oct 28-Nov. 2—Florida Automobile Dealers Assn., Cruise to Montego Bay and Port-au-Prince.

THE

PULSE

OF

AUTOMOTIVE

BUSINESS

Automotive Wholesaler's Sales Inventories

Data from Bureau of Census, Dept. of Commerce

Region	Per Cent Change In			
	Sales	Inventories		
	Jan. 1960 from Jan. 1959	Jan. 1960 from Dec. 1959	Jan. 1960 from Jan. 1959	Jan. 1960 from Dec. 1959
New England	+15	0	+34	+7
Middle Atlantic	+7	-4	-2	-1
East North Central	+18	-9	...	-3
West North Central	-15	+3	+1	0
South Atlantic	+7	+6	+4	-1
East South Central	-7	+5	+10	+4
West South Central	-5	-5	+12	+1
Mountain	-2	-10	+5	+1
Pacific	-2	-5	+8	0
United States	+7	-1	+4	0

Monthly Sales of Automotive Jobbers incl., Tire and Tube Wholesalers*

All Data are in Millions of Dollars

Month	1960	1959	Per Cent Change
Jan.	\$376	\$332	+6.62
Feb.	...	333	...
Mar.	...	375	...
Apr.	...	392	...
May	...	399	...
June	...	429	...
July	...	418	...
Aug.	...	405	...
Sept.	...	428	...
Oct.	...	439	...
Nov.	...	429	...
Dec.	...	379	...
Total—Year	\$4,768	...	...

Monthly Sales of Franchised Car Dealers*

All Data are in Millions of Dollars

Month	1960	1959	Per Cent Change
Jan.	\$2,566	\$2,531	+1.38
Feb.	...	2,450	...
Mar.	...	2,821	...
Apr.	...	2,842	...
May	...	3,033	...
June	...	3,198	...
July	...	2,803	...
Aug.	...	2,772	...
Sept.	...	2,305	...
Oct.	...	2,956	...
Nov.	...	2,284	...
Dec.	...	2,177	...
Total—Year	\$32,472	...	...

Monthly Sales of Gasoline Service Stations*

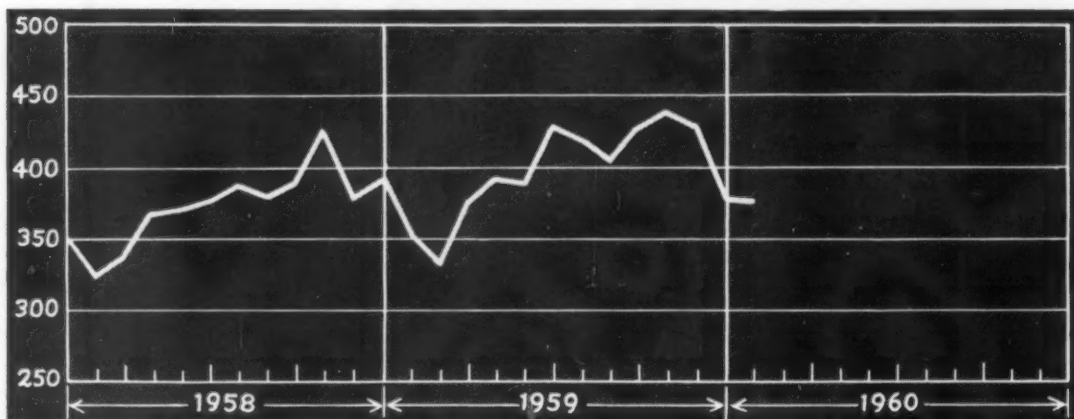
All Data are in Millions of Dollars

Month	1960	1959	Per Cent Change
Jan.	\$1,354	\$1,282	+5.62
Feb.	...	1,197	...
Mar.	...	1,318	...
Apr.	...	1,348	...
May	...	1,427	...
June	...	1,450	...
July	...	1,516	...
Aug.	...	1,504	...
Sept.	...	1,419	...
Oct.	...	1,462	...
Nov.	...	1,433	...
Dec.	...	1,437	...
Total—Year	\$16,793	...	...

* Estimated by the Bureau of the Census.

MONTHLY SALES OF AUTOMOTIVE WHOLESALERS INCLUDING TIRE AND TUBE WHOLESALERS

(All Data in Millions of Dollars)



NEWSETTES

Continued from page III

Name Change Voted

Shareholders of The Dayton Rubber Company granted permission to change the corporate name to The Dayco Corporation. More than 85 per cent of those voting were in favor. Shareholders also approved a stock option plan by 82 per cent. The plan is limited to five years and 50,000 shares.

The re-election of three members of the board of directors was also unanimously approved by shareholders. The directors elected for three year terms, include C. M. Christie, president; H. H. Kahn and B. G. McCloud, Jr.

Two Novi Executives Predict Banner Year

The year 1960 should prove to be a banner year in the sale of automotive air conditioners report Ross O. Bartlett and Gerard A. Hebert of Novi Sales and Service Co., Inc.

Bartlett is sales manager for Novi, joining the firm in 1955 as branch manager in Detroit. In 1956 he was named a district manager and in 1957 was appointed to his present post.



Bartlett

Hebert

Hebert is chief engineer at Novi. Under his supervision the Novi magnetic clutch was developed. Hebert began his automotive experience in 1939 when he ran his own gauge manufacturing plant.



Handing a packet of goldfish to R. I. Mitchell (right), automotive distributor in Maine, is William B. Banta, sales manager for American Hammered, Automotive Replacement division, Sealed Power Corp. The goldfish, sealed in plastic containers, were distributed during the recent ASI show to point up American Hammered's ad theme: "Fishing for Oil Control?"

PAWA Taking Shape

The Pennsylvania Automotive Wholesalers Association, a newly formed association, held a third meeting of its temporary board in Pittsburgh. It planned a dues structure, services and a future meeting with Philadelphia area representatives.

Bylaws were recommended with an annual dues section calling for payments based on the number of employees. Provision was made for admission of warehouse distributors for \$100.00 and for manufacturers agents or representatives at \$50.00, as associated non-voting members. Dues are payable in installments on a quarterly basis for a slight carrying charge. More information on this group can be obtained by writing: Adrian Carter, temporary secretary, Guttman Supply Co., Speers Rd., Belle Vernon, Pa.

INSTITUTE

Continued from page I

and inventory control methods . . .
realize profits through use of quality materials to do quality work . . .
emphasize craftsmanship . . .
recognize the sales value of quality work . . .
properly advertise and promote quality-vs-price . . .
properly figure costs . . .
educate insurance adjusters go after jobs—and get repeat business.

Before the annual meeting adjourned, O'Doherty was re-named chairman of the executive committee. A. S. Boehm (Black and Decker Manufacturing Co.) was re-elected treasurer.

Members of the ARI, in addition to those already named, include: Bear Manufacturing Co., Rock Island, N.Y.; Binks Manufacturing Co., Chicago; Klean-Strip Co., Memphis; Martin-Senour Co., Chicago; Neilson Chemical Co., Detroit; Park Chemical Co., Detroit; H. K. Porter, Inc., Somerville, Mass.; Rinshed-Mason Co., Detroit; Sherwin Williams Co., Cleveland; Speedway Manufacturing Co., Cicero, Ill.; Official Products Co., Atlanta; and Halfpenny & Haunn, Chicago.

AC Sales Conference

The AC Spark Plug Division National Sales Conference was held in Flint recently. The company's entire replacement sales field force, including regional, zone and territory managers, dealer merchandisers, national accounts representatives and office managers were invited for the three day conference.

Continued on page VI

NEWSETTES

Continued from page V

MEMA to Hold April Seminar

Wide interest in markets abroad was indicated in a recent vote among both member and non-member manufacturers according to the Motor and Equipment Manufacturers Association.

Also indicated in the vote was the preference for the proposed seminar on exports for April. MEMA accordingly has set up the seminar for April 27 and 28.

Those interested in attending the seminar should contact F. J. Lanning, MEMA headquarters at 250 West 57th St., New York 19, N.Y. (Telephone number is COLUMBUS 5-4544).

Officers Elected

Herbert W. Gledhill, Jr., vice president and general sales manager of Shepard Niles Crane & Hoist Corporation was elected president of the Hoist Manufacturers Association, Inc. at its recent annual meeting held in the Sheraton-Cleveland Hotel.

Other officers elected were: vice president, Fred E. Rau, hoist sales manager, Yale & Towne Manufacturing Co., Phila.; director, Weldon C. Miles, president, American Engineering Company, Phila.; director, Raymond A. Davies, sales manager, Chester Hoist Division, Lisbon, Ohio; director, Edward J. Byrne, sales manager, Chisholm-Moore Hoist Division, Tonawanda, N.Y.; Joe H. Peritz was re-elected executive secretary and treasurer and C. M. Dinkins, general counsel.

Cowhig Elected

David A. Cowhig, executive vice president of Wilkening Manufacturing Co., maker of Pedrick piston rings has been elected president of the Metal Manufacturers Association of Philadelphia.



Joseph M. Maleno of New Britain, second from right, Tidewater dealer, and John E. Anderson of Simsbury, Tidewater service station merchandiser, first from right, each won a home air conditioner in the national Prizarama Sweepstakes sponsored by Purolator Products, Inc. Henry J. Erd, New England district manager for Purolator made the awards. Robert G. Dow, division TBA supervisor for Tidewater Oil Co. looks on.

APRA Board Approves Compensation Plan

Meeting in Nashville, Tenn., last month, the Board of Directors of the Automotive Parts Rebuilders Association approved a plan for Workmen's Compensation Insurance. The Board noted that the plan should afford substantial savings for Rebuilders in 31 states and the District of Columbia.

The Workmen's Compensation plan will be managed by Bruce Dodson & Co. of Kansas City, Mo. It was explained in detail to the board; it was approved unanimously.

In other business, the board designated the following sites for the annual APRA Trade Show and Convention: 1961, Los Angeles; 1962, Washington, D.C., and 1963, Denver, Colo.

The 1960 show is set for Oct. 10, 11 and 12 at the Hotel Conrad Hilton in Chicago. Plans for the 1960 Convention include a program with speakers of national reputation. Senator Homer E. Capehart of Indiana will head the list of distinguished speakers.

Others, prominent in the fields of Employee and Labor Relations, will provide addresses of interest to owners and top management of Rebuilders and Suppliers alike.

The Board studied for two full days plans for the association to better serve its membership. A broad outline of objectives to be attained during the next five to ten years was surveyed. Immediate approval was given for several studies to broaden services and benefits within the next few months. The APRA Chicago Headquarters office again is being enlarged and approval for new equipment was voted.

The J. W. Boulton, Sr. Memorial Award to three Rebuilders for their outstanding contributions to the industry, and the W. Roy Weldon Memorial Award for outstanding contributions by a member of both the national trade press and the regional trade press will be made at the annual Convention in Chicago in October.

A.S.I.A. Executive Courses

Enrollments have begun for the Management Course, May 8-13, co-sponsored by A.S.I.A. and the University of Illinois. Cost is \$130 all-inclusive of room, board and tuition. Registrations for the two A.S.I.A. Sales Management Institutes are also being accepted. Cost will be \$155 for either the Eastern or Western area course. San Diego (Calif.) State College held lead-off program April 10-15. Syracuse (N.Y.) University will host the second Course May 22-27.

Problems of Small Firms

Senate Small Business Committee plans a hard look this year at problems of small firms. Among other things, the subcommittees will study dual distribution of tires and petroleum products. Also set for investigation are: impact of long strikes in industry on small firms; tax problems, including equipment depreciation; and the burden of government paper work in small firms.

Foreign Wage Competition

Congress is eyeing legislation to offset low wage advantages foreign competitors have over American industry. Sen. Keating, R., N.Y., is sponsoring the "Fair Labor Standards Trade Act of 1960." The bill would permit the president to raise tariff duties as much as 100 per cent if it was found foreign wage competition is injuring domestic industries.

Safety Service Clinic

The Kentucky Automotive Wholesalers Association announced that a safety clinic for garage men and automotive service outlets was sponsored recently by D & W Auto Parts of Mayfield and Murray, Kentucky and Metropolis, Illinois. The meeting was attended by

over 100 service men. Joe B. Wright of D & W Auto Parts has set as a goal, increased driving safety and fewer vehicle accidents caused by faulty equipment. The program was presented by Allen Anchors, sales representative and John Black, safety engineer for the Wagner Electric Corporation.

Using the theme "You Can't Guarantee Work You Don't Do," the repairmen were charged with the responsibility of insuring that equipment passing through their shops is safe in all respects. Points stressed were: proper installations, use of quality merchandise and equipment, proper maintenance based on adequate training in the systematic use of check-off lists to insure thoroughness in checking all equipment.

OAWA Elects Officers

The third annual convention of Ohio Automotive Wholesalers Association elected the following officers for the term 1960-1961. They are: President, Bill Hedges, Bill Hedges Auto Parts; vice president, G. Earl Koch, Auto & Aero Supply Co., Inc.;



Shown above: Donald E. Noble, president of Rubbermaid, Incorporated and a company secretary share her box of chocolates. The occasion? Company celebrated February—its sweetest sales month in history—by giving each of its nearly 800 Wooster, Ohio employees a box of chocolates. Candy was also sent to company's salesmen from coast-to-coast. Gross sales in February were \$2,600,00 compared to \$2,300,000 for August, 1959, the previous record month.

treasurer, Henry Fickes, Reliable Auto Parts; secretary, Dwight M. Hazeltine, Buckeye Auto Parts.



Interior view of ARO Equipment Corporation's lubricating equipment mobile display van. The van travels through the U.S. demonstrating tools and equipment for lubricating service.

Motor Age's

WHO'S WHO

Richard H. Wheeler has been appointed vice president-manufacturing of Warner Automotive Division of Borg-Warner Corporation.

Ira A. Lutz has been elected senior vice president, treasurer and chief operating officer of Clinton Engines Corporation. **Cletus V. Erlacher, Jr.** has been elected vice president and general sales manager.



Edward E. Frick was recently named general manager of Micro-Lube of Dallas, Texas. He is a native of Wawaka, Ind. and was formerly associated with Bemis Bro. Bag Co. of Chicago.



Harvey C. Hopkins has been elected vice president for purchasing of ACF Industries, Incorporated. He will coordinate purchasing activities of the company's operating divisions.



Jack F. Whitaker, photo, has been elected president of The Whitaker Cable Corporation. He succeeds his father, **Jack P. Whitaker**, who has been elected chairman of the board of directors.

Dr. Russell D. O'Neal has been elected vice president in charge of engineering for the Bendix Aviation Corporation. **Roy J. Sandstrom** was appointed general manager and **L. B. Young** has been named assistant general manager of the Corporation.

Fred H. Harris has been elected to the new position of vice president in charge of specialty sales for Gould-National Batteries, Inc.

The American Brakeblok Division of American Brake Shoe Company has appointed



John D. Hinton, left and **Rudolph F. Utermohlen** as vice presidents of the company. Hinton will be in charge of original equipment friction material sales. Utermohlen will be in charge of replacement sales of friction materials.

Glenn N. Keim, AC Spark Plug Division, Los Angeles replacement sales, has been appointed director of product services.

G. C. Cusack, executive vice president of Pure Carbonic Company, a division of Air Reduction Company, Inc., has been named president of the division.



Arthur J. Welch, photo, has been elected president and general manager of the Spring Division of Borg-Warner Corporation. He succeeds **Harry P. Troendly** who was advanced to chairman of the Division's supervisory board.

Charles W. Dunn has been appointed market development manager for the Mohawk Rubber Company.

C. E. Raymond has been promoted to sales supervisor, Lemeco Products, Automotive Equipment Division and **Jack Evans** has been promoted to vice president in charge of sales, Lemeco Automotive, Inc., Tractor Division.



Lester L. Swartz has been appointed advertising manager of the Bendix Aviation Corporation. In addition to his new duties he will also continue as supervisor of Exhibit and Show Operations for the Corporation.



Charles K. Aaron has been named assistant to the president of Wayne Pump Canada, Ltd., Toronto. He will assist in managing the Canadian operation. He was formerly assistant export manager for the company.



Robert S. Conybeare has been named recently to the newly-created position of director of product planning and market development for Rubbermaid, Incorporated.

MOTOR AGE

newscoop

*AMA Lists Cars For 1961
Unitized Body For Ford?
Horsepower Race Halted?
Small Ford Trucks Due
1976 Car Forecast
Russian Car Imports
T-Bird To Get New Look*

In the days ahead HERE'S WHAT TO LOOK FOR!!!

(Items gathered and edited by Ed Janicki, Bill Montgomery and Neil Regeimbal)

AMA Roster Lists Cars For 1961

REPORTS OVER PAST FEW MONTHS that this or that car will be dropped in 1961 are punched full of holes by a recent announcement from the Automobile Manufacturers Assn.... In its roster of American-made passenger cars which will be displayed at the National Automobile Show in October, AMA lists all current brands in production. In fact, there'll be one new entry in show.... That'll be Checker Cab's Superba.



Ford May Adopt Unitized Body

FORD REPORTEDLY WILL BE NEXT MAKE to come out with a unitized body on standard passenger cars.... It'll be a two-piece design similar to Chrysler's.... GM also is showing considerable interest in unitized body design. Sources note that General Motors is buying competitive cars and consigning them to GM proving grounds for study.

Old Names Reappear On New Cars

INTERESTING TO NOTE HOW CERTAIN NAMES of yesteryear reappear on the automotive scene.... Did you know, for example, that name Comet has appeared on at least 18 vehicles since 1913?

Most of the vehicles bearing Comet name had a short life.... Last surviving ancestor died in 1947.... Only one year after it was brought out... 12 newcomers were announced that same year.... Only Kaiser-Frazer was in production at year-end.

Ford And Chevy Battling For First

WHO'S IN FIRST? Both Ford and Chevrolet are.... Depends on what you're talking about.... It gets more confusing each year. For calendar year sales, Chevrolet took the jump on Ford.... It reported a sales lead of 40,000 units over Ford for the first two months of this year.

Ford wasted no time in countering Chevy's claim.... One day after Chevrolet announced its figures, Ford issued a release saying it was 96,000 units ahead of Chevrolet in "model year" sales.... Figures include all cars made by both divisions.

Horsepower Race Called Off

THE GALLOPING HORSES HAVE COME TO A SCREECHING HALT.... They barely got away from starting gate when the race was called off.... Chevrolet, Ford and Valiant have decided not to promote their recently announced higher horsepower engines for their smaller cars.

That does not mean, however, that the hotter engines are being dropped.... All companies will continue to make them available.... But only on special requests.... Ford will continue to offer two optional engines for the Falcon, a 100-hp. job and a 128-hp. version; Corvair, its optional 95-hp. engine; and Valiant, a power pack that soups up to 148-hp.



Accidents Are One Of The Biggest Killers

ACCIDENTS STILL KILL AT THE RATE of better than 21 persons for each 100,000 people in country.... U.S. Public Health Service says 36,981 motor-vehicle deaths in 1958 is a slightly better rate when compared with total population than preceding year. But it still ranks as one of the biggest death causes, along with heart diseases, cancer, and strokes.

Pills Cause Many Driving Accidents

PILLS ARE CAUSING HIGHWAY SAFETY experts some real headaches.... Justice Department and other officials are still trying to stamp out illegal sales of amphetamine sulfate tablets to motorists and drivers.

They can cause dangerous errors in judgement and sometimes hallucinations.... Ordinary cold pills in some cases produce extreme drowsiness, causing drivers to lose control of their vehicles, causing many accidents during cold season.

Small Ford Trucks Due In June

FIRST PROTOTYPES OF SMALL FORD TRUCKS start coming off assembly line in June.... They're really small.... They have wheelbase measuring only 90 inches, or about five inches shorter than Volkswagen Microbus.

There will be three body types—a pickup, styled after Falcon passenger car, a van and a bus.... All will use Falcon's 144 cubic inch engine.... Trucks will officially be introduced at the end of year.



One Out Of Ten Cars Sold Is An Import

IMPORTED CARS LAST YEAR ACCOUNTED for almost one out of every 10 sold in this country.... U.S. Commerce Department reports imported car sales totaled about half a million.... American firms produced almost 5.6 million.

But more of the imports are made by U.S. firms based overseas.... Such firms now account for 28 per cent of all imported cars.... Department predicts U.S. compact car production may reduce the number of imports sold here in future years.

Medium-Priced Cars Holding Their Own

CONTRARY TO EARLIER FORECASTS, sales of medium-priced cars are holding their own.... Some trade sources see no appreciable drop in market penetration by these makes in 1960.... One source estimates their percentage share will decrease only two points from last year's 25 per cent.

Same source estimates that so-called low-priced group will be hit hardest by new compact cars.... Their share dropping from 52 per cent to 42 per cent.... As far as his division is concerned, Ed Cole, Chevrolet's general manager, disagrees with those predictions.... He notes that sales of the Corvair have represented "new business for Chevrolet" and it has not been "feeding off our regular line."

Front-Engine For Corvair?

THERE'S A LUKEWARM RUMOR in Detroit that Chevrolet is working on a front-engine Corvair for introduction in 1961.... Chevrolet spokesmen do not deny or confirm rumbling.... Their only comment: "Interesting."

1976 Forecast Calls for 114 Million Vehicles

THERE'LL be 114 MILLION MOTOR VEHICLES traveling the highways—and needing repairs, service, fuel—by 1976.... U.S. Bureau of Public Roads makes prediction for rise from the present 70 million.

With more cars and drivers, mileage by 1976 will zoom to 1.2 trillion, compared to 696 billion last year.... Gasoline consumption will jump to 97 billion gallons, from present 55 billion.. 1976 forecast calls for 95 million cars, 18.6 million trucks and buses.



Neil Regimbal
MOTOR AGE's News
Editor, Wash., D.C.

Turmoil Over Russian Car Imports

RUSSIAN CARS BEGINNING TO TOUCH OFF controversy even before they've been shipped to this country.... At request of Sen. Vance Hartke, D., Ind., U.S. customs officials are checking whether Red cars will be "dumped" in this country—sold at prices less than they're sold in Russia and at a price which will hurt U.S. competitors.

He believes they will be.... They'll sell here for about \$1500.... Compared with \$2500 at tourists rate of exchange or \$6250 at official rate of exchange in Russia.

Tax Evaders Beware!

GOVERNMENT CRACKDOWN ON TAX EVASION is going full speed ahead.... Billions of dollars in lost taxes are at stake.... Government is losing \$10 billion a year from one source alone—unreported income of independent businessmen.

Experts say self-employed report only 75 cents out of every dollar earned.... An IRS drive is on against taxpayers who skip interest and dividend payments in filing returns.... This costs government \$1 billion a year.

Safety Value Of Seat Belts Proven

SEAT BELTS WILL BE STANDARD EQUIPMENT in only a few years, government health experts predict.... But they admit it'll take longer to get people to use them.... A. L. Chapman, Assistant Surgeon General, says safety value of the belts has been proven by government studies.

Move Toward Forced Mass Transportation

PROPOSAL IN A NEW U.S. COMMERCE DEPARTMENT transportation policy study for communities to consider putting tolls on urban gateway roads to encourage use of mass transportation is stirring a storm of protest.

Study also suggests higher parking tolls to force use of mass transportation instead of cars... Opponents say this would disrupt normal transportation balance and harm large segments of U.S. business.



T-Bird To Get New Look

ONE OF BIGGEST STYLING CHANGES next year will come from Dearborn.... The Thunderbird, which for years has carried the same body lines, will get a new dress next year.... Those who have seen it say you won't recognize the new T-Bird.

Hydraulic Windshield Wiper System To Be Standard

AT LEAST ONE CAR COMPANY NEXT YEAR will offer a new hydraulic windshield wiper system as standard equipment.... All hydraulic units currently offered are an extra-cost option.... New wiper motor will be powered by hydraulic pump that operates car's power steering system.

Changes In Bridge Heights Slow Highway Building

GOVERNMENT BRIDGE GOOF HAS BOGGED DOWN highway building in at least 21 states.... That means added costs, no extra benefits.... Changes in some 8,000 bridges already built may cost another \$800 million for the already-struggling highway program.

Pentagon agreed to 14-foot minimum heights in 1956, then pondered three years before deciding it needs at least 16-foot clearances.



Lincoln And Buick Production Schedules Down

WHILE AUTO PRODUCERS STARTED CUTTING production schedules last month to balance inventories, overall picture didn't look that bad.... All auto makers, with exception of two, turned out more cars in first nine weeks of year than the like period last year.

The only two makers which turned out fewer cars than last year were Lincoln and Buick, according to AMA figures. All others showed either modest or significant gains.... Pacemaker Dodge continued to forge ahead with Dart, with total output more than triple that of last year.



"Circle of Safety" program builds customer goodwill. It also creates great demands for sales & service

Vehicle safety checks sell service

DID you know that less than half states require periodic motor vehicle inspection by law? As many as 50 per cent or more of all vehicles inspected in these states are rejected each year because one or more parts require immediate service attention. One out of five of more than 3,000,000 vehicles voluntarily safety-checked during 1959 were in need of safety service to one or more parts affecting safe driving. That is why periodic Safety-Checks are needed for ALL vehicles.

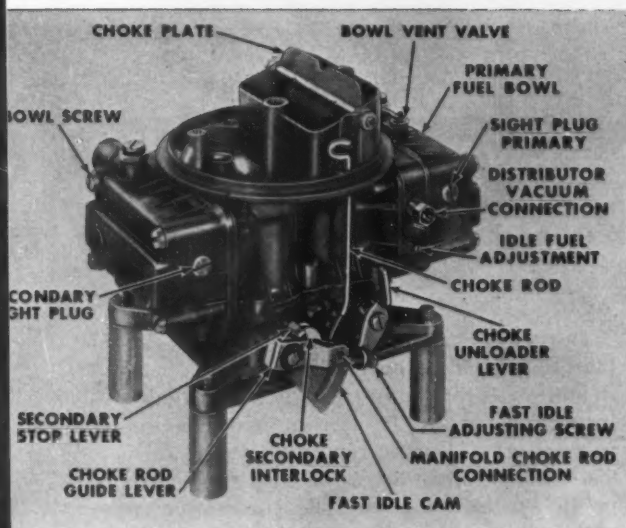
During May and June, the National Vehicle Safety-Check program will be conducted in states not requiring periodic motor vehicle inspection by law. It offers an excellent opportunity for dealers, repair shops and service stations to contact car owners through a public service program. The automotive repair industry can take an active part in the Vehicle Safety-Check program in two ways. First, by Safety-Checking vehicles, especially in areas where no community program is con-

Continued on page 136

Handbills and window displays can be used to promote the "Circle of Safety" safety-check



Service techniques on the new Holley CARBURETOR

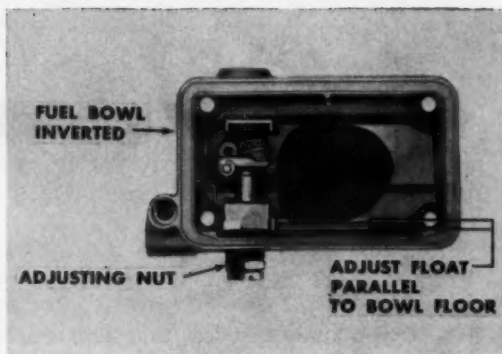


Above: Full view of Holly's new carburetor.

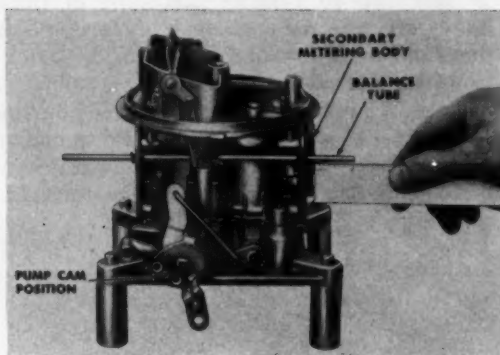
THE new Holley carburetor on the Dodge Polara has some outstanding differences from conventional carburetor practice. Simply though, the carburetor functions just like all others. It contains a fuel float level system, where the fuel enters the bowl through a fuel screen, past the inlet needle valve into the float bowl assembly. The fuel then entering fills the bowl, raising the float, which exerts a pressure against the needle valve to cut off the fuel at a predetermined level.

Externally-Adjusted Float Level

The outstanding feature of the new Holley is the externally-adjusted fuel or float level. The needle valve assembly slides up and down in the bowl casing, and is retained in position



Invert bowl. Set float parallel with bowl topside.

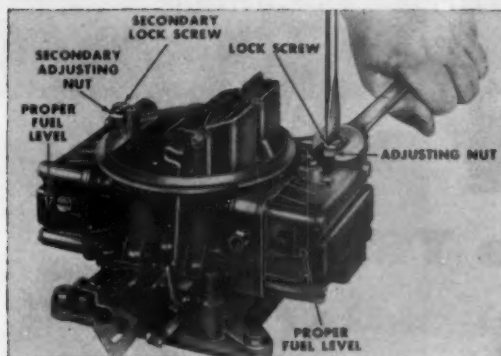


Set balance tube to extend 1" at secondary end.

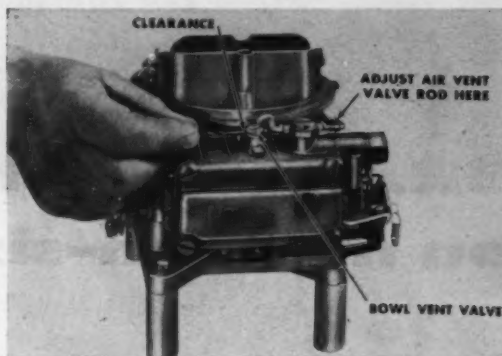
by a lock nut. It is adjusted by turning an adjusting screw. All calibrations within a carburetor are predetermined on a consistent fuel level.

If the bowl fuel level is not maintained, all other circuits will not function as designed. The float spring is utilized under the float in some applications to keep the float in a stabilized position. The float chamber is vented internally by the vent tube at all times. At curb idle, or when the engine is stopped the float

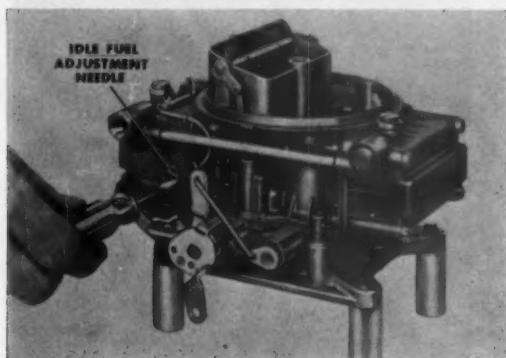
A discussion in depth of service steps to use when working on the unique idle air by-pass adjustment and externally-adjusted float level



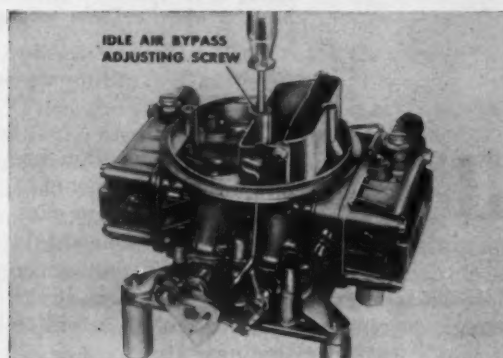
Raise or lower needle to control level at port.



Throttle closed tight; disc clearance: .050"-.070".



Seat idle needle lightly; back out one full turn.



Turn by-pass needle into seat lightly out 1 turn.

chamber is vented externally by a mechanically operated poppet valve, on the outside of the bowl.

It is opened to allow the escape of excess vapor pressure. This eliminates the possibility of percolation or gas boiling out of the main nozzles. This causes a hard starting complaint in hot weather.

The idle system of this carburetor utilizes two almost identical idle systems, two for each primary and secondary bores. Since the

two passages function identically, it will only be necessary to discuss one side. The fuel flows from the float chamber through the main jet, then into the small angular horizontal passage, that leads across to a vertical passage. Fuel flows up this vertical passage, past the idle feed restriction.

The fuel then flows through a short horizontal passage, and is mixed with air coming in from the idle air bleed. This fuel and air

Continued on page 128



BY JACK MONTGOMERY
Technical Editor

Are you getting your share of truck repairs? Offer truck brake service. It's easy and extremely profitable

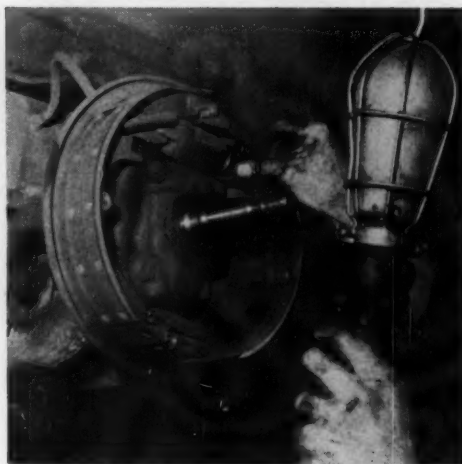
BRAKE SERVICE on medium-size TRUCKS

Pulling wheel cylinder boot back to check for wheel cylinder seepage.

RELINING brakes on medium-size trucks takes about the same amount of work as on the regular passenger cars. On any brake job the hydraulic system should be checked out very thoroughly, including the hoses, master cylinder, hydraulic lines, brake drums, springs, etc. Particular attention should be paid to cleanliness also.

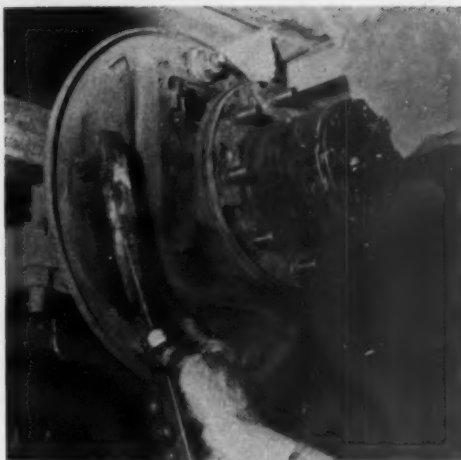
Clean all parts, except the lining and shoe assemblies, in a cleaning fluid. Remove dirt and corrosion from the brake carrier plate. Inspect all parts for distortion and excessive wear. Replace worn or distorted parts. Inspect drums for scoring or out of round. Recondition or replace drums if necessary. Carefully pull the edge of each boot away from the brake cylinder, and note whether the interior is wet with brake fluid.

Excessive brake fluid at this point, indicates leakage past the piston cups. Either replace or rebuild the brake cylinder as necessary. Check the operation of the adjusting screw. If the screw does not operate freely,



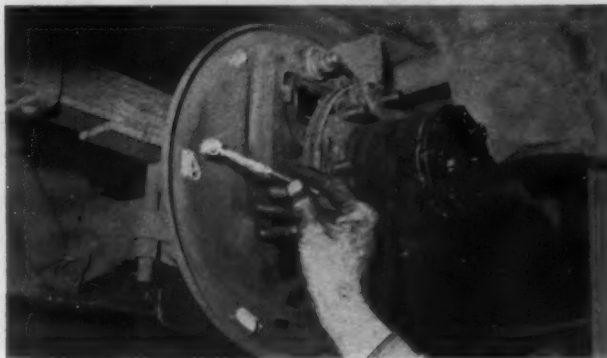
remove the screw from the socket nut, and clean all parts. Apply lubrication to the threaded end of the screw. Then install the screw and the nut in the socket.

Inspect the condition of the brake lining, if the lubricant appears on the brake linings or



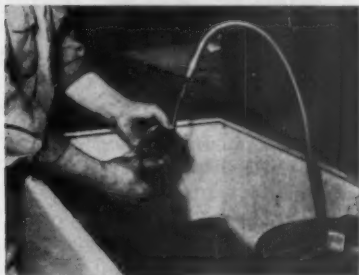
Backing plates are wire brushed & blown clean with compressed air.

Applying lubricant to backing plate area where the brake shoes contact.



Clean and lubricate star wheel and screw star wheel all the way in.

Clean the wheel cylinder thoroughly before honing and rebuilding.



it is worn excessively or close to rivet head, install new lining. Lining should be installed in full sets.

When assembling shoes to the backing plate, apply a lubricant to all moving contact points of the brake assembly.

Then one places the adjusting screw assembly between the brake shoes, with the wheel of the screw aligned with the slot in the brake carrier plate. Then install the adjusting screw spring.

Place the brake shoes on the carrier plate, with the primary shoe toward the front and upper ends of the brake shoes properly positioned on the anchor pin. Install the brake shoe hold-down pins, springs and cups. When installing the pull back springs, install the primary shoe retracting spring first.

To insure a good brake job, all service operations require that absolute cleanliness be observed. Any foreign matter in the system will tend to clog the lines. Also it will ruin the rubber cups of the wheel, and main cylinders, and cause inefficient operation, or even failure of the braking system. Dirt or grease on the brake lining will cause that brake to grab first on the brake application, and fade out on a heavy brake application.

Only the best quality brake fluids should be used. Use brake fluids that are satisfactory for any atmospheric temperature, hot or cold. Be sure it has all the qualities that are necessary for satisfactory operation, such as high boiling point to prevent evaporation and tendency to vaporlock and the ability to still re-

Continued on page 136



Terry Nance, owner of Larson's Garage believes in advertising shop's services.



Service truck of Larson's Garage carries an attention getting message to spark customer interest.

Promote sales and service with *Signs*

Attention getting signs always bring in new customers. Read how Terry Nance's sign paid



By WM. MONTGOMERY
News Editor



EVERYONE who travels past Larson's Garage in Denver, Colo., notices a sign which reads "Recommended By Terry Nance." Just who is Terry Nance? This question arouses curiosity in the passerby's mind. Finally the would-be customer ends up in this place of business when his car is in trouble.

"Terry" is none other than the owner and master mechanic. He is usually approached by the customer as to what authority he has for recommending. Then, Terry politely hands the customer his card. "The humor



This traffic-stopper sign was displayed at a garage in Philadelphia. It brought in customers for a battery checkup.



Signs should be brief, in bold type, easy-to-read and properly placed.

seems to lessen the tension of the car-troubled customer," relates Terry.

He is firmly convinced that it is aggressive merchandising—promoting his firm to the public with good humor. "Nothing is more irritating to a customer than to have trouble. Often a little humor and friendly treatment puts him at ease. And in this frame of mind you can discuss the repair problems much easier and gain his confidence. Many times he tells of other troubles which he would like to have us check."

Terry is not afraid of work and many times he is found working as hard as his two regular mechanics. Nor does he believe in price cutting to gain customers. A fair price permits you guarantee of an expert job, believes Terry. "The customer wants his assurance. Like yourself, when something is wrong you don't go to the cheapest doctor to see what is the matter—you go where you feel you will receive the best attention. A customer thinks the same way about his car—he has a sizeable investment in it and wants to protect it."

Skilled mechanics are hired on a salary basis. Terry feels they do better work if on a salary.

Terry is constantly on the lookout for additional qualified mechanics. He hopes through the IGO, of which he is a member, that their

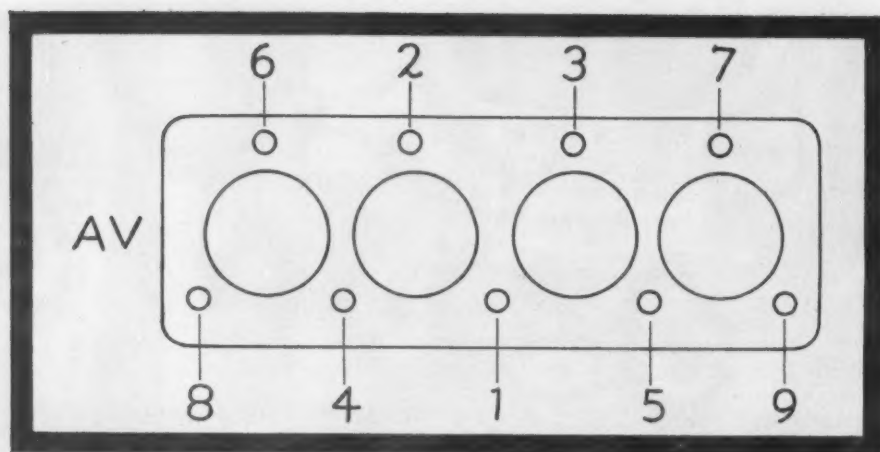


This dealer made customers among teenagers' and their parents by making his station the safety headquarters for the local teenage driving test.

registration program will aid in securing qualified men.

"I firmly feel our role as garagemen can only be held in high public esteem through qualified work—and there is no reason why we can't have a little good humor along with it," Terry states.

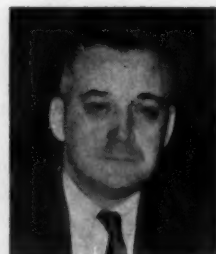
If you have a merchandising idea or a sales promotion that has proved successful send it in to Motor Age. \$7.50 will be paid for each merchandising idea or promotion accepted and used.



Correct sequence for tightening cylinder head bolts.

How to do **VALVE** work on a **SIMCA ENGINE**

Most imports have easy-to-work-on overhead valve design. Service is easy and profitable



By **TERENCE McCABE**
Service Editor

JUST like any internal combustion engine, the imports also require valve service. Quite frequently because of many being used as second cars, they will require valve service at closer intervals.

Slow speed operation and stop-go driving will help contribute to gum formations on the valve stems and guides. This gum formation will cause the valves to stick and burn. This condition results in an engine miss, or hard starting complaint. The loss of one cylinder becomes very critical on these small four cylinder imports.

Their owners become quite conscious of this condition because of the quick drop in engine power. Remember, dropping one cylin-

der on most imports means a 25 per cent loss of power.

Similarities Discussed

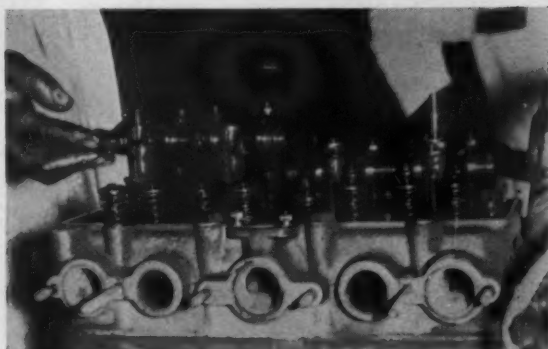
Valve service on the imports is just like valve service on any domestic unit. After all they are just simple four-stroke cycle, gasoline, internal combustion engines. Very few of them use hydraulic lifters, and almost predominantly they are of over-head valve design.

This design factor in itself makes for easier valve service. The cylinder head, rockers, valves, etc. are much smaller than that of conventional domestic engines. You don't have to be a Charles Atlas to lift the heads off the

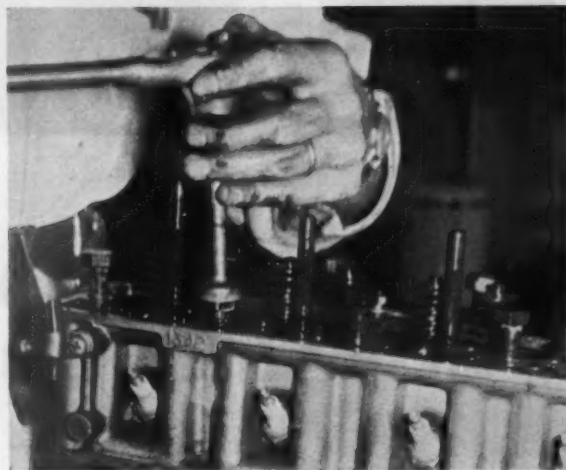


Valve adjustment is done in the conventional manner, with feeler gauge and adjusting tool.

Removing two piece rocker shaft assembly.



Removing cylinder head attaching bolts and washers.



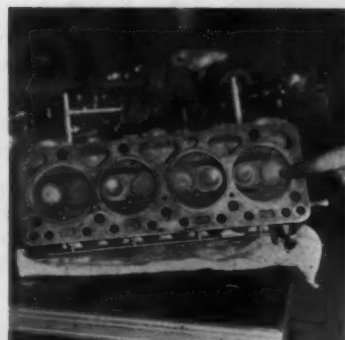
block. In most cases the heads will be made of aluminum. Any one can pick them up with one hand.

The tools required are the same as those you would use on domestic engines. Manufacturers of valve and seat dressing equipment supply pilots, stones, reamers and special tools for all the imports. Almost all of the British units produced after 1953 would have bolts and nuts of U.N.F. (unified national fine).

On these units American wrenches and tools can be used. However, all the automobiles produced on the main continent of Europe use metric size nuts, bolts, and wrenches. Complete sets of metric hand tools are available from any American tool supplier or your local jobber.

The Simca imported from Paris by Chrysler is one example of a continental car. This car uses metric sizes. The use of any other type of wrench but metric, will result in damage to the nuts and

Continued on page 138



Under side view of cylinder head showing valve and spark plug arrangement.

Chrysler Imperial marine engine develops 225 h.p. at 4000 rpm.



By FRANK P. TIGHE
Editor of MOTOR AGE



Don't miss the BOAT— It's a BOOMING BUSINESS

WHAT was at one time a "nuisance" service, a job to "please the customer at no profit," namely servicing outboard and inboard motors—may well be source of new income for many MOTOR AGE readers.

Recreational water craft sales and service reached a new high for 1959. Estimated sales for '59 was set at \$2.75 billion at retail level. It is forecast that the increase in 1960 will be in the neighborhood of from 15 to 32 per cent over 1959. The rapid growth of the recreational boating industry has been phenomenal.

A report issued jointly this year by the National Association of Engine and Boat Manufacturers, and the Outboard Boating Club of America indicate the following; in 1959, 7.8 million recreational craft were operating in American water ways. The total

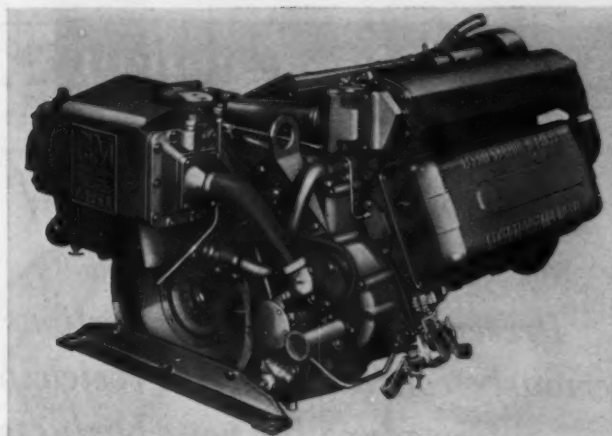
in 1958 was 7.33 million. The users of these recreational pleasure-craft increased from 37 million, in 1958, to 39 million in 1959.

The total of inboard engines, both gasoline and Diesel in use for 1959 was 804,000. This compared with 774,000 in service as of 1958. In 1959, a total of 30,800 inboard engines were produced. Gasoline engines accounted for 28,000, of these, while Diesel contributed 2,800. These figures do not include the private conversion of automotive type engine.

Many of these conversions have been accomplished by automotive machine shops, and by automotive independent servicemen. Automobile service and sales outlets have joined their marine cousins in the service and merchandising of recreational water-craft of all types.

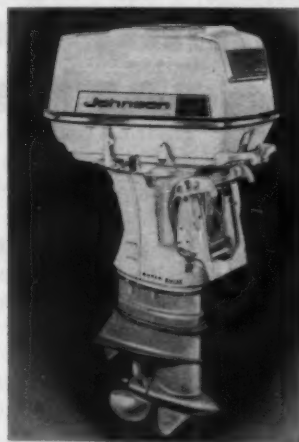


Evinrude's new 75-hp Starflite 11 outboard is a 4-cyl. V-type engine of 89.46 cu. in. displ.

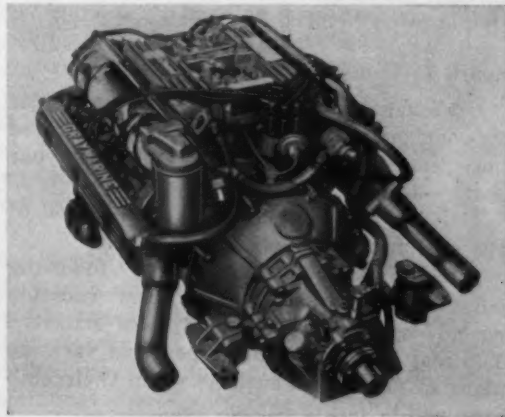


GM Diesel's Series 53 small-boat Diesel is a new inclined version of the 4-53 vertical engine introduced last year.

Johnson's Super Sea-Horse 75 outboard topped their '59 line.



Graymarine offers Fireball V-8 in five versions. The latest features a 4-barrel downdraft carb.



The recreational boating industry is growing. Servicing and repairing marine engines brings big profits

Automotive sales forces and service departments have the personnel to do an outstanding job. Automobile sales forces have been trained in the sales and financing of mobile equipment. They have years of good experience in handling retail sales.

This experience is of prime value in the merchandising of these water craft. Their service departments are fully equipped and staffed to handle any and all service on these engines and boats, when out of the water. Some are located in such a position as to offer dry dock and wharf services.

Several dealerships and service shops along the Ohio River, as an example and the Great Lakes are in a position to offer these services. In Philadelphia, as an example, a long-time Buick dealer is doing an outstanding mer-

Continued on page 140



By WM. H. WOLFE
Managing Editor

Presenting a series of questions to be seriously considered when reviewing vacation plans for your business staff

Planning employee VACATION policy

EVERYBODY loves a vacation. It can do you and your staff a world of good. You and they come back refreshed, ready to tackle the business problems in the months ahead.

Now of course policies will vary as to vacations depending on the size of your operation. You may not even have found it feasible to have a vacation policy of any extent. Nonetheless, the review of the following questions should prove helpful.

Spark to Consideration

Obviously all these questions will not bear on your particular operation. It is intended here that they spark consideration on your part:—as a member of management:

1. Have you set aside a certain period of the year for your vacation schedule?

2. Have you considered the following points in establishing the total vacation period? Service work loads? Sales activity? You may want to consider personal vacation plans of employees such as when their children will be out of school, etc.

Close Down for Vacation?

3. Have you considered the possibility of closing your business operations to give everyone the same vacation time? Generally, such a step requires advance notice for your customers. If you take the same period of time each year, say the last two weeks in July, they soon are schooled to it.

4. Have you anticipated the possibility some of the employees will want a split vacation? (Part of their vacation at one time and part at another). Obviously you don't want this policy in effect where you close down the shop or dealership for a specified two weeks.

5. Will you grant extra time off at no pay when employees request the additional vacation time? And how long can the extra time be? What rules have you set up to apply to this extra time?

6. Do you have a plan where an employee can allow his vacation time to accumulate? Here it is well to figure just how much time can accumulate. And whether the days accumulated can be carried over to the next year's vacation.



Some service establishments find it useful to close down operations for a week or two-week period in order that all personnel may be away at one time.

Figuring in Overtime Wages

7. How will the amount of vacation salary be determined? This means taking a look at overtime wages. Again, will layoffs be considered in arriving at the amount of the vacation salary? If you have any year-end bonus plans, these must be considered in determining the average weekly salary for employees.

8. Have you worked out a plan for figuring vacation pay for employees on an incentive plan? You will want to consider such items as: the average for the year; average for month preceding vacation, etc.

Holidays Falling During Vacation

9. Have you worked out a plan for handling the problem of holidays that fall during the vacation period? Extra time off is generally the case here. Some places though, allow extra pay on top of regular wages for holidays worked.

10. How will you handle an employee request for double pay for working instead of taking a vacation? This often comes up. The recommendation usually is to discourage

this request as the worker psychologically feels he had "no vacation." Then too, the change of pace benefit is absent.

The check list raised a lot of questions, didn't it? Now, you can see the desirability of some kind of written form or policy about vacations.

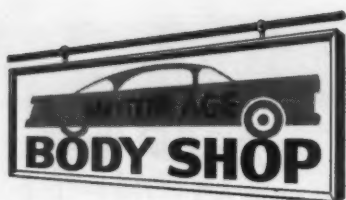
Spell it out in black and white so an employee knows just where he stands. Naturally, the time of hire is the time to go over the vacation policy with him.

Now for just a few more questions for the check list—

11. Have you set a minimum time an employee must work before he qualifies for vacation? Some firms have an employee accrue a day's vacation for every month worked after a certain cutoff date during the year hired.

For example, if John Doe were hired on the 1st of May, 1960—and the cutoff period was July 1, 1960, then he would accrue two days vacation. If the cutoff day were June 1, he would accrue one day vacation for 1960.

12. What about posting a vacation schedule? Plenty of advance time is the suggested rule here. Letting employees select their own time is generally done on a seniority basis. Also swapping of dates of one employee to another can lead to confusion. A firm rule about final time of selection is best here.



Hydraulic equipment aids UNITIZED BODY WORK

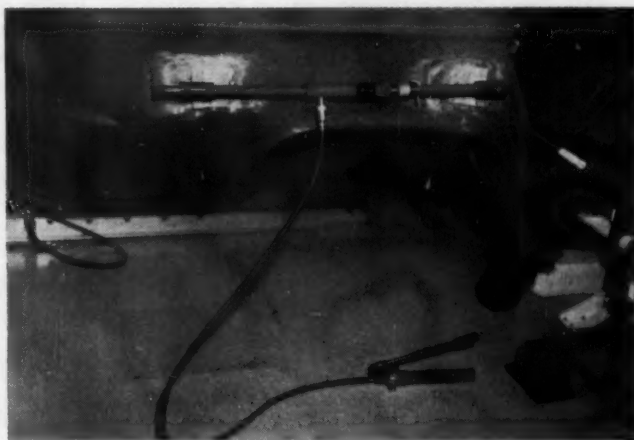


Showing hydraulic equipment arrangement to apply pressure to roof and upper right corner of windshield post.

THE unitized body is making its appearance on the American scene more and more. It has proven very popular with the buying public, as is evidenced by the climbing sales of the small American and imported cars.

What is the unitized body, and why has it gained in popularity? First off, the unitized body or as some imports call it "mono-weld" construction, is a rigid, welded reinforced metal shell. It supports the engine, drive train and wheel suspensions without the use of a conventional chassis frame. The body is

Tension being applied to Corvair fender hydraulically for removing a panel buckle in photo at right.



With the unitized body it is well to remember that body work and frame work no longer can be separate

used to support and align all other members that compose the completed unit. The major variations in structure from the conventional occur in the floor pan and methods of mounting and securing running gear parts.

Why Popularity Gain?

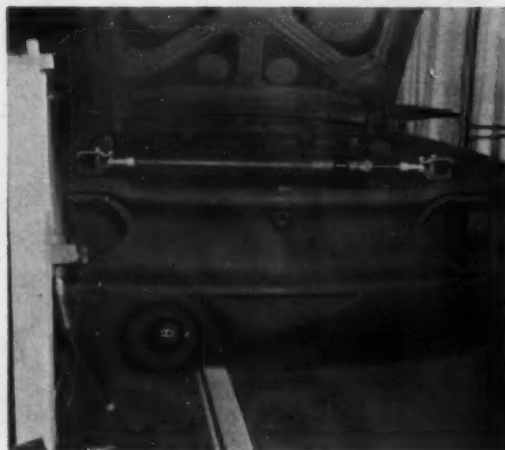
The unitized body shell has gained in popularity for at least four reasons:

1. Greater safety to passengers because of the staunchness of the body structure.
2. This type of construction produces rigid-

Continued on page 152



View of hook-up for applying pressure in relieving panel strain is shown directly above.



Above: Rear view of car showing hydraulic pull to align deck opening.



Method of attaching plate adapter to roof of vehicle by soldering to clean surface.

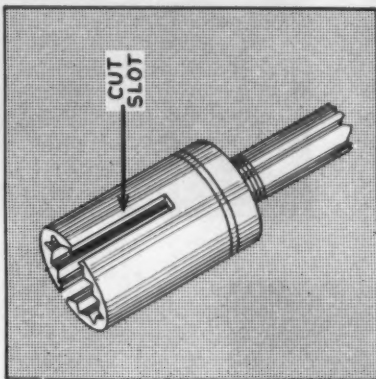
\$7.50 CAR and TRUCK PAID FOR EACH KINK USED shop kinks

52

Altered Socket Loosens Thumbscrew-Type Hose Clamps

Harvey Muller, Box 6, Danboro, Pa.

49. I have a very good shop kink which will be helpful to the mechanic. For fast loosening or tightening of thumbscrew-type hose clamps, use an altered socket. To make this simply saw a $\frac{3}{16}$ in. wide slot down the middle for $\frac{3}{4}$ of an inch in a $\frac{3}{8}$ in. socket. A larger socket can be similarly altered to remove petcocks.



Replacing Windshield Wiper Drive Cables in 1957 Fords

Charles Erwin Cohn, 7720 Marquette Ave., Chicago 49, Ill.

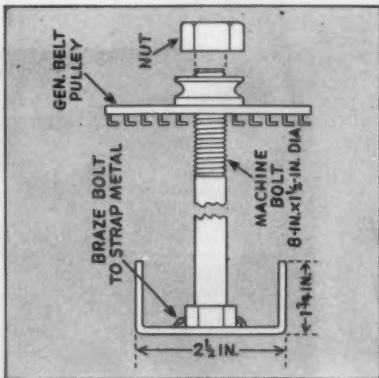
50. Replacing the windshield wiper drive cables in a 1957 Ford is complicated by the difficulty of hooking the cables to the motor drive pulley, especially for the right-hand cable. The job can be greatly simplified by paying attention to the following points. It is very helpful to remove the wiper motor and both wiper arms. This allows more freedom of movement, allowing the cable clips on the drive pulley to be brought

into a more accessible position. The righthand cable which goes underneath the pulley is very difficult to put in place when the lefthand cable is not disturbed because there is no room for your hands under the pulley. The trick is to work a piece of string around the bottom lefthand cable, from front to back. Tie the end of the string to the cable end and use the string to fish the end around the pulley.

Inexpensive Spring Compressor For Automatics

C. Lambert, 4100 Bonisteel Drive, Ft. Huron, Mich.

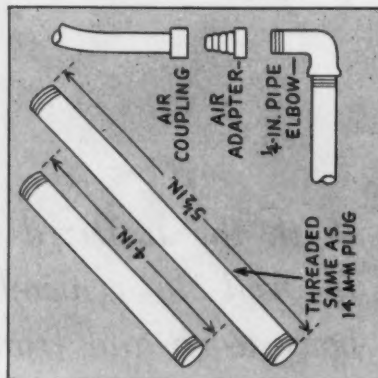
50. We made a tool for compressing the spring in the clutch so that lock ring may be removed and installed easily. To make it we use a $\frac{1}{2}$ inch machine bolt 8 inches long, a piece of strap metal 6x1x18 inches and an old generator pulley. Use a $\frac{1}{2}$ inch die and thread the $\frac{1}{2}$ inch bolt for half its length thru. Bend a piece of strap metal as shown.

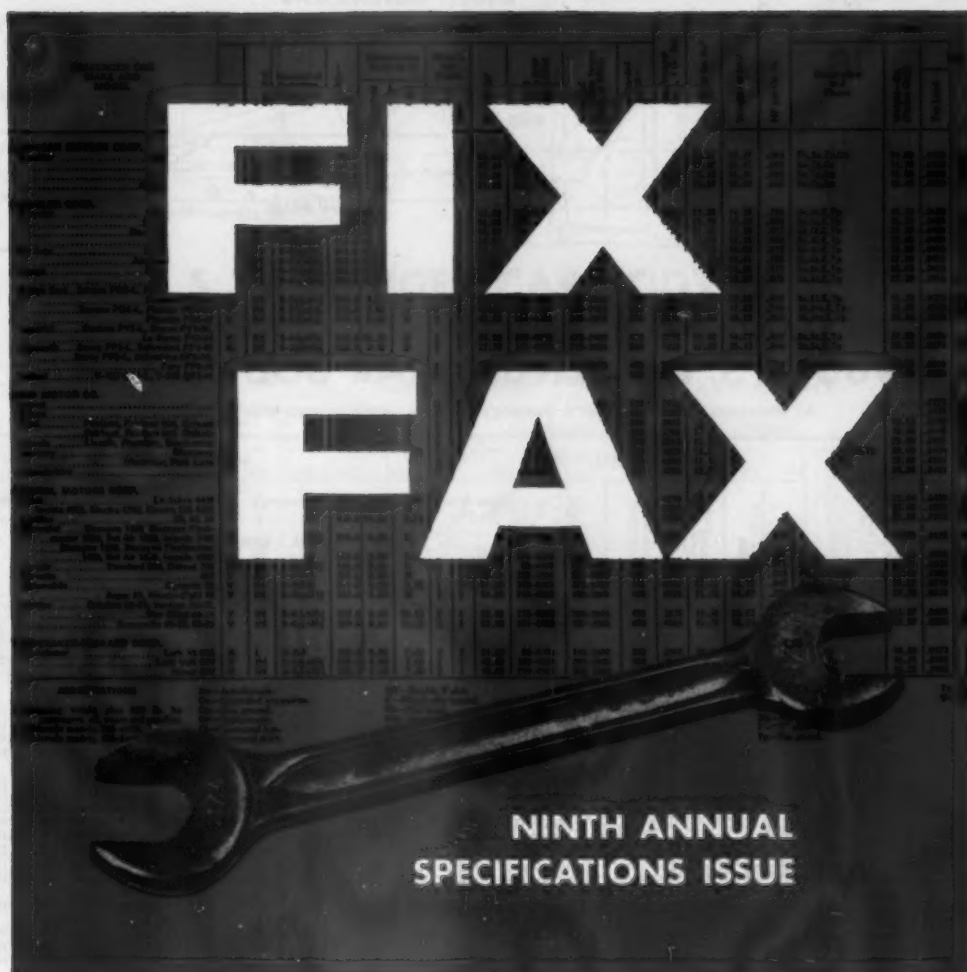


Tools For Easy Installation of Valve Seals

Larry Iacono, 121 West 21st St., Chester, Pa.

52. These tools are made from a $\frac{1}{4}$ inch O.D. pipe threaded at each end. With them it is easy to install valve seals or valve springs on V8 Chevys. Remove spark plugs and screw in one of desired length of pipe to extend past manifold. Screw on $\frac{1}{4}$ inch pipe elbow and turn to desired position which air coupling can be pushed on. Before adding air pressure both valves in cylinder must be closed.





NINTH ANNUAL SPECIFICATIONS ISSUE

FIX FAX SPECIFICATIONS IDEAL REFERENCE GUIDE

Again as an annual feature, MOTOR AGE is proud to present in the following pages forty consecutive tables chocked full with statistical information on the service of United States cars and trucks as well as imported cars.



MARCUS AINSWORTH
Statistical Editor

A complete index may be found on page 5, the regular table of contents page of all of MOTOR AGE'S features.

The information was

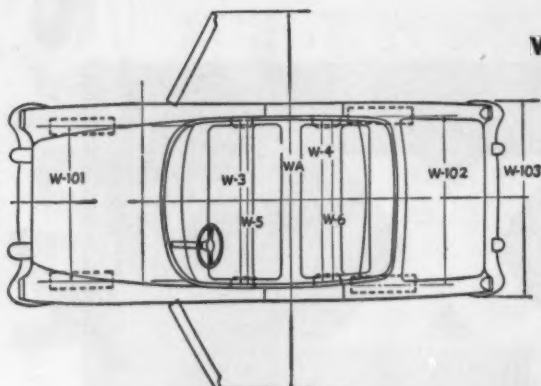
compiled under the direction of Marcus Ainsworth, Statistical Editor, along with Harold M. Nelson, Specifications Editor.

Your attention is directed to the front cover of this issue. Here in the upper left hand corner is marked a space for punching out the issue. Then the issue may be hung up in your service department or shop for quick reference.

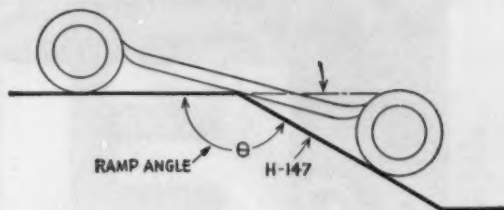
Open now to the 9th annual Fix Fax tables!



HAROLD NELSON
Specifications Editor



WIDTH DIMENSIONS



1960 U. S. PASSENGER CAR BODY DATA

All dimensions apply to 5 or 6 passenger, 4 door sedan or equivalent model.

PASSENGER CAR MAKE AND MODEL	MISCELLANEOUS							GENERAL DIMENSIONS (Ins.)										
	Type of Finish	Hood Hinge Location	Hood Counterbalanced	Hood Release Control	Windshield Type	Rear Window Type	Glass Area (Sq. In.)	L101	Overhang		Tread		Overall Dimensions					
									L104	L105	W101	W102	L103	W103	WA	HB		
1									Front—Including Bumper Guards	Rear—Including Bumper Guards	Front	Rear	Length—Bumper to Bumper	Width	Width— Doors Open	Height— Unloaded		
AMERICAN MOTORS CORP.																		
Rambler..... American 6000 ¹	E	R	Y	Ex	SC	C	740.0	700.0	1293.0	100.0	31.4	48.9	54.6	55.0	178.3	73.0	137.3	.....
..... Six 8010 ¹	E	R	Y	Ex	SC	C	1154.0	1236.0	1292.0	108.0	32.1	49.4	57.8	58.0	189.5	72.2	145.0	.....
..... Rebel 6020 ¹	E	R	Y	Ex	SC	C	1154.0	1236.0	1292.0	108.0	32.1	49.4	58.8	59.1	189.5	72.2	145.0	.....
..... Ambassador 6080 ¹	E	R	Y	Ex	CC	C	1372.0	1236.0	1292.0	117.0	32.1	49.4	57.8	59.1	196.5	72.2	145.0	.....
CHRYSLER CORP.																		
Chrysler..... Windoor PC1-L ¹	E	R	Y	In	CC	C	1575.0	1283.0	1186.0	122.0	34.7	58.7	61.0	59.7	215.4	79.4	154.1	57.4
..... Saratoga PC2-M ¹	E	R	Y	In	CC	C	1575.0	1283.0	1186.0	126.0	34.7	58.7	61.0	59.7	219.4	79.4	154.1	57.4
..... New Yorker PC3-H ¹	E	R	Y	In	CC	C	1575.0	1283.0	1186.0	126.0	34.7	58.9	61.2	60.0	219.6	79.4	154.1	57.4
..... 300-F PC3-H ²	E	R	Y	In	CC	C	1575.0	1778.0	1254.0	126.0	34.7	58.9	61.2	60.0	219.6	79.4	167.0	56.7
De Soto..... Fireflite PS1-L ¹	E	R	Y	In	CC	C	1575.0	1283.0	1186.0	122.0	34.7	58.7	61.0	59.7	215.4	79.4	154.1	56.8
..... Adventurer PS3-M ¹	E	R	Y	In	CC	C	1575.0	1980.0	1186.0	122.0	34.7	60.3	61.0	59.7	217.0	79.4	154.1	56.8
Dodge..... Matador PD1-L ¹	E	R	Y	Ex	CC	C	1575.0	1283.0	1186.0	122.0	33.5	57.1	61.0	59.7	212.6	78.0	154.0	56.7
..... Polara PD2-H ¹	E	R	Y	Ex	CC	C	1575.0	1283.0	1186.0	122.0	33.5	57.1	61.0	59.7	212.6	78.0	154.0	56.7
Dodge Dart..... Seneca PD3-L ¹	E	R	Y	Ex	CC	C	1575.0	1344.0	1186.0	118.0	33.5	57.1	61.0	60.1	208.6	78.0	154.0	56.7
..... Pioneer PD3-M ¹ , Phoenix PD3-H ¹	E	R	Y	Ex	CC	C	1575.0	1344.0	1186.0	118.0	33.5	57.1	61.0	60.2	208.6	78.0	154.0	56.7
..... Seneca PD4-L, Pioneer PD4-M	E	R	Y	Ex	CC	C	1575.0	1344.0	1186.0	118.0	33.5	57.1	61.0	60.2	208.6	78.0	154.0	56.7
..... Phoenix PD4-H	E	R	Y	Ex	CC	C	1575.0	1344.0	1186.0	118.0	33.5	57.1	61.0	60.2	208.6	78.0	154.0	56.7
Imperial Custom PY1-L ¹ , Crown PY1-M ¹	E	R	Y	In	CC	C	1622.0	1479.0	1342.0	129.0	39.7	57.6	61.8	62.2	226.3	80.5	159.7	58.4
Plymouth..... Le Baron PY1-H ¹	E	R	Y	In	CC	C	1622.0	802.0	1342.0	129.0	39.7	57.6	61.8	62.2	226.3	80.5	159.7	58.4
..... Savoy PP1-L ¹	E	R	Y	Ex	CC	C	1575.0	1344.0	1148.0	118.0	33.2	58.2	60.9	59.6	209.4	78.6	154.0	56.7
..... Belvedere PP2-M ¹	E	R	Y	Ex	CC	C	1575.0	1344.0	1148.0	118.0	33.2	58.2	60.9	59.7	209.4	78.6	154.0	56.7
..... Fury PP2-H ¹	E	R	Y	Ex	CC	C	1575.0	1344.0	1148.0	118.0	33.2	58.2	60.9	59.7	209.4	78.6	154.0	56.7
Valiant..... V-100 QX1-L ¹	E	R	Y	Ex	C	C	985.0	919.0	1434.0	106.5	29.2	48.0	56.0	55.5	183.7	70.4	131.8	55.6
..... V-200 QX1-H ¹	E	R	Y	Ex	C	C	985.0	919.0	1434.0	106.5	29.5	48.0	56.0	55.5	184.0	70.4	131.8	55.6
FORD MOTOR CO.																		
Comet..... (1)	E	R	N	Ex	C	FI	1235.3	893.3	1313.8	114.0	30.8	50.1	55.0	54.5	194.9	70.4	134.4	.....
Falcon..... (1)	E	R	N	Ex	C	C	1235.3	1146.1	1313.8	109.5	29.4	42.3	55.0	54.5	181.2	70.0	134.4	56.4
Ford Fairlane ¹ , Fairlane 500 ¹ , Galaxie ¹	E	R	Y	In	C	C	1474.1	1848.5	1450.6	119.0	34.0	60.7	61.0	60.0	213.7	81.5	154.8	56.0
Lincoln..... Lincoln ¹ , Premiere ¹	E	R	Y	In	CC	C	1701.1	1597.3	1439.7	131.0	34.7	61.4	61.0	61.0	227.2	80.3	157.9	56.4
..... Continental ¹	E	F	Y	In	CC	FI	1701.1	633.1	1439.7	131.0	34.7	61.4	61.0	61.0	227.2	80.3	157.9	56.4
Mercury..... Monterey ¹	E	F	Y	In	CC	C	1882.9	1345.8	1258.8	126.0	35.3	57.6	60.0	60.0	219.2	81.5	159.4	57.5
..... Montclair ¹	E	F	Y	In	CC	C	1882.9	1345.8	1258.8	126.0	35.3	57.6	60.0	60.0	219.2	81.5	159.4	57.5
..... Park Lane ¹	E	F	Y	In	CC	C	1882.9	1850.5	1308.8	126.0	35.3	57.6	60.0	60.0	219.2	81.5	159.4	57.5
Thunderbird..... (2)	E	F	Y	In	C	FI	1250.0	750.0	913.0	113.0	35.6	56.7	60.0	57.0	205.3	77.0	163.3	54.2
GENERAL MOTORS CORP.																		
Buick..... Le Sabre 4400 ¹ , Invicta 4000 ¹	L	R	Y	Ex	CC	C	1734.6	1557.6	1393.3	123.0	35.2	59.7	62.2	60.0	217.9	80.0	149.1	57.2
..... Electra 4700 ¹	L	R	Y	Ex	CC	C	1734.6	1557.6	1486.2	126.3	35.2	59.7	62.2	60.0	221.2	80.0	149.1	57.5
..... Electra 225 4800 ¹	L	R	Y	Ex	CC	C	1734.6	1557.6	1621.4	126.3	35.2	64.4	62.2	60.0	225.8	80.0	149.1	57.6
Cadillac..... 60 ¹	L	R	Y	Ex	CC	C	1734.6	1557.6	1621.4	130.0	34.8	60.5	61.0	61.0	225.0	79.9	154.4	.....
..... 62 ¹	L	R	Y	Ex	CC	C	1706.3	1309.1	1216.4	130.0	34.8	60.5	61.0	61.0	225.0	79.9	154.4	.....
..... 76 ¹	L	R	Y	Ex	CC	C	1742.8	461.7	2081.0	149.8	34.8	60.5	61.0	61.0	244.8	79.9	154.7	.....
Chevrolet..... Biscayne 1100 ¹ , 1200 ¹	L	R	Y	Ex	C	C	1740.1	1553.7	1394.3	119.0	32.6	59.2	60.3	59.3	210.6	80.8	148.9	58.1
..... Biscayne, Fleetmaster 1300 ¹ , 1400 ¹	L	R	Y	Ex	C	C	1120.3	1106.4	1154.8	108.0	30.3	41.7	54.0	54.0	180.0	66.9	129.3	52.8
Bel Air 1500 ¹ , 1600 ¹ , Impala 1700 ¹ , 1800 ¹	L	R	Y	Ex	C	FI	908.0	408.0	500.0	102.0	33.0	42.2	57.0	59.0	177.2	72.6	133.5	52.4
Corvair..... Standard 500 ¹ , Deluxe 700 ¹	L	L	F	N	In	CC	1809.7	1622.0	1609.0	123.0	35.3	59.3	61.0	61.0	220.8	80.6	154.4	58.3
Corvette..... 367	L	L	F	N	In	CC	1809.7	1622.0	1609.0	123.0	35.3	59.3	61.0	61.0	220.8	80.6	154.4	58.3
Oldsmobile..... Dynamic 88 ¹ , Super 88 ¹	L	R	Y	Ex	CC	C	1718.9	1557.6	1393.3	122.0	35.2	58.6	64.0	64.0	213.7	80.7	148.9	58.4
..... Ninety-Eight 98 ¹	L	R	Y	Ex	CC	C	1718.9	1557.6	1393.3	122.0	35.2	58.6	64.0	64.0	213.7	80.7	148.9	58.4
Pontiac..... Catalina 66-21 ¹	L	R	Y	Ex	CC	C	1718.9	1557.6	1393.3	122.0	35.2	58.6	64.0	64.0	213.7	80.7	148.9	58.4
..... Ventura 66-23 ¹	L	R	Y	Ex	CC	C	1718.9	1557.6	1393.3	122.0	35.2	58.6	64.0	64.0	213.7	80.7	148.9	58.4
..... Star Chief 66-24 ¹	L	R	Y	Ex	CC	C	1718.9	1557.6	1393.3	124.0	35.2	61.5	64.0	64.0	220.7	80.7	148.9	58.7
..... Bonneville 66-27 ¹	L	R	Y	Ex	CC	C	1718.9	1557.6	1393.3	124.0	35.2	61.5	64.0	64.0	220.7	80.7	148.9	58.4
STUDEBAKER-PACKARD CORP.																		
Studebaker..... Lark VI 60S ¹	E	R	N	In	C	C	1121.7	897.7	1244.1	106.5	26.4	40.1	57.4	56.8	175.0	71.4	135.4	59.2
..... Lark VIII 60V ¹	E	R	N	In	C	C	1121.7	897.7	1244.1	106.5	26.4	40.1	57.4	56.8	175.0	71.4	135.4	59.2
..... Hawk 60V ¹	E	R	N	In	FI	C	855.0	910.0	1098.0	120.5	34.9	48.0	57.4	56.8	204.0	71.3	135.3	57.9

ABBREVIATIONS
1—Four door sedan.
2—Four door hardtop sedan.

3—Two door hardtop sedan.
4—Two door convertible.
C—One piece curved.

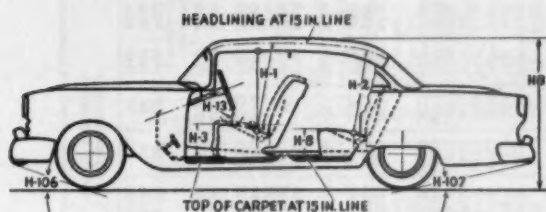
CC—Compound curved.
E—Enamel.
Ex—External.

F—Front.
FI—Three piece flat.
FI—One piece flat.

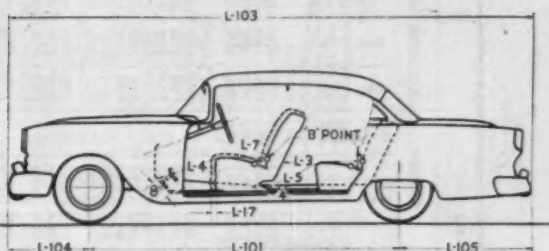
In—Internal.
L—Acrylic lacquer.
N—No.

R—Rear.
SC—Single curved.
Y—Yes.

HEIGHT DIMENSIONS



LENGTH DIMENSIONS



1960 U. S. PASSENGER CAR BODY DIMENSIONS

All dimensions apply to 5 or 6 passenger, 4 door sedan or equivalent model.

PASSENGER CAR MAKE AND MODEL	HEIGHT DIMENSIONS									WIDTH DIMENSIONS				LENGTH DIMENSIONS				
	Interior					Exterior				Interior				Interior				
	H1	H2	H3	H8	H13	H108	H107	H147	H156	W3	W4	W5	W8	L3	L4	L5	L7	L17
	Front Headroom	Rear Headroom	Front Cushion Height to Floor	Rear Cushion Height to Floor	Steering Wheel Clearance to Seat Cushion	Angle of Approach— Deg	Angle of Departure— Deg	Ramp Breakover Angle—Deg	Minimum Road Clearance—In	Front Seat Shoulder Room	Rear Seat Shoulder Room	Front Seat Hip Room	Rear Seat Hip Room	Back of Front Seat to Rear Seat Back	Leg Room— Front	Leg Room— Rear	Steering Wheel Clearance	Adjustment of Front Seat
2																		
AMERICAN MOTORS CORP.																		
Rambler..... American 6000 ¹	35.3	34.0	10.0	11.9	7.7	19.8	14.3	15.3	6.6 M	51.5	49.8	58.0	45.3	25.1	44.0	37.5	16.0	6.0
..... Six 6010 ¹	35.0	34.0	10.2	14.2	5.6	21.6	14.4	14.3	6.7 M	57.7	57.6	59.8	60.1	26.9	43.0	40.0	14.4	6.0
..... Rebel 6020 ¹	34.1	34.2	11.3	13.4	6.4	21.4	14.0	13.8	6.8 M	57.7	57.6	59.3	60.1	26.8	43.0	40.0	14.4	6.0
..... Ambassador 6080 ¹	35.0	34.0	10.2	14.2	5.5	21.9	13.3	12.5	6.3 M	57.7	57.6	59.8	60.1	26.9	43.0	40.0	14.4	6.0
CHRYSLER CORP.																		
Chrysler..... Windsor PC1-L1	34.6	34.5	12.3	12.8	6.3	18.6	10.4	12.4	5.2 M	60.3	59.6	63.0	62.4	29.4	46.2	44.1	16.2	4.5
..... Saratoga PC2-M1	34.6	34.5	12.3	12.8	6.3	16.2	10.7	12.4	5.5 M	60.3	59.6	63.0	62.4	29.4	46.2	44.1	16.3	4.5
..... New Yorker PC3-H1	34.6	34.5	12.3	12.8	6.3	18.7	11.0	12.6	5.9 M	60.3	59.6	63.0	62.4	29.4	46.2	44.1	16.3	4.5
..... 300F PC3-H2	34.1	34.2	11.3	11.6	6.4	16.3	10.6	10.6	5.8 M	55	55	55	55	28.6	45.8	35.4	16.3	4.5
De Soto..... Fireflite PS1-L1	34.6	34.5	12.3	12.8	6.3	18.6	10.4	12.4	5.2 M	60.3	62.8	63.0	62.4	29.4	46.2	44.1	16.2	4.5
..... Adventurer PS3-M1	34.6	33.1	12.3	12.8	6.3	18.6	10.4	12.4	5.2 M	60.3	62.8	63.0	62.4	29.4	46.2	44.1	16.2	4.5
Dodge..... Matarider PD-1L1, Polara PD2-H1	34.6	34.5	12.0	13.3	5.6	24.1	10.2	12.6	5.2 M	60.4	59.6	63.0	62.4	29.4	46.3	44.1	16.3	4.5
Dodge Dart..... Sencor PD3-L1, PD4-L1, Pioneer PD3-L1, PD4-L1, Phoenix PD3-H1, PD4-H1	34.6	34.5	12.0	13.3	5.6	24.1	9.9	12.6	5.0 M	60.4	59.6	63.0	62.4	27.4	46.3	43.6	16.3	4.5
Imperial..... Custom PY1-L1, Crown PY1-M1, Le Baron PY1-H1	34.5	33.8	11.7	12.4	6.7	16.1	10.4	13.6	5.6 M	64.0	62.0	61.6	60.2	31.0	46.3	42.9	19.2	5.0
Plymouth..... Savoy PP1-L1, PP2-L1, Belvedere PP1-M1, PP2-M1, Fury PP2-H1	34.6	34.2	12.2	12.9	6.0	19.4	10.9	12.6	5.0 M	60.4	59.6	63.0	62.4	28.4	45.4	38.1	16.3	4.5
Valiant..... V-100 QX1-L1, V-200 QX1-H1	33.6	33.4	11.4	14.0	6.2	27.0	12.5	11.4	5.4 ELT	54.6	54.1	57.0	56.9	28.0	44.4	36.9	15.1	4.5
FORD MOTOR CO.																		
Comet..... ⁽¹⁾	33.9	32.8	11.4	14.2	5.5	23.5	12.0	12.0	5.9	55.2	55.2	57.1	57.0	27.1	43.3	39.4	13.0	4.0
Falcon..... ⁽¹⁾	33.9	32.8	11.4	14.2	5.5	25.0	19.4	13.4	5.9 RS	55.2	55.2	57.1	57.0	27.9	43.3	39.4	13.0	4.0
Ford..... Fairlane ¹ , Fairlane 500 ¹ , Galaxie ¹	34.0	33.9	9.3	13.2	5.2	20.9	11.7	11.5	5.5	59.5	61.0	62.2	63.6	31.7	43.3	41.6	12.8	5.5
Lincoln..... Lincoln ¹ , Premiere ¹	34.0	33.7	11.3	14.2	5.0	22.7	11.7	12.9	6.0 RFC	62.3	63.4	60.2	65.2	35.2	44.0	44.9	15.4	3.0
..... Continental ¹	34.9	33.9	11.3	14.2	5.0	22.7	11.7	12.9	6.0 RFC	62.3	63.4	60.4	64.9	31.2	44.0	41.4	15.4	3.0
Mercury..... Monterey ¹	33.2	32.9	10.4	12.4	4.9	20.0	11.1	11.1	5.7 FDL	60.5	60.0	62.5	62.8	32.5	44.2	43.6	14.8	5.0
..... Montclair ¹	33.2	32.9	10.4	12.4	4.9	20.5	11.5	7.1	6.0 FDL	60.5	60.8	62.5	62.8	32.5	44.2	43.6	14.8	5.0
..... Park Lane ²	33.3	32.9	10.4	13.0	4.9	20.8	11.5	7.1	6.0 FDL	60.5	60.8	62.5	62.8	30.2	44.2	41.4	14.8	5.0
Thunderbird..... ⁽²⁾	34.5	33.3	11.0	13.1	5.7	19.7	13.1	12.5	5.9 ST	56.2	54.1	59.6	48.7	29.2	43.4	38.1	14.4	4.0
GENERAL MOTORS CORP.																		
Buick..... Le Sabre 4400 ¹	34.7	33.9	11.2	13.5	4.8	25.4	14.4	12.0	6.0 ES	60.6	59.0	65.4	65.1	28.8	44.2	42.2	14.8	4.8
..... Invicta 4600 ¹	34.7	33.9	11.0	13.4	4.8	25.4	14.4	12.0	6.0 ES	60.6	59.0	65.4	65.1	28.8	44.2	42.2	14.8	4.8
..... Electra 4700 ¹	34.2	33.4	11.4	14.2	4.4	25.9	14.6	12.3	6.2 ES	60.6	59.0	65.2	65.0	31.9	43.8	45.0	14.5	4.8
..... Electra 225 4800 ¹	34.2	33.3	11.4	14.2	4.4	25.9	13.4	12.3	6.2 ES	60.6	59.0	65.2	64.9	31.4	43.8	45.0	14.5	4.8
Cadillac..... 60 ¹	33.9	33.1	10.5	13.7	4.4	23.0	12.2	12.2	5.9	59.7	59.1	62.7	64.2	31.0	45.8	44.5	15.7	4.8
..... 62 ¹	32.8	34.0	10.2	12.2	4.7	23.0	12.2	12.2	5.9	60.5	59.0	65.2	65.6	31.8	46.0	43.3	16.0	4.8
..... 75 ¹	36.3	34.8	9.6	13.1	5.5	25.2	13.5	12.1	7.5	60.5	58.8	65.5	60.1	29.1	44.2	37.9	14.4	4.8
Chevrolet..... Biscayne 1100 ¹ , 1200 ¹ Biscayne Fleetmaster 1300 ¹ , 1400 ¹	36.1	34.3	10.0	13.5	5.0	27.0	12.0	11.5	6.0 M	60.5	59.0	65.3	65.4	29.2	44.5	42.5	13.7	4.5
..... Bel Air 1500 ¹ , 1800, Impala 1700 ¹ , 1900 ¹	33.6	33.2	10.0	11.5	5.1	22.2	16.0	13.5	6.0 VL	54.0	53.6	58.4	58.0	24.5	43.8	38.2	15.9	4.0
Corvair..... Standard 500 ¹ , Deluxe 700 ¹	25.3	N	7.3	N	5.3	20.6	16.5	7.5	5.9 RSH	49.4	N	59.0	N	N	45.1	N	16.0	4.4
Corvette..... 967 ⁴	34.7	33.9	10.2	13.5	4.6	24.2	12.9	11.1	6.2 FXB	60.6	59.0	65.4	65.2	28.6	45.0	42.2	13.7	4.8
Oldsmobile..... Dynamic 88 ¹	34.7	33.5	9.9	14.0	4.6	24.2	12.9	11.1	6.2 FXB	60.6	59.0	65.4	65.0	28.6	44.6	42.4	13.7	4.8
..... Super 88 ¹	34.2	33.4	10.3	14.2	4.2	24.7	13.2	11.0	6.2 FXB	60.6	59.0	65.2	65.0	31.9	44.5	45.5	13.3	4.8
..... Ninety-Eight 98 ¹	34.2	33.4	10.3	14.2	4.2	24.7	13.2	11.0	6.2 FXB	60.6	59.0	65.2	65.0	31.9	44.5	45.5	13.3	4.8
Pontiac..... Catalina 60-21 ¹	34.0	33.9	9.8	13.6	5.0	21.8	12.6	11.5	5.8 FSB	60.6	59.0	65.4	65.4	29.1	45.3	41.9	15.4	4.8
..... Ventura 60-23 ¹	32.9	34.2	9.8	12.0	5.2	21.8	12.6	11.5	5.8 FSB	60.6	59.0	65.3	66.2	29.5	45.3	40.8	15.5	4.8
..... Star Chief 60-24 ¹	34.6	33.9	9.6	13.5	5.2	21.8	12.1	11.3	5.8 FSB	60.6	59.0	65.4	65.4	29.1	45.2	41.4	15.5	4.8
..... Bonneville 60-25 ¹	32.9	34.2	9.6	11.7	6.2	21.8	12.1	11.3	5.8 FSB	60.5	59.0	65.2	66.2	29.5	45.2	40.3	15.5	4.8
STUDEBAKER-PACKARD CORP.																		
Studebaker..... Lark VI 60S ¹ , Lark VIII 60V ¹	36.0	35.0	12.8	11.0	5.5	20.5	18.5	12.5	6.1 ERC	55.5	54.5	59.5	59.0	26.0	44.0	39.0	13.8	5.5
..... Hawk 60V ¹	34.5	33.8	10.9	9.0	4.5	20.0	17.0	14.0	6.9 HPC	55.0	53.0	59.5	58.0	26.5	44.0	37.0	13.8	5.5

ABBREVIATIONS

- 1—Four door sedan.
- 2—Four door hardtop sedan.
- 3—Two door hardtop.
- 4—Two door convertible.

ELT—At exhaust pipe, bottom of front longitudinal and transmission cross members.
ERC—Engine rear cross member.
ES—Exhaust system.
FDL—Frame at dash line.

FSB—Frame side bar.
FXB—Frame cross bar.
HPC—Body front hinge pillar cross member.
IS—Not applicable; individual seats.
M—Muffler.

N—None.
RFC—Resonator front clamps.
RS—Rear suspension.
RSH—Rear spring front hanger.
ST—Spare tire well.
VL—Various locations.

GENERAL ENGINE DATA, PISTONS

PASSENGER CAR MAKE AND MODEL	STANDARD ENGINE										PISTONS																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																								
	Type	Valve Arrangement	Number of Cylinders, Bore and Stroke	Piston Disp.—	Compression Ratio (to 1)		Mounting Points	Taxable HP	Published BHP at Engine RPM	Published Torque (lb. ft. at engine RPM)	Recommended Idle speed—RPM	Shipping Weight—5-8 Pass., 4 Dr., Sed.	Weight per Cu. In. ¹	HP per Cu. In.	Description and Finish	Weight—Oz. (Piston Only)	Clearance			Ring Groove Depth																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																															
					Standard	Optional											Top Land	Bottom Skirt	No. 1 Ring	No. 2 Ring	No. 3 Ring																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																														
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Rambler	IL	LH	8-3 1/4"x4 1/2"	185.6	8.00	N	23.44	90-3300	150-1600	550	2484	15.31	33.27	.480	Fl.Sa.Tp.Co	14.00	.0180	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008	.0008

OPTIONAL ENGINE DATA

PASSENGER CAR MAKE AND MODEL	ENGINE						CARBURETOR				IGNITION		AXLE RATIO (Standard)		Changes From Standard Engine			
	Type	Valve Arrangement	Number of Cylinders, Bore and Stroke	Cyl. In.	Compression Ratio (to 1)	Published Maximum BHP at Engine RPM	Published Maximum Torque (Lb. ft. at rpm)	Operating Torque Clearance		Exhaust System Type	CARBURETOR		IGNITION			Std. Transmission	Auto. Transmission	
								Intake	Exhaust		Make	Model	Number Used	Type				Make	Distributor
10																		
AMERICAN MOTORS CORP.	8-9010 Rambler	HH	8-3 1/2x4 1/2	188.6	8.70	138-4500	185-1860	.012H	.010H	S	Curt	WCD-2887-S	1	D	DR	1110246	3.31	Car
	8-4020	HH	8-3 1/2x3 1/2	250.0	8.70	215-4500	280-2960	.012H	.014H	D	Hyd	4180	1	FB	AL	1110246	3.15	CR, Ex, Car, IT, AR
	8-5080	HH	8-4 1/2x3 1/2	327.0	9.70	270-4700	360-2600			D	Hyd		1	FB	AL	1110246	3.31	CR, Ex, Car, IT, AR
		HH	8-4 1/2x3 1/2	327.0	9.70	270-4700	360-2600			D	Hyd		1	FB	AL	1110246	3.31	CR, Ex, Car, IT, AR
CHRYSLER CORP.	8-PP3-H, 300F	V	8-4 1/2x3 1/2	413.0	10.10	400-5200	485-3200	.010C	.020C	D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.31	VL, Ex, Car, IT
	8-PS1-L, PS3-M	V	8-4 1/2x3 1/2	383.0	10.00	325-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	2.83	Ex, Car
	8-PP3-L, PS3-M	V	8-4 1/2x3 1/2	383.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	2.83	Ex, Car, Dis, IT
	8-PD2-H	V	8-4 1/2x3 1/2	383.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.31	Ex, Car, Dis, IT
	8-PD4-H	V	8-4 1/2x3 1/2	383.0	10.00	310-4500	435-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.31	Complete Engine
	8-PD4-H	V	8-4 1/2x3 1/2	383.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.31	Complete Engine
	8-PD2-L, PP2-M	V	8-4 1/2x3 1/2	381.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.31	Complete Engine
	8-PD2-L, PP2-M	V	8-4 1/2x3 1/2	381.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.31	Complete Engine
	8-PP2-L, PP2-M	V	8-4 1/2x3 1/2	383.0	10.00	310-4500	435-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	2.83	Car, Dis, IT
	8-PP2-L, PP2-M	V	8-4 1/2x3 1/2	383.0	10.00	325-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	2.83	Complete Engine
	8-PP2-L, PP2-M	V	8-4 1/2x3 1/2	383.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.15	Car, IT
	8-PP2-L, PP2-M	V	8-4 1/2x3 1/2	383.0	10.00	330-4500	480-2800	Hyd		D	Hyd	AFB-2803-S	1	FB	AL	1110246	3.15	Car, IT
Valiant	8-6-21, QX1-H	HH	8-3 1/2x3 1/2	176.0	10.90	148-5200	193-4200	.010H	.020H	D	Hyd	AFB-2803-S	1	FB	Chr	2095050	3.23	CR, Ex, Car, Dis, IT, AR
FORD MOTOR CO.	8-Fairlane, Fairlane 500, Galaxie 500	V	8-4 1/2x3 1/2	383.0	8.90	235-4400	300-2400	Hyd		SC	F-H	COAF-12127E	1	D	DR	1110982	2.91	Complete Engine
	8-Fairlane, Fairlane 500, Galaxie 500	V	8-4 1/2x3 1/2	383.0	8.90	240-4400	300-2400	Hyd		SC	F-H	COAF-12127G	1	D	DR	1110982	2.81	Complete Engine
	8-Fairlane, Fairlane 500, Galaxie 500	V	8-4 1/2x3 1/2	383.0	8.90	240-4400	300-2400	Hyd		SC	F-H	COAF-12127H	1	D	DR	1110982	2.81	Complete Engine
	8-Fairlane, Fairlane 500, Galaxie 500	V	8-4 1/2x3 1/2	383.0	8.90	240-4400	300-2400	Hyd		SC	F-H	COAF-12127H	1	D	DR	1110982	2.81	Complete Engine
GENERAL MOTORS CORP.	8-4400	V	8-4 1/2x3 1/2	384.0	9.00	225-4400	375-2400	Hyd		SC	C-S	WGD	1	D	DR	1110982	2.07	CR, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	10.25	225-4400	375-2400	Hyd		SC	C-S	WGD	1	D	DR	1110982	2.07	CR, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	10.25	225-4400	375-2400	Hyd		SC	C-S	WGD	1	D	DR	1110982	2.07	CR, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	9.50	225-4400	375-2400	Hyd		SC	C-S	WGD	1	D	DR	1110982	2.07	CR, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	9.50	225-4400	375-2400	Hyd		SC	C-S	WGD	1	D	DR	1110982	2.07	CR, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	9.50	225-4400	375-2400	Hyd		SC	C-S	WGD	1	D	DR	1110982	2.07	CR, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	11.00	200-4500	355-3200	Hyd	.010H	D	Rec	70130154	3	D	DR	1110948	3.36	CR, VL, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	11.25	330-5500	365-3600	.012H	.010H	D	Rec	7772600	1	FB	DR	1110918	3.70	CR, VL, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	11.25	330-5500	365-3600	.012H	.010H	D	Rec	7772600	1	FB	DR	1110918	3.70	CR, VL, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	11.25	330-5500	365-3600	.012H	.010H	D	Rec	7772600	1	FB	DR	1110918	3.70	CR, VL, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	11.25	330-5500	365-3600	.012H	.010H	D	Rec	7772600	1	FB	DR	1110918	3.70	CR, VL, Car, Dis, IT, AR
	8-1200, 1400, 1600, 1800	V	8-4 1/2x3 1/2	384.0	11.25	330-5500	365-3600	.012H	.010H	D	Rec	7772600	1	FB	DR	1110918	3.70	CR, VL, Car, Dis, IT, AR
Cervair	8-857	V	8-3 1/2x3 1/2	140.0	8.00	80-4500	125-2500	Hyd		SC	Rec	7015171	1	FB	DR	1110280	3.50	CR, Car, Dis, IT
	8-857	V	8-3 1/2x3 1/2	140.0	8.00	80-4500	125-2500	Hyd		SC	Rec	7015171	1	FB	DR	1110280	3.50	CR, Car, Dis, IT
	8-857	V	8-3 1/2x3 1/2	140.0	8.00	80-4500	125-2500	Hyd		SC	Rec	7015171	1	FB	DR	1110280	3.50	CR, Car, Dis, IT
	8-857	V	8-3 1/2x3 1/2	140.0	8.00	80-4500	125-2500	Hyd		SC	Rec	7015171	1	FB	DR	1110280	3.50	CR, Car, Dis, IT
Oldsmobile	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
Pontiac	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
Studebaker-Packard Corp.	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
Studebaker-Packard Corp.	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT
	8-487	V	8-3 1/2x3 1/2	203.0	11.00	275-5200	305-4400	.013H	.010H	D	Rec	70131087	2	FB	DR	1110981	3.70	CR, VL, Car, Dis, IT

1—With Power Flite only; Torque Flite, 2.80.
2—Carter only; Stromberg, WW.
3—Carter only; Rochester, 40C.
4—Front only; center, 7013000; rear, 7013017.
5—Front only; rear, 7013017.
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PISTON RINGS, PISTON PINS, AND CONNECTING RODS

PASSENGER CAR MAKE AND MODEL	PISTON RINGS										PISTON PIN										CONNECTING RODS																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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ABBREVIATIONS
 1—Upper only; lower, Ferros.
 2—Upper only; lower, Ferros.
 3—Upper only; lower, Ferros.
 4—Upper only; lower, Ferros.
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 6—Upper only; lower, Ferros.
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 49—Upper only; lower, Ferros.
 50—Upper only; lower, Ferros.

2A—Steel alloy.
5C3—Silichrome steel.
5I—Steel.
Std—Standard.
Var—Various.

10

Tik--Turn ignition key to extreme clockwise position.

IGNITION TIMING, SPARK PLUGS, AND CLUTCHES

PASSENGER CAR MAKE AND MODEL	IGNITION TIMING		SPARK PLUGS		Cylinder Numbering System and Firing Order	Suppression Location	CLUTCH (PEDAL OPERATED)												
	C/o deg. @ RPM	Mark Location	Make and Model	Thread (mm)			Lightening Torque (ft. lb.)	Clap	Total Plate Pressure			Facing			Release Bearing	Method of Dampening			
									Type	Pressure	Number of Driven Discs	Material	Inside Diam. (in.)	Outside Diam. (in.)			Effective Area (sq. in.)	Thickness (in.)	Engagement Method
AMERICAN MOTORS CORP. Rambler B-4010 B-4015 B-4020 TC	3B...	VD	AL-AL-71	14	30	.035	1,3,3,2,4	HT	BB	D	Co	1216	1						
	9B...	VD	AL-AL-71	14	30	.035	1,3,3,2,4	HT	BB	D	Co	1446	1						
	TC...	VD	AL-AL-71	14	30	.035	1,4R,2R,3,3R,3L,4L,1R	Die	BB	D	Co	1664	1						
CHRYSLER CORP. Chrysler De Soto Dodge Dodge Dart Dodge Dart Imperial Plymouth Plymouth Valiant	10B-500	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	N	D	Co	2200	1						
	8B-	VD	AL-A32	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-L	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-M	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	N	D	Co	1629	1						
	8-PS1-L	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-M	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-L	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-M	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-L	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
	8-PS1-M	VD	AL-A42	14	31	.035	1,4R,2R,3,3R,3L,4L,1R	SP, HT	BB	D	Co	1629	1						
FORD MOTOR CO. Comet Falcon Ferd Lincoln Mercury Mustang Thunderbird	2B-500	VD	CH-F14V	18	25	.034	1,3,3,2,4	G.V.R. Wh. HT	L	D	Co	1200	1						
	2B-500	VD	CH-F14V	18	25	.034	1,3,3,2,4	G.V.R. Wh. HT	L	D	Co	1200	1						
	4B-500	VD	CH-F14V	18	25	.034	1,4R,4L,2,3R,3L,2R	G.V.R. Wh. HT	L	D	Co	1276	1						
	4B-500	VD	CH-F14V	18	25	.034	1,4R,4L,2,3R,3L,2R	G.V.R. Wh. HT	L	D	Co	1276	1						
	4B-500	VD	CH-F14V	18	25	.034	1,4R,4L,2,3R,3L,2R	G.V.R. Wh. HT	N	D	Co	1602	1						
	4B-500	VD	CH-F14V	18	25	.034	1,4R,4L,2,3R,3L,2R	G.V.R. Wh. HT	N	D	Co	1602	1						
	4B-500	VD	CH-F14V	18	25	.034	1,4R,4L,2,3R,3L,2R	G.V.R. Wh. HT	N	D	Co	1602	1						
	4B-500	VD	CH-F14V	18	25	.034	1,4R,4L,2,3R,3L,2R	G.V.R. HT	L	D	Co	1576	1						
GENERAL MOTORS CORP. Buick Cadillac Chevrolet Corvair Covolt Oldsmobile Pontiac	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	L	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
	5B-400	HB	AC-445	14	28	.033	1R,1L,4R,4L,2,3R,3L,2R	HT, Co, G.V.R.	N	D	Co	1846	1						
STUDEBAKER-PACKARD CORP. Studebaker B-40V B-40V Hawk	2B-550	VD	CH-H17	14	28	.031	1,3,3,2,4	Die, Co, G.V.R.H	BB	D	Co	1100	1						
	4B-550	VD	CH-H17	14	28	.031	1,4R,2R,3,3R,3L,4L,1R	Die, Co, G.V.R.H	BB	D	Co	1994	1						

ABBREVIATIONS

B—Ball.
B-Borg and Beck or Auburn.
BB—Borg and Beck.
BD—Borgite die with vibration damper.
BG—Borg and Beck with Long driven member.
B-L—Borg and Beck with Long driven member.
CB—Camshaft balancers.
CF—Crankshaft bearings.
CH—Champion.
CI—Cramped flat springs.
CO—Coil.
CP—Camshaft pulley.
CS—Coil springs.
D—Dry.
DD—Double dampers.
DF—Distributor.
F—Flywheel.
G—Grind straps on cylinder head.
GM—General.
H—Hood.
HB—Harmonic balancer.
HT—High tension lead.
L—Left bank or driver's side.
M—Motor.
MD—Molded asbestos.
MMA—Molded woven asbestos.
N—Noise.
NP—Oil pipe grounded.
P—Pre-lubricated.
R—Right bank or passenger's side.
R-Resistor.
S—Springs.
SS—Spring steel.
TD—Top center.
TDS—Throd disc with vibration damper.
VD—Vibration dampener.
VR—Voltage regulator.
WA—Woven asbestos.
WH—Wheels.

LAMP BULBS, FUSES AND CIRCUIT BREAKERS

LAMP BULBS (Trade Number)										FUSES AND CIRCUIT BREAKERS (Trade Number)												
PASSENGER CAR MAKE AND MODEL	Headlamp			Parking Light	Tail Light	Stop Light	Direction Signal		Clock Light	Headlamp	Headlamp Beam Indicator	Parking Light	Tail Light	Stop Light	Direction Indicator	License Plate Light	Instrument Light	Ignition Light	Back-Up Light	Dome Light	Clock	
	High Beam	Low Beam	Arrangement				Headlamp Beam Indicator															
								Front														Rear	Indicator
12	N	6012	S	57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	N	N
	6001	4002	4002	57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	AGA2	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
AMERICAN MOTORS CORP. B-6000 B-6010; B-6020, 6080	6001	4002	4002	57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
	4001	4002	4002	57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
				57	57	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	1034	SFE1	AGA2
CHRYSLER CORP. B-PC-1-L, PC-2-M, PC-3-H B-PS-1-L, PS-3-H B-PD-1-L, PD-2-H B-PD-3-L, PD-3-H B-PD-4-L, PD-4-M B-PD-5-L, PD-5-M B-PD-6-L, PD-6-M B-PD-7-L, PD-7-M B-PD-8-L, PD-8-M B-PD-9-L, PD-9-M B-PD-10-L, PD-10-M B-PD-11-L, PD-11-M B-PD-12-L, PD-12-M B-PD-13-L, PD-13-M B-PD-14-L, PD-14-M B-PD-15-L, PD-15-M B-PD-16-L, PD-16-M B-PD-17-L, PD-17-M B-PD-18-L, PD-18-M B-PD-19-L, PD-19-M B-PD-20-L, PD-20-M B-PD-21-L, PD-21-M B-PD-22-L, PD-22-M B-PD-23-L, PD-23-M B-PD-24-L, PD-24-M B-PD-25-L, PD-25-M B-PD-26-L, PD-26-M B-PD-27-L, PD-27-M B-PD-28-L, PD-28-M B-PD-29-L, PD-29-M B-PD-30-L, PD-30-M B-PD-31-L, PD-31-M B-PD-32-L, PD-32-M B-PD-33-L, PD-33-M B-PD-34-L, PD-34-M B-PD-35-L, PD-35-M B-PD-36-L, PD-36-M B-PD-37-L, PD-37-M B-PD-38-L, PD-38-M B-PD-39-L, PD-39-M B-PD-40-L, PD-40-M B-PD-41-L, PD-41-M B-PD-42-L, PD-42-M B-PD-43-L, PD-43-M B-PD-44-L, PD-44-M B-PD-45-L, PD-45-M B-PD-46-L, PD-46-M B-PD-47-L, PD-47-M B-PD-48-L, PD-48-M B-PD-49-L, PD-49-M B-PD-50-L, PD-50-M B-PD-51-L, PD-51-M B-PD-52-L, PD-52-M B-PD-53-L, PD-53-M B-PD-54-L, PD-54-M B-PD-55-L, PD-55-M B-PD-56-L, PD-56-M B-PD-57-L, PD-57-M B-PD-58-L, PD-58-M B-PD-59-L, PD-59-M B-PD-60-L, PD-60-M B-PD-61-L, PD-61-M B-PD-62-L, PD-62-M B-PD-63-L, PD-63-M B-PD-64-L, PD-64-M B-PD-65-L, PD-65-M B-PD-66-L, PD-66-M B-PD-67-L, PD-67-M B-PD-68-L, PD-68-M B-PD-69-L, PD-69-M B-PD-70-L, PD-70-M B-PD-71-L, PD-71-M B-PD-72-L, PD-72-M B-PD-73-L, PD-73-M B-PD-74-L, PD-74-M B-PD-75-L, PD-75-M B-PD-76-L, PD-76-M B-PD-77-L, PD-77-M B-PD-78-L, PD-78-M B-PD-79-L, PD-79-M B-PD-80-L, PD-80-M B-PD-81-L, PD-81-M B-PD-82-L, PD-82-M B-PD-83-L, PD-83-M B-PD-84-L, PD-84-M B-PD-85-L, PD-85-M B-PD-86-L, PD-86-M B-PD-87-L, PD-87-M B-PD-88-L, PD-88-M B-PD-89-L, PD-89-M B-PD-90-L, PD-90-M B-PD-91-L, PD-91-M B-PD-92-L, PD-92-M B-PD-93-L, PD-93-M B-PD-94-L, PD-94-M B-PD-95-L, PD-95-M B-PD-96-L, PD-96-M B-PD-97-L, PD-97-M B-PD-98-L, PD-98-M B-PD-99-L, PD-99-M B-PD-100-L, PD-100-M B-PD-101-L, PD-101-M B-PD-102-L, PD-102-M B-PD-103-L, PD-103-M B-PD-104-L, PD-104-M B-PD-105-L, PD-105-M B-PD-106-L, PD-106-M B-PD-107-L, PD-107-M B-PD-108-L, PD-108-M B-PD-109-L, PD-109-M B-PD-110-L, PD-110-M B-PD-111-L, PD-111-M B-PD-112-L, PD-112-M B-PD-113-L, PD-113-M B-PD-114-L, PD-114-M B-PD-115-L, PD-115-M B-PD-116-L, PD-116-M B-PD-117-L, PD-117-M B-PD-118-L, PD-118-M B-PD-119-L, PD-119-M B-PD-120-L, PD-120-M B-PD-121-L, PD-121-M B-PD-122-L, PD-122-M B-PD-123-L, PD-123-M B-PD-124-L, PD-124-M B-PD-125-L, PD-125-M B-PD-126-L, PD-126-M B-PD-127-L, PD-127-M B-PD-128-L, PD-128-M B-PD-129-L, PD-129-M B-PD-130-L, PD-130-M B-PD-131-L, PD-131-M B-PD-132-L, PD-132-M B-PD-133-L, PD-133-M B-PD-134-L, PD-134-M B-PD-135-L, PD-135-M B-PD-136-L, PD-136-M B-PD-137-L, PD-137-M B-PD-138-L, PD-138-M B-PD-139-L, PD-139-M B-PD-140-L, PD-140-M B-PD-141-L, PD-141-M B-PD-142-L, PD-142-M B-PD-143-L, PD-143-M B-PD-144-L, PD-144-M B-PD-145-L, PD-145-M B-PD-146-L, PD-146-M B-PD-147-L, PD-147-M B-PD-148-L, PD-148-M B-PD-149-L, PD-149-M B-PD-150-L, PD-150-M B-PD-151-L, PD-151-M B-PD-152-L, 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B-PD-200-L, PD-200-M B-PD-201-L, PD-201-M B-PD-202-L, PD-202-M B-PD-203-L, PD-203-M B-PD-204-L, PD-204-M B-PD-205-L, PD-205-M B-PD-206-L, PD-206-M B-PD-207-L, PD-207-M B-PD-208-L, PD-208-M B-PD-209-L, PD-209-M B-PD-210-L, PD-210-M B-PD-211-L, PD-211-M B-PD-212-L, PD-212-M B-PD-213-L, PD-213-M B-PD-214-L, PD-214-M B-PD-215-L, PD-215-M B-PD-216-L, PD-216-M B-PD-217-L, PD-217-M B-PD-218-L, PD-218-M B-PD-219-L, PD-219-M B-PD-220-L, PD-220-M B-PD-221-L, PD-221-M B-PD-222-L, PD-222-M B-PD-223-L, PD-223-M B-PD-224-L, PD-224-M B-PD-225-L, PD-225-M B-PD-226-L, PD-226-M B-PD-227-L, PD-227-M B-PD-228-L, PD-228-M B-PD-229-L, PD-229-M B-PD-230-L, PD-230-M B-PD-231-L, PD-231-M B-PD-232-L, PD-232-M B-PD-233-L, PD-233-M B-PD-234-L, PD-234-M B-PD-235-L, PD-235-M B-PD-236-L, PD-236-M B-PD-237-L, PD-237-M B-PD-238-L, PD-238-M B-PD-239-L, PD-239-M B-PD-240-L, PD-240-M B-PD-241-L, PD-241-M B-PD-242-L, PD-242-M B-PD-243-L, PD-243-M B-PD-244-L, PD-244-M B-PD-245-L, PD-245-M B-PD-246-L, PD-246-M B-PD-247-L, 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B-PD-295-L, PD-295-M B-PD-296-L, PD-296-M B-PD-297-L, PD-297-M B-PD-298-L, PD-298-M B-PD-299-L, PD-299-M B-PD-300-L, PD-300-M B-PD-301-L, PD-301-M B-PD-302-L, PD-302-M B-PD-303-L, PD-303-M B-PD-304-L, PD-304-M B-PD-305-L, PD-305-M B-PD-306-L, PD-306-M B-PD-307-L, PD-307-M B-PD-308-L, PD-308-M B-PD-309-L, PD-309-M B-PD-310-L, PD-310-M B-PD-311-L, PD-311-M B-PD-312-L, PD-312-M B-PD-313-L, PD-313-M B-PD-314-L, PD-314-M B-PD-315-L, PD-315-M B-PD-316-L, PD-316-M B-PD-317-L, PD-317-M B-PD-318-L, PD-318-M B-PD-319-L, PD-319-M B-PD-320-L, PD-320-M B-PD-321-L, PD-321-M B-PD-322-L, PD-322-M B-PD-323-L, PD-323-M B-PD-324-L, PD-324-M B-PD-325-L, PD-325-M B-PD-326-L, PD-326-M B-PD-327-L, PD-327-M B-PD-328-L, PD-328-M B-PD-329-L, PD-329-M B-PD-330-L, PD-330-M B-PD-331-L, PD-331-M B-PD-332-L, PD-332-M B-PD-333-L, PD-333-M B-PD-334-L, PD-334-M B-PD-335-L, PD-335-M B-PD-336-L, PD-336-M B-PD-337-L, PD-337-M B-PD-338-L, PD-338-M B-PD-339-L, PD-339-M B-PD-340-L, PD-340-M B-PD-341-L, PD-341-M B-PD-342-L, PD-342-M B-PD-343-L, PD-343-M B-PD-344-L, PD-344-M B-PD-345-L, PD-345-M B-PD-346-L, PD-346-M B-PD-347-L, PD-347-M B-PD-348-L, PD-348-M B-PD-349-L, PD-349-M B-PD-350-L, PD-350-M B-PD-351-L, PD-351-M B-PD-352-L, PD-352-M B-PD-353-L, PD-353-M B-PD-354-L, PD-354-M B-PD-355-L, PD-355-M B-PD-356-L, PD-356-M B-PD-357-L, PD-357-M B-PD-358-L, PD-358-M B-PD-359-L, PD-359-M B-PD-360-L, PD-360-M B-PD-361-L, PD-361-M B-PD-362-L, PD-362-M B-PD-363-L, PD-363-M B-PD-364-L, PD-364-M B-PD-365-L, PD-365-M B-PD-366-L, PD-366-M B-PD-367-L, PD-367-M B-PD-368-L, PD-368-M B-PD-369-L, PD-369-M B-PD-370-L, PD-370-M B-PD-371-L, PD-371-M B-PD-372-L, PD-372-M B-PD-373-L, PD-373-M B-PD-374-L, PD-374-M B-PD-375-L, PD-375-M B-PD-376-L, PD-376-M B-PD-377-L, PD-377-M B-PD-378-L, PD-378-M B-PD-379-L, PD-379-M B-PD-380-L, PD-380-M B-PD-381-L, PD-381-M B-PD-382-L, PD-382-M B-PD-383-L, PD-383-M B-PD-384-L, PD-384-M B-PD-385-L, PD-385-M B-PD-386-L, PD-386-M B-PD-387-L, PD-387-M B-PD-388-L, PD-388-M B-PD-389-L, PD-389-M B-PD-390-L, PD-390-M B-PD-391-L, PD-391-M B-PD-392-L, PD-392-M B-PD-393-L, PD-393-M B-PD-394-L, PD-394-M B-PD-395-L, PD-395-M B-PD-396-L, PD-396-M B-PD-397-L, PD-397-M B-PD-398-L, PD-398-M B-PD-399-L, PD-399-M B-PD-400-L, PD-400-M B-PD-401-L, PD-401-M B-PD-402-L, PD-402-M B-PD-403-L, PD-403-M B-PD-404-L, PD-404-M B-PD-405-L, PD-405-M B-PD-406-L, PD-406-M B-PD-407-L, PD-407-M B-PD-408-L, PD-408-M B-PD-409-L, PD-409-M B-PD-410-L, PD-410-M B-PD-411-L, PD-411-M B-PD-412-L, PD-412-M B-PD-413-L, PD-413-M B-PD-414-L, PD-414-M B-PD-415-L, PD-415-M B-PD-416-L, PD-416-M B-PD-417-L, PD-417-M B-PD-418-L, PD-418-M B-PD-419-L, PD-419-M B-PD-420-L, PD-420-M B-PD-421-L, PD-421-M B-PD-422-L, PD-422-M B-PD-423-L, PD-423-M B-PD-424-L, PD-424-M B-PD-425-L, PD-425-M B-PD-426-L, PD-426-M B-PD-427-L, PD-427-M B-PD-428-L, PD-428-M B-PD-429-L, PD-429-M B-PD-430-L, PD-430-M B-PD-431-L, PD-431-M B-PD-432-L, PD-432-M B-PD-433-L, PD-433-M B-PD-434-L, PD-434-M B-PD-435-L, PD-435-M B-PD-436-L, PD-436-M B-PD-437-L, PD-437-M B-PD-438-L, PD-438-M B-PD-439-L, PD-439-M B-PD-440-L, PD-440-M B-PD-441-L, PD-441-M B-PD-442-L, PD-442-M B-PD-443-L, PD-443-M B-PD-444-L, PD-444-M B-PD-445-L, PD-445-M B-PD-446-L, PD-446-M B-PD-447-L, PD-447-M B-PD-448-L, PD-448-M B-PD-449-L, PD-449-M B-PD-450-L, PD-450-M B-PD-451-L, PD-451-M B-PD-452-L, PD-452-M B-PD-453-L, PD-453-M B-PD-454-L, PD-454-M B-PD-455-L, PD-455-M B-PD-456-L, PD-456-M B-PD-457-L, PD-457-M B-PD-458-L, PD-458-M B-PD-459-L, PD-459-M B-PD-460-L, PD-4																						

TRANSMISSIONS—Conventional and Conventional with Overdrive

PASSENGER CAR MAKE AND MODEL	TYPE	CONVENTIONAL TRANSMISSION										OVERDRIVE																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																						
		Manual	Manual with O.D.	Automatic	No. of Forward Speeds	Ratios			Specify Gears	Lubricant				Lubricant																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																				
						First	Second	Third		Reverse	Capacity (pt.)	Type Recommended	SAE Viscosity Number			Capacity (pt.)	Separate Filter	Type Recommended	SAE Viscosity Number																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																															
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ABBREVIATIONS
 1—80 or 90.
 2—Direct drive in fourth speed.
 AS—All forward speeds.
 EP—Extreme pressure.
 ME—Mild extreme pressure.
 MO—Mineral gear lubricant.
 MNO—Mineral oil.
 MP—Multipurpose gear oil.
 N—None.
 NA—Not available.
 Out—Optional.
 P—Planetary.
 ST—Second and third speeds.
 Std—Standard.
 Y—Yes.

PB—Push button.
R—Reverse.
S—Super range.
TCG—Torque converter with planetary gear.
WC—Water cooled.

PROPELLER SHAFT, AND REAR AXLE

PROPELLER SHAFT										REAR AXLE													
PASSENGER CAR MAKE AND MODEL																							
Number Used	Type	Intermediate Bearing		Universals				Type	Drive and Torque Taken Through	Gear Type	Drive Pinion Offset	Gearing				Ring Gear		Pinion Adjustment	Pinion Bearing Adjustment	Wheel Bearing Type	Lubricant Capacity (Pl.)		
		Type	Lubrication	Make	Number Used	Type	Bearing																
							Type					Lubrication											
18																							
AMERICAN MOTORS CORP.																							
1	Ex	N	P	Mec	1	Cr	AF	RS	Hy	1.50	1	3.31	3.70	3.31	13-43	9-34	7.54	Sh	C-R	3			
1	TT	AF	P	Mec	1	Cr	AF	TT	Hy	1.50	1	3.31	4.11	3.70	13-43	9-37	7.54	Sh	C-R	3			
1	TT	AF	P	Mec	1	Cr	AF	TT	Hy	1.50	1	3.31	4.10	3.54	15-41	10-41	8.75	Sh	C-R	4			
CHRYSLER CORP.																							
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1	Ex	N	P	Det	2	B-C	AF	RS	Hy	1.50	2	2.83	3.31	N	14-41	N	8.75	Sh	TR	3 1/2			
1</																							

STANDARD TIRES		SERVICE BRAKES										PARKING BRAKES									
Rev. per Mile	Tire Size	Type	Power Brake Type	Effective Area (sq. in.)	Percent Brake Effectiveness—Front	Drum		Brake Lining				Wheel Cylinder Bore		Master Cylinder Bore	Available Pedal Travel	Line Pressure at 100 lb. Pedal Load	Shoe Clearance	Type of Control	Location of Control	Operation on	
						Type and Material	Diameter	Size (length-width-thickness)		Segments per Shoe		Size (length-width-thickness)									Segments per Shoe
PASSENGER CAR MAKE AND MODEL																					
16																					
AMERICAN MOTORS CORP.																					
8-4000	8.00/14	BDS	N	139.5	60.2	CIA	9	9	7.62-2.00-19	7.62-2.00-19	1	1	8.82-2.00-19	8.82-2.00-19	1	1	635	.015	PH	BL	RB
8-4010 Rambler	8.40/15	WSA	Vac	153.8	60.2	CIA	9	9	7.62-2.25-19	7.62-2.25-19	1	1	8.82-2.50-19	8.82-2.50-19	1	1	635	.015	PH	BL	RB
8-4020	7.50/14	BDS	Vac	167.5	62.4	CIS	10	10	8.90-2.50-19	8.46-1.75-19	1	1	11.08-2.50-19	10.89-1.75-19	1	1	785	.015	FO	BL	RB
8-4080	8.00/14	BDS	Vac	167.5	62.4	CIS	10	10	8.90-2.50-19	8.46-1.75-19	1	1	11.08-2.50-19	10.89-1.75-19	1	1	785	.015	FO	BL	RB
CHRYSLER CORP.																					
8-4000	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4010	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen	11	11	11.50-2.50-20	11.50-2.50-20	1	1	11.50-2.50-20	11.50-2.50-20	1	1	650	NMA	FO	BL	TO
8-4014	8.00/14	BDS	Vac	230.0	60.0	Gen															

Ser-Servo.
SS-Self-erping and self-centering.
TH-^{ing} handle, twist release.
TO-Transmission output shaft.
Vac-Vacuum.
WSA-Water Servo-Action.

N—None.
NMA—No major adjustment required.
PH—Pull handle.
RS—Rear service brakes.
RV—Riveted.

\$S—Cast iron alloy with steel flange.
SS—Composite pressed steel disc and cast iron.
\$—Duo-Servo.
SS—Duo-Servo, self-adjusting.

AL—Below instrument panel, left of steering column.
Bon—Bonded.
Can—Centrifuge.
CI—Cast iron.

OLD—Adjust to light drag and back off seven notches.

ABBREVIATIONS

- April

FRONT SUSPENSION AND STEERING

FRONT SUSPENSION

STEERING

PASSENGER CAR MAKE AND MODEL

17

AMERICAN MOTORS CORP.										CHRYSLER CORP.										FORD MOTOR CO.										GENERAL MOTORS CORP.										STUDEBAKER-PACKARD CORP.																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																			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WHEEL ALIGNMENT, AND REAR SUSPENSION

[illegible]

RAE—Reverse Elliott.
St—Steel.
TB—Teflon bearing.
TC—Torque tube drive.
Th—Threaded.
TR—Tapered roller.
Var—Various.

N—Negative.
NB—Needle bearings.
NW—Nylon thrust washer.
OL—Outrigger mounted leaf springs.
P—Positive.
PC—Pivoted control arms.
PL—Outboard, parallel, longitudinal.

IIK—Integral with kingpin.
 IS—Independent swing type.
 L—Left side.
 Lo—Leaf.
 LI—Link.
 LL—Longitudinal leaf.
 Mon—Monroe.

F-G—Ford or Gabriel.
Fo—Ford.
FW—Friction washer.
Gab—Gabriel.
G-M—Gabriel or Monroe.
HAS—High alloy steel.
Ho—Hotchkiss.

BN—Bushings and needle bearing.
BS—Ball socket.
Bu—Bushings.
Chr—Chrysler.
Co—Coil.
D—Direct acting.
Del—Delco.

ABBREVIATIONS

—Left side only; right side, $\frac{1}{2}N$ to $\frac{1}{2}P$.
S—Alloy steel.
B—Ball bearing.
C—Ball and cone.
—Ball joint.

E • April

FOREIGN PASSENGER CARS....1960

MAKE AND MODEL	Location in Chassis	ENGINE					CHASSIS										Cooling System Capacity (Qt.)	Fuel Tank Capacity (Gal.)							
		Number of Cylinders Bore and Stroke (in.)	Max. Brake Horsepower at Specified R.P.M.	Piston Displacement (Cu. In.)	Compression Ratio (To - 1)	Cylinder Arrangement	Valve Location	TREAD (in.)		OVERALL DIMENSIONS (in.)				Shipping Weight (Lbs.)	Carburetor—No. Used and Type	Cooling Medium			Shifting Method	No. of Forward Speeds	Final Drive Type	Front Suspension Type	Rear Suspension Type	Service Brakes	
								Wheelbase (in.)	Front	Rear	Length Including Bumpers and Bumper Curves	Width	Height—Road to Roof, No Load												Tires (in.)
GREAT BRITAIN																									
A.C.	Ace	Fr	6-2.54x3.90	90-4500	121.4	9.00	IL	IH	90.0	50.0	50.0	151.5	59.5	52.0	5.50/16	1885	3-Ho	W HS	4	Hy	LS	LS	H	8 1/2	15.7
	Ace	Fr	6-2.58x3.74	125-5750	122.0	9.00	IL	IH	90.0	50.0	50.0	151.5	59.5	52.0	5.50/16	1885	3-DD	W HS	4	Hy	LS	LS	H	8 1/2	15.7
	Accra	Fr	6-2.54x3.90	90-4500	121.4	9.00	IL	IH	90.0	50.0	50.0	180.0	61.0	52.0	5.50/16	1840	3-Ho	W HS	4	Hy	LS	LS	H	8 1/2	15.7
	Accra	Fr	6-2.80x3.78	105-4750	121.5	8.50	IL	IH	90.0	50.0	50.0	180.0	61.0	52.0	5.50/16	1840	3-Ho	W HS	4	Hy	LS	LS	H	8 1/2	15.7
	Accra	Fr	6-2.58x3.74	125-5750	122.0	9.00	IL	IH	90.0	50.0	50.0	180.0	61.0	52.0	5.50/16	2173	3-DD	W HS	4	Hy	LS	LS	H	8 1/2	15.7
	Greyhound	Fr	6-2.54x3.90	90-4500	121.4	9.00	IL	IH	100.0	54.0	54.0	180.0	65.3	52.0	5.50/16	2240	3-Ho	W HS	4	Hy	CS	CS	H	8 1/2	15.7
	Greyhound	Fr	6-2.80x3.78	105-4750	121.5	8.50	IL	IH	100.0	54.0	54.0	180.0	65.3	52.0	5.50/16	2240	3-Ho	W HS	4	Hy	CS	CS	H	8 1/2	15.7
	Greyhound	Fr	6-2.58x3.74	125-5750	122.0	9.00	IL	IH	100.0	54.0	54.0	180.0	65.3	52.0	5.50/16	2240	3-DD	W HS	4	Hy	CS	CS	H	8 1/2	15.7
Alvis	TD-21	Fr	6-3.31x3.54	115-4000	182.0	8.50	IL	IH	111.5	54.0	54.1	188.5	68.0	60.0	6.00/15	3300	2-5d	W HA	4	Hy	CS	LS	H	12	17.2
Armstrong-Siddeley	Star Sapphire	Fr	6-3.82x3.54	165-4280	243.5	7.50	IL	IH	114.0	57.0	57.9	194.0	74.0	62.0	6.70/16	3782	2-SD	W Au	3	Hk	CS	LS	H	20	19.2
	Star Limousine	Fr	6-3.82x3.54	140-4000	243.0	7.50	IL	IH	135.0	58.0	58.9	212.0	74.5	67.0	7.60/15	4144	1-Do	W HS	4	Hk	CS	LS	H	20	19.2
Aston Martin	DB4	Fr	6-3.82x3.62	240-5500	223.8	8.25	IL	TO	96.0	54.0	53.5	176.4	66.0	52.0	6.00/16	2912	2-SS	W HS	4	Hk	CS	CS	HV	17	22.9
	DB4-GT	Fr	6-3.82x3.62	302-6000	223.8	9.00	IL	TO	93.0	54.0	53.5	171.4	66.0	52.0	6.00/16	2800	3-DS	W HS	4	Hk	CS	CS	H	17	36.0
Austin	7	Fr	4-2.48x2.88	37-5500	51.7	8.30	IL	IH	80.0	47.8	48.9	120.3	55.5	53.0	5.20/16	1287	1-Sm	W HS	4	(1)	RS	RS	H	6 1/2	6.6
	A55	Fr	4-2.68x3.50	55-4400	90.9	8.30	IL	IH	99.2	48.0	49.9	178.1	63.5	59.0	5.50/14	2307	1-Sm	W HS	4	Hy	CS	LS	H	7 1/2	12.0
	A99	Fr	6-3.28x3.50	112-4780	177.7	8.23	IL	IH	108.0	54.0	53.3	187.5	68.5	60.5	7.00/14	3233	2-5d	W HA	3	Hy	CS	LS	H	12 1/2	19.2
Austin-Healey	3000	Fr	6-3.28x3.50	130-4750	177.7	9.03	IL	IH	91.7	48.8	50.0	157.5	60.0	49.0	5.90/15	2381	2-5d	W HS	4	Hy	CS	LS	H	11 1/2	14.4
Bentley	52	Fr	6-4.10x3.60	380.2	8.00	V	IH	IH	123.0	58.5	60.0	211.8	74.0	64.0	6.20/15	4480	2-SS	W Au	4	Hk	CT	LS	HM	12 1/2	21.6
	Continental 52	Fr	6-4.10x3.60	380.2	8.00	V	IH	IH	123.0	58.5	60.0	212.0	74.5	66.0	6.20/15	4424	2-SS	W Au	4	Hk	CT	LS	HM	12 1/2	21.6
Berkeley	895	Fr	2-2.71x3.51	40-5500	42.2	7.25	IL	IH	70.0	42.5	42.0	125.5	50.0	46.0	5.20/12	924	1-SS	A HS	4	Ch	CS	CS	H	N	6.6
	B105	Fr	2-2.71x3.51	50-6250	42.2	8.00	IL	IH	70.0	42.5	42.0	125.5	50.0	46.0	5.20/12	924	1-SS	A HS	4	Ch	CS	CS	H	N	6.6
	QB95	Fr	2-2.71x3.51	40-5500	42.2	7.25	IL	IH	78.0	46.3	46.0	133.5	54.0	46.0	5.20/12	924	1-3d	A HS	4	Ch	CS	CS	H	N	6.6
	QB105	Fr	2-2.71x3.51	50-6250	42.2	8.00	IL	IH	78.0	46.3	46.0	133.5	54.0	46.0	5.20/12	924	1-3d	A HS	4	Ch	CS	CS	H	N	6.6
Citroen	ID19	Fr	4-3.07x3.93	66-4500	116.5	7.50	IL	IH	123.0	58.0	51.3	189.0	70.5	59.9	6.50/15	2580	1-SD	W HS	4	SB	HP	HP	H	8 1/2	14.0
	DS19	Fr	4-3.07x3.93	75-4500	116.5	7.50	IL	IH	123.0	58.0	51.3	189.0	70.5	59.9	6.50/15	2580	1-DD	W Pr	4	SB	HP	HP	H	9 1/2	14.0
Daimler	Majestic	Fr	6-3.40x4.25	147-4400	231.0	7.50	IL	IH	114.0	56.0	57.0	196.0	73.3	62.3	6.30/16	3084	2-SS	W Au	3	Hk	CS	LS	HV	11 1/2	18.0
	DK400	Fr	6-3.75x4.25	167-3800	281.0	7.00	IL	IH	130.0	60.0	63.0	217.0	77.0	70.0	7.30/16	4992	2-SS	W Pr	4	Hk	CS	LS	HV	18	20.0
	SP250	Fr	6-3.00x2.75	140-5800	152.5	8.20	V	IH	92.0	50.0	48.0	168.0	60.5	51.0	5.50/15	2090	2-Sm	W HS	4	Hk	CS	LS	H	13	14.0
Fairthorne	Atometa	Fr	2-2.77x3.60	34-5750	40.0	8.50	IL	IH	81.0	49.0	48.0	129.0	60.0	48.0	5.20/13	890	1-3d	A HS	4	TT	CS	CS	H	N	6.5
	Atom Major	Fr	4-2.47x2.99	42-5000	57.8	8.50	IL	IH	81.0	49.0	48.0	129.0	60.0	48.0	5.20/13	895	2-SS	W HS	4	TT	CS	CS	H	9	10.0
	Electron Minor	Fr	4-2.48x2.99	42-5000	57.8	8.50	IL	IH	81.0	49.0	48.0	129.0	60.0	48.0	5.20/13	895	2-SS	W HS	4	TT	CS	CS	H	9	10.0
	Electron	Fr	4-2.85x2.63	93-6800	67.0	10.50	IL	OC	82.0	49.0	45.5	138.0	58.0	46.0	5.20/15	1150	2-SS	W HS	4	TT	CS	CS	H	6	10.0
	Zeta	Fr	6-3.25x3.13	158.0	8.00	IL	IH	IH	107.0	53.0	52.0	180.0	68.0	60.0	6.40/13	2582	1-SD	W HS	4	TT	CS	CS	H	10	10.0
Ford	Escort	Fr	4-2.80x3.64	38-4500	71.0	7.00	IL	L	87.0	46.0	47.5	146.4	60.0	52.0	5.50/13	1781	1-SD	W HS	3	Hk	CS	LS	H	6	8.4
	Prescot	Fr	4-3.19x1.91	41-5000	60.8	8.00	IL	IH	87.0	46.0	47.5	149.7	60.0	58.0	5.20/13	1715	1-SD	W HS	4	Hk	CS	LS	H	5 1/2	8.4
	Anglia	Fr	4-3.19x1.91	41-5000	60.8	8.00	IL	OC	90.5	46.0	45.4	153.9	57.3	56.0	5.20/13	1660	1-SD	W HS	4	Hk	CS	LS	H	5 1/2	8.4
	Conquest Mk. II	Fr	4-3.25x3.13	61-4400	103.9	7.50	IL	IH	104.5	53.0	52.0	171.8	60.0	50.0	5.50/13	2387	1-SD	W HS	3	Hk	CS	LS	H	9	11.7
	Zephyr Mk. II	Fr	6-3.25x3.13	90-4400	155.0	7.80	IL	IH	107.0	53.0	52.0	179.0	68.0	60.0	6.40/13	2582	1-SD	W HS	3	Hk	CS	LS	H	11	12.7
	Zodiac	Fr	6-3.25x3.13	90-4400	155.0	7.80	IL	IH	107.0	53.0	52.0	180.0	68.0	60.0	6.40/13	2524	1-SD	W HS	3	Hk	CS	LS	H	11	12.7
Frazer-Nash	Continental	Fr	6-3.23x2.95	173-5000	193.3	8.20	V	IH	100.0	50.0	53.5	186.5	60.0	52.0	6.00/16	2800	2-SS	W HS	4	SB	CS	TB	H	11 1/2	16.1
	Continental	Fr	6-2.91x2.95	140-4800	157.3	7.80	V	IH	100.0	50.0	53.5	186.5	60.0	52.0	6.00/16	2800	2-SS	W HS	4	SB	CS	TB	H	11 1/2	16.1
Hillman	Minx Series IIIA	Fr	4-3.11x3.00	57-4800	91.2	8.50	IL	IH	96.0	49.0	48.5	162.7	60.0	50.5	5.50/15	2172	1-SD	W HS	4	TT	CS	LS	H	6 1/2	7.3
Hummer	Hawk Series IA	Fr	4-3.19x4.33	78-4400	138.2	7.50	IL	IH	110.0	56.0	55.5	184.9	68.5	61.0	6.00/15	2800	1-Do	W HA	5	Hk	CS	LS	H	12 1/2	15.0
	Super Snipe Series II	Fr	6-3.44x3.25	130-4800	180.9	8.00	IL	IH	110.0	56.0	55.5	184.9	68.5	61.0	6.00/15	2800	1-Do	W HA	5	Hk	CS	LS	HV	15 1/2	15.0
Jaguar	Mark 2	Fr	6-3.27x3.01	120-5750	181.5	8.00	IL	OC	107.4	55.0	53.4	186.0	66.0	57.5	6.40/15	3089	2-SD	W HA	4	Hk	CS	LS	HV	12	14.5
	Mark 2	Fr	6-3.27x3.01	210-5800	210.0	8.00	IL	OC	107.4	55.0	53.4	186.0	66.0	57.5	6.40/15	3169	2-SS	W HA	4	Hk	CS	LS	HV	13 1/2	14.5
	Mark 2	Fr	6-3.43x4.17	220-5500	230.6	8.00	IL	OC	107.4	55.0	53.4	186.0	66.0	57.5	6.40/15	3109	2-SS	W HA	4	Hk	CS	LS	HV	13 1/2	14.5
	XK 150	Fr	6-3.27x4.17	210-5500	210.0	8.00	IL	OC	102.0	51.0	51.6	177.0	64.5	55.0	6.00/16	3090	2-SS	W HA	4	Hk	TB	LS	HV	13 1/2	16.8
	XK 150	Fr	6-3.43x4.17	220-5500	230.6	8.00	IL	OC	102.0	51.0	51.6	177.0	64.5	55.0	6.00/16	3090									

1960 FOREIGN PASSENGER CARS

MAKE AND MODEL	ENGINE										CHASSIS										Cooling System Capacity (Qt.)	Fuel Tank Capacity (Gal.)					
	Location in Chassis	Number of Cylinders Bore and Stroke (in.)	Max. Brake Horsepower at Specified R.P.M.	Piston Displacement (Cu. In.)	Compression Ratio (To - 1)	Cylinder Arrangement	Valve Location	Wheelbase (in.)	TREAD (in.)		OVERALL DIMENSIONS (in.)				Shipping Weight (Lbs.)	Carburetor—No. Used and Type	Cooling Medium	Shifting Method	No. of Forward Speeds	Final Drive Type			Front Suspension Type	Rear Suspension Type	Service Brakes		
									Front	Rear	Length Including Bumpers and Bumper Guards	Width	Height—Road to Roof, No Load	Tires (in.)													
GREAT BRITAIN—continued																											
Morris.....Mini-Minor	Fr	4-2.48x2.09	34-5500	51.7	8.30	IL	IH	80.0	47.8	45.9	120.0	55.0	53.0	5.20/10	.....	1-Sm	W	HS	4	(2)	RS	RS	H	H	2 1/2	5.5	
.....Oxford Series V	Fr	4-2.68x3.50	53-4350	90.9	8.30	IL	IH	99.2	48.9	49.9	178.1	63.5	59.8	5.90/14	.....	1-S5m	W	HS	4	Hk	CS	CS	LS	LS	8 1/2	10.0	
.....1000	Fr	4-2.48x2.96	37-4800	57.8	8.30	IL	IH	86.0	50.6	50.3	148.0	61.0	60.0	5.00/14	1708	1-Sm	W	HS	4	Hy	TB	LS	H	H	5	7.8	
Noble.....200	R	1-2.26x2.28	10-5250	9.1	6.30	S	N	80.0	48.0	18.0	120.5	46.6	46.3	4.40/8	683	1-Sd	A	HS	4	Ch	CL	CS	M	N	3.8		
Peerless.....GT2 Phase II	R	4-3.27x3.62	100-5000	121.5	8.00	IL	IH	94.5	50.0	50.0	162.0	63.0	60.0	5.50/15	2350	2-Sd	W	HS	4	Hk	.....	LS	H	H	7	14.5	
Riley.....4/68	Fr	4-2.88x3.50	64-5000	90.9	8.30	IL	IH	99.2	48.9	49.9	178.1	63.5	59.8	5.90/14	.....	2-Sm	W	HS	4	Hk	CS	CS	LS	H	H	6 1/2	10.0
Rolls Royce.....Silver Cloud II	Fr	6-4.10x3.60	.....	380.2	8.00	V	IH	123.0	58.5	60.0	211.8	74.8	64.0	8.20/15	4480	2-S5	W	Au	4	Hk	CT	LS	HM	12 1/2	21.6		
.....Phantom V	Fr	6-4.10x3.60	.....	380.2	8.00	V	IH	145.0	60.9	64.0	230.0	79.0	69.0	8.90/15	5411	2-S5	W	Au	4	Hk	CT	LS	HM	13 1/2	28.8		
Rover.....80	Fr	4-3.56x3.50	77-4250	139.2	7.00	IL	F	111.0	52.5	51.5	178.3	65.6	63.8	6.40/15	3186	1-S5	W	HS	5	Hk	CS	CS	LS	H	H	8 1/2	11.5
.....100	Fr	6-3.06x3.63	104-4750	160.3	7.80	IL	F	111.0	52.5	51.5	178.3	65.6	63.8	6.40/15	3241	1-S5	W	HS	5	Hk	CS	CS	LS	H	H	11 1/2	11.5
.....3 Litre	Fr	6-3.06x4.13	115-4500	182.7	8.75	IL	F	110.5	55.0	56.0	186.5	70.0	66.3	6.70/15	3490	1-SD	W	HA	4	Hk	TB	LS	HV	11 1/2	14.0		
Singer.....Gazelle Series III A	Fr	4-3.11x3.00	64-4600	91.2	8.50	IL	IH	96.0	49.0	48.5	163.5	60.8	59.5	5.60/15	2226	2-SD	W	HS	4	TT	CS	LS	H	H	6 1/2	10.0	
Standard.....Pennant	Fr	4-2.48x2.99	37-5000	57.8	8.00	IL	IH	84.0	48.5	48.5	147.0	58.0	58.0	5.60/13	.....	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	4 1/2	8.4
.....Ensign	Fr	4-2.99x3.62	60-4000	102.0	8.00	IL	IH	102.0	51.5	51.5	171.5	67.5	66.0	5.90/15	.....	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	8 1/2	14.4
.....Vanguard III Estate Car	Fr	4-3.35x3.62	68-4000	127.6	7.50	IL	IH	102.0	51.0	51.0	170.5	67.5	66.0	5.50/16	.....	1-SD	W	HA	3	Hk	CS	CS	LS	H	H	8 1/2	14.4
.....Vanguard III	Fr	4-3.35x3.62	68-4000	127.6	7.50	IL	IH	102.0	51.5	51.5	172.0	67.5	66.0	5.90/15	.....	1-SD	W	HA	3	Hk	CS	CS	LS	H	H	8 1/2	14.4
Sunbeam.....Rapier Series III	Fr	4-3.11x3.00	78-5400	91.2	9.20	IL	IH	96.0	48.8	48.5	162.5	60.8	58.0	5.60/15	2250	2-SD	W	HS	4	TT	CS	LS	H	H	6 1/2	10.0	
.....Alpine	Fr	4-3.11x3.00	84-5300	91.2	9.20	IL	IH	96.0	51.0	48.5	185.3	60.8	51.5	5.60/13	2085	2-SD	W	HS	4	TT	CS	LS	H	H	7	9.0	
Triumph.....Sedan	Fr	4-2.48x2.99	39-4500	57.8	8.00	IL	IH	84.0	48.5	48.5	142.0	58.0	59.0	5.20/13	1620	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	4 1/2	8.4
.....Station Wagon	Fr	4-2.48x2.99	39-4500	57.8	8.00	IL	IH	84.0	48.5	48.5	147.0	58.0	59.0	5.60/13	1710	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	4 1/2	8.4
.....Herald Saloon	Fr	4-2.48x2.99	39-4500	57.8	8.00	IL	IH	91.5	48.0	48.0	153.0	60.0	52.0	5.20/13	1680	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	6	8.4
.....Herald Coupe	Fr	4-2.48x2.99	51-6000	57.8	8.50	IL	IH	91.5	48.0	48.0	153.0	60.0	51.3	5.20/13	1650	2-S5	W	HS	4	Hk	CS	CS	LS	H	H	8	8.4
.....TR-3	Fr	4-3.27x3.62	100-5000	121.5	8.50	IL	IH	88.0	45.0	45.0	151.0	55.5	55.0	5.50/15	2010	2-SD	W	HS	4	Hk	CS	CS	LS	H	H	8 1/2	14.4
Vauxhall.....Victor FD	Fr	4-3.13x3.00	55-4200	92.0	7.80	IL	IH	96.0	50.0	50.0	167.8	63.5	58.0	5.60/13	2130	1-SD	W	HS	3	Hk	CS	CS	LS	H	H	7 1/2	9.6
.....Victor FW	Fr	4-3.13x3.00	55-4200	92.0	7.80	IL	IH	96.0	50.0	50.0	167.8	63.5	58.3	5.90/13	2271	1-SD	W	HS	3	Hk	CS	CS	LS	H	H	7 1/2	9.6
Wolseley.....15/60	Fr	4-2.88x3.50	53-4350	90.9	8.30	IL	IH	99.2	48.9	49.9	178.1	63.5	59.8	5.90/14	.....	1-Sm	W	HS	4	Hk	CS	CS	LS	H	H	6 1/2	10.0
.....6/99	Fr	6-3.20x3.50	112-4750	177.7	8.23	IL	IH	108.0	53.8	53.3	188.0	66.5	60.0	7.00/14	.....	2-Sm	W	HA	3	Hk	CS	CS	LS	H	H	10 1/2	16.0
AUSTRALIA																											
Holden.....FC	Fr	6-3.00x3.13	72-4400	132.5	7.00	IL	IH	105.0	54.5	54.5	176.0	66.9	62.9	6.40/13	2390	1-SD	W	HS	3	Hk	CS	CS	LS	H	H	9 1/2	11.4
AUSTRIA																											
Steyr-Puch.....500-D	R	2-2.72x2.50	16-4600	30.0	8.80	Op	IH	71.7	43.6	44.3	115.8	51.5	53.5	4.90/12	.....	1-De	A	HS	4	.....	LS	CS	H	N	5.3		
.....500-DL	R	2-2.72x2.50	20-4600	30.0	8.80	Op	IH	71.7	43.6	44.3	115.8	51.5	53.5	4.90/12	.....	1-De	A	HS	4	.....	LS	CS	H	N	5.3		
CZECHOSLOVAKIA																											
Skoda.....Octavia	Fr	4-2.88x2.95	40-4200	66.6	7.00	IL	IH	94.5	47.6	49.2	160.0	63.0	56.3	5.00/15	1962	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	6 1/2	8.0
.....Octavia Super	Fr	4-2.83x2.95	45-4200	73.3	7.00	IL	IH	94.5	47.6	49.2	160.0	63.0	56.3	5.00/15	1962	1-SD	W	HS	4	Hk	CS	CS	LS	H	H	6 1/2	8.0
.....Felicia	Fr	4-2.68x2.95	50-5500	66.6	8.40	IL	IH	94.5	47.6	49.2	160.0	63.0	56.3	5.00/15	1964	2-SD	W	HS	4	Hk	CS	CS	LS	H	H	6 1/2	8.0
Tatra.....603	R	8-2.95x2.76	95-4800	151.0	6.50	V	IH	108.3	55.6	55.1	200.0	75.2	61.0	6.70/15	.....	2-DD	A	HS	4	SB	CS	CS	H	N	14.5		
FRANCE																											
Alpine.....A-108-850	R	4-2.26x3.12	40-5200	56.0	8.80	IL	IH	82.5	48.0	48.0	145.0	76.6	48.7	5.00/15	.....	1-SD	W	HS	4	Hk	CS	CS	H	H	4 1/2	28.0	
.....A-108-804	R	4-2.34x3.12	63-6250	54.0	9.80	IL	IH	82.5	48.0	48.0	145.0	76.6	48.7	5.00/15	.....	1-DD	W	HS	5	Hk	CS	CS	H	H	4 1/2	28.0	
Arista.....Sport 2/4	Fr	4-3.21x2.92	42-5000	52.0	.....	Op	IH	94.4	.....	.....	173.0	65.6	56.0	5.50/15	1853	2-.....	A	HS	4	.....	.....	.....	H	.....	8.9		
Citroen.....2CV	Fr	2-2.58x2.42	13-4200	25.8	7.00	Op	OC	94.3	49.5	49.5	146.5	56.2	63.0	5.30/15	1075	1-SD	A	HS	4	.....	CS	CS	H	N	4.4		
.....ID19P	Fr	4-3.04x3.90	66-4500	116.5	7.50	IL	OC	123.0	59.0	51.0	188.5	70.4	57.7	6.50/15	2420	1-SD	W	HS	4	.....	TH	TH	H	H	8 1/2	14.3	
.....ID19F	Fr	4-3.04x3.90	66-4500	116.5	7.50	IL	OC	123.0	59.0	51.0	188.5	70.4	60.0	6.50/15	2760	1-SD	W	HS	4	.....	TH	TH	H	H	8 1/2	14.3	
.....DS19	Fr	4-3.04x3.90	78-4500	116.5	7.50	IL	OC	123.0	59.0	51.0	188.5	70.4	57.7	6.50/15	2510	1-DD	W	HS	4	.....	TH	TH	H	H	9 1/2	14.3	
Deutsch-Bennot.....Le Mans	Fr	2-3.32x2.92	57-5700	52.2	8.10	Op	IH	93.8	50.7	50.7	162.0	63.5	49.1	5.60/15	.....	1-Du	A	HS	4	.....	LS	TB	H	N	13.2		
.....HBR-6	Fr	2-3.32x2.92	57-5700	52.2	8.10	Op	IH	106.5	47.5	47.5	165.0	61.6	49.1	5.60/15	.....	1-Du	A	HS	4	.....	LS	TB	H	N	13.2		
Facel-Vega.....Facelina	Fr	4-3.22x3.07	115-6400	100.0	9.40	IL	OC	96.5	51.3	51.3	163.5	64.0	48.5	5.90/14	2180	2-Du	W	HS	4	Hk	CS	CS	LS	H	H	12	16.0
.....Excellence	Fr	8-4.25x3.38	335-4600	383.0	10.00	V	IH	124.8	55.9	57.0	206.7	72.0	54.5	7.00/15	4230	1-Fb	W	Au	3	Hk	CS	CS	LS	H	H	26	33.0
.....HK-500	Fr	8-4.25x3.38	360-5200	383.0	10.00	V	IH	104.7	55.9	57.0	181.1	70.8	53.0	6.70/15	3585	1-Fb	W	HS	4	Hk	CS	CS	LS	H	H	26	27.9
Panhard.....PL-17	Fr	2-3.35x2.95	50-5000	51.9	7.20	Op	IH	101.0	51.3	51.3	180.0	63.0	57.5	5.00/15	.....	1-SD	A	HS	4	.....	LS	TB	H	N	11.0		

FOREIGN PASSENGER CARS—continued

MAKE AND MODEL	Location in Chassis	ENGINE					CHASSIS												Cooling System Capacity (Qt.)	Fuel Tank Capacity (Gal.)						
		Number of Cylinders Bore and Stroke (In.)	Max. Brake Horsepower at Specified R.P.M.	Piston Displacement (Cu. In.)	Compression Ratio (To - 1)	Cylinder Arrangement	Valve Location	Wheelbase (In.)	TREAD (In.)		OVERALL DIMENSIONS (In.)				Shipping Weight (Lbs.)	Carburetor—No. Used and Type	Cooling Medium	Shifting Method			No. of Forward Speeds	Final Drive Type	Front Suspension Type	Rear Suspension Type	Service Brakes	
									Front	Rear	Length Including Bumpers and Bumper Guards	Width	Height—Road to Roof, No Load	Tires (In.)												
FRANCE—continued																										
Peugeot.....403	Fr	4-3.13x2.68	65-4750	89.6	7.50	IL	IH	105.0	52.8	52.0	176.0	66.0	59.3	5.80/15	2262	1-SD	W	HS	4	TT	LS	CS	H	9 1/2	13.3	
Renault.....R-1062	R	4-2.15x3.15	28-4500	45.6	7.75	IL	IH	82.7	48.0	48.1	143.2	56.3	57.9	5.00/15	1268	1-De	W	HS	3	SA	CT	CS	H	4 1/4	7.3	
.....R-1090 Dauphine	R	4-2.28x3.15	32-4250	51.6	8.00	IL	IH	89.5	49.2	48.0	156.9	60.0	56.8	5.00/15	1448	1-De	W	HS	3	SA	CT	CS	H	4 1/4	8.5	
.....R-1091 Dauphine Gordini	R	4-2.28x3.15	40-5000	51.6	8.00	IL	IH	89.5	49.2	48.0	156.9	60.0	56.9	5.50/15	1462	1-De	W	HS	4	SA	CT	CS	H	4 1/4	8.5	
.....R-1092 Caravelle	R	4-2.28x3.15	40-5000	51.6	8.00	IL	IH	89.5	49.2	48.0	170.1	61.8	54.5	5.50/15	1620	1-De	W	HS	3	SA	CT	CS	H	4 1/4	8.5	
.....R-1103 Fregate	Fr	4-3.46x3.46	77-4000	130.7	7.00	IL	IH	110.2	55.1	55.1	184.8	67.7	63.0	6.40/15	2822	1-De	W	HS	4	De	CT	CT	H	9 1/2	15.3	
.....R-1104 Transfluide	Fr	4-3.46x3.46	80-4000	130.7	7.50	IL	IH	110.2	55.1	55.1	184.8	67.7	63.0	6.40/15	2976	1-De	W	HS	3	De	CT	CT	H	9 1/2	15.3	
Simca.....																										
.....Aronde P-60 Etoile 8CV	Fr	4-2.67x2.95	40-4600	86.5	7.10	IL	IH	96.3	49.4	49.2	164.8	61.5	56.0	5.60/14	1988	1-SD	W	HS	4	Hk	CT	CL	H	5 1/4	9.5	
.....Aronde P-60 Flash Engine	Fr	4-2.91x2.95	52-.....	78.7	7.10	IL	IH	96.3	49.4	49.2	164.8	61.5	56.0	5.60/14	1988	1-SD	W	HS	4	Hk	CT	CL	H	5 1/4	9.5	
.....Arlane	Fr	4-2.91x2.95	52-.....	78.7	7.10	IL	IH	108.0	54.0	53.0	178.0	68.1	56.3	6.50/15	.....	1-SD	W	HS	4	Hk	CT	LS	H	5 1/4	13.2
.....Aronde P-60 Flash Special Engine	Fr	4-2.91x2.95	60-.....	78.7	8.00	IL	IH	96.3	49.4	49.2	164.8	61.5	56.0	5.60/14	1988	1-SD	W	HS	4	Hk	CT	CL	H	5 1/4	9.5	
.....Vedette	Fr	6-2.60x3.31	84-4800	143.4	7.50	V	L	106.9	53.9	52.7	187.0	69.1	67.0	6.50/15	2690	1-DD	W	HS	3	Hk	CT	LS	H	19	13.2	
GERMANY																										
Auto Union.....1000	Fr	3-2.91x2.99	44-4500	59.9	7.25	IL	N	82.0	50.8	53.0	165.1	66.7	57.7	5.60/15	.....	1-De	W	HS	4	BG	LS	LS	H	8 1/2	12.0
.....1000-S	Fr	3-2.91x2.99	50-4500	59.9	7.25	IL	N	82.0	50.8	53.0	165.1	66.7	57.7	5.60/15	.....	1-De	W	HS	4	BG	LS	LS	H	8 1/2	12.0
.....1000 Sp.	Fr	3-2.91x2.99	55-4500	59.9	8.00	IL	N	82.0	50.8	53.0	164.0	66.0	52.0	6.00/15	.....	1-DD	W	HS	4	BG	LS	LS	H	8	13.5
BMW.....600	R	2-2.91x2.67	23-4000	35.7	6.80	Op	IH	86.9	47.5	46.2	114.0	55.2	54.0	5.20/10	1215	.....	A	HS	4	BQ	CS	CS	H	N	6.0
.....700	R	2-3.07x2.87	35-5200	42.5	7.50	Op	IH	83.5	50.0	47.3	139.2	58.3	53.0	5.20/12	1410	1-SD	A	HS	4	BQ	CS	CS	H	N	8.7	
.....2.0	Fr	2-2.91x2.95	110-4800	157.0	7.00	V	IH	111.6	52.3	55.7	186.0	70.0	60.0	6.40/15	3010	1-DD	W	HS	4	TT	TB	TB	H	9 1/2	18.0	
.....2.6	Fr	2-2.91x2.95	115-4800	157.0	7.00	V	IH	111.6	52.3	55.7	186.0	70.0	60.0	6.40/15	3110	1-DD	W	HS	4	TT	TB	TB	H	9 1/2	18.0	
.....2.6 Luxus	Fr	3-2.22x2.95	140-4800	193.0	7.20	V	IH	111.6	52.3	55.7	186.0	70.0	60.0	6.40/15	3220	1-DD	W	HS	4	TT	TB	TB	H	9 1/2	18.0	
.....3.2	Fr	3-2.22x2.95	162-4800	193.0	7.30	V	IH	111.6	52.3	55.7	186.0	70.0	60.0	6.50/15	3240	2-DD	W	HS	4	TT	TB	TB	H	10 1/2	18.0	
.....3.2 Super	Fr	3-2.22x2.95	162-4800	193.0	7.30	V	IH	111.6	52.3	55.7	187.0	67.0	56.4	6.00/16	3310	2-DD	W	HS	4	TT	TB	TB	H	10 1/2	18.0	
.....503	Fr	3-2.22x2.95	173-5000	193.0	7.80	V	IH	87.5	56.6	56.0	172.4	66.0	50.0	6.00/16	2825	2-DD	W	HS	4	TT	TB	TB	H	10 1/2	18.0	
.....507	Fr	3-2.22x2.95	173-5000	193.0	7.80	V	IH	87.5	56.6	56.0	172.4	66.0	50.0	6.00/16	2825	2-DD	W	HS	4	TT	TB	TB	H	10 1/2	18.0	
Borgward.....Isabella	Fr	4-2.92x3.30	80-4700	91.0	7.70	IL	IH	101.3	52.5	53.5	171.5	68.0	56.5	5.90/13	2180	1-SD	W	HS	4	TT	CS	CS	H	8 1/4	10.1	
.....Isabella TS	Fr	4-2.92x3.30	75-5400	91.0	8.35	IL	IH	101.3	52.5	53.5	171.5	68.0	56.5	5.90/13	2180	1-DD	W	HS	4	TT	CS	CS	H	8 1/4	10.1	
.....Combi	Fr	4-2.92x3.30	80-4700	91.0	7.00	IL	IH	101.3	52.5	53.5	171.5	68.0	56.5	5.40/13	2410	1-SD	W	HS	4	TT	CS	CS	H	8 1/4	10.1	
.....Coupe	Fr	4-2.92x3.30	75-5400	91.0	8.20	IL	IH	101.3	52.5	53.5	171.5	67.5	53.0	5.90/13	2380	1-DD	W	HS	4	TT	CS	CS	H	8 1/4	10.1	
.....Big Six	Fr	6-2.96x3.33	100-.....	136.5	8.20	IL	IH	101.3	52.5	53.5	171.5	68.0	56.5	5.75/13	.....	1-DD	W	HS	4	.....	CS	CS	H	4	12.1
Denzel.....1300 Serien Super	Fr	4-3.04x2.61	65-5400	78.1	8.50	Op	IH	81.9	51.1	51.1	142.4	.....	46.8	5.60/15	1389	2-DD	A	HS	4	.....	TB	TB	H	N	12.1
DKW.....1980	Fr	3-2.66x2.66	34-4300	45.0	8.00	IL	N	85.7	46.5	47.3	156.9	62.0	55.0	5.20/12	.....	1-De	W	HS	4	BG	TB	TB	HM	7 1/2	9.3
FMR.....KR-200, KR-201	R	1-2.54x2.26	10-5000	11.7	6.60	S	N	80.0	43.0	42.0	118.0	49.0	47.0	4.40/8	506	1-SD	A	HS	4	.....	RT	RT	M	N	3.6
.....TG-500	R	2-2.62x2.73	20-5000	30.2	6.50	IL	N	74.9	44.0	41.0	120.0	51.0	50.0	4.40/10	770	1-De	A	HS	4	.....	RT	RT	M	N	7.9
Ford.....Taunus 12M	Fr	4-2.50x3.64	43-4250	71.5	7.40	IL	L	96.0	48.0	48.0	160.0	61.8	60.0	5.60/13	1650	1-SD	W	HS	3	Hk	CT	LS	H	7 1/2	9.0	
.....Taunus 12M Super	Fr	4-3.23x2.79	60-4500	91.4	6.80	IL	IH	96.0	48.0	48.0	160.0	61.8	60.0	5.60/13	1906	1-SD	W	HS	3	Hk	CT	LS	H	7 1/2	9.0	
.....Taunus 17M	Fr	4-3.31x3.02	67-4400	103.6	7.20	IL	IH	102.5	50.0	50.0	172.3	65.7	59.1	5.90/13	2150	1-SD	W	HS	3	Hk	CT	LS	H	8 1/2	11.9	
Goggomobil.....Regent T300	R	2-.....	15-5000	17.8	.....	IL	.....	70.8	.....	.....	114.2	50.4	51.5	4.40/10	926	.....	A	.....	4	.....	CS	CS	H	N	6.6
.....Regent T400	R	2-2.64x2.21	20-5000	23.9	6.00	IL	N	70.8	.....	.....	114.2	50.4	51.5	4.40/10	926	1-SS	A	.....	4	.....	CS	CS	H	N	6.6
.....Mayfair T3300	R	2-.....	15-5000	17.8	.....	IL	.....	70.8	.....	.....	119.5	53.9	48.4	4.90/10	1060	.....	A	Pr	4	.....	CS	CS	H	N	6.6
.....Mayfair T5400	R	2-2.64x2.21	20-5000	23.9	6.00	IL	N	70.8	42.0	42.0	119.5	53.9	48.4	4.90/10	1000	1-SS	A	Pr	4	.....	CS	CS	H	N	6.6
.....Royal T780	Fr	2-3.04x2.81	30-4800	41.6	7.20	Op	IH	78.7	47.2	45.6	134.8	57.9	54.3	4.80/12	1330	2-SD	A	HS	4	.....	CS	LS	H	N	10.5
Goliath.....1100 Sed., Deluxe, Cpe.	Fr	4-2.91x2.52	40-4250	66.5	7.30	Op	.....	89.4	50.3	51.6	161.0	64.2	57.1	5.60/13	1894	1-Ho	W	HA	4	BG	LS	LS	H	8 1/2	12.0
.....1100 Deluxe, Coupe	Fr	4-2.91x2.52	55-5000	66.5	7.90	Op	.....	89.4	50.3	51.6	161.0	64.2	53.5	5.60/13	2094	2-Ho	W	HA	4	BG	LS	LS	H	8 1/2	12.0
Isetta.....200	R	1-2.83x2.37	16-5200	18.0	7.00	S	IH	58.0	47.3	20.5	92.6	54.3	52.7	4.80/10	772	.....	A	HS	4	Ch	CS	LS	H	N	3.4
Lloyd.....Alexander	Fr	2-3.03x2.52	24-4500	36.4	6.80	IL	OC	78.8	41.4	43.4	132.0	55.5	55.1	4.25/15	1240	1-SD	A	HS	4	.....	LS	CS	H	N	6.6
.....Alexander TS	Fr	2-3.03x2.52	29-5000	36.4	7.20	IL	OC	78.8	41.4	43.4	132.0	55.5	55.1	4.25/15	1240	1-SD	A	HS	4	.....	LS	CS	H	N	6.6
.....Arabella	Fr	4-2.72x2.36	42-4800	54.8	7.50	Op	IH	86.0	47.3	47.3	149.6	59.4	54.9	5.20/13	1530	1-SD	W	HS	4	.....	CS	CS	H	8 1/2	9.3
.....Arabella Coupe	Fr	4-2.72x2.36	.....	54.8	.....	Op	IH	86.0	47.3	47.3	.....	.....	.....	5.20/13	.....	.....	W	HS	4	.....	CS	CS	H	8 1/2	9.3
Mercedes-Benz.....180	Fr	4-3.32x3.26	78-4500	115.8	7.00	IL	OC	104.3	56.3	58.0	177.2	68.5	61.4	6.40/13	2390	1-SD	W	HS	4	Hy	CS	CS	H	8	12.3	
.....180D	Fr	4-2.92x3.90	46-3500	107.8	19.00	IL	IH	104.3	56.3	58.0																

FOREIGN PASSENGER CARS—continued

MAKE AND MODEL	ENGINE										CHASSIS										Cooling System Capacity (Qt.)	Fuel Tank Capacity (Gal.)			
	Location in Chassis	Number of Cylinders Bore and Stroke (In.)	Max. Brake Horsepower at Specified R.P.M.	Piston Displacement (Cu. In.)	Compression Ratio (To - 1)	Cylinder Arrangement	Valve Location	TREAD (In.)		OVERALL DIMENSIONS (In.)				Shipping Weight (Lbs.)	Carburetor—No. Used and Type	Ceiling Medium	Shifting Method	No. of Forward Speeds	Final Drive Type	Front Suspension Type			Rear Suspension Type	Service Brakes	
								Wheelbase (In.)	Front	Rear	Length Including Bumpers and Bumper Guards	Width	Height—Road to Roof, No Load												Tires (In.)
GERMANY—continued																									
NSU.....Prinz II	R	2-2.92x2.57	26-4800	34.5	6.80	IL	OC	78.7	46.8	46.8	126.0	55.4	53.8	1107	1-SD	A	HS	4	Hk	CS	CS	H	N	6.6	
.....Prinz 30	R	2-2.92x2.57	36-5500	34.5	8.50	IL	OC	78.7	46.8	46.8	126.0	55.4	53.5	1107	1-SD	A	HS	4	Hk	CS	CS	H	N	6.6	
.....Sport Prinz	R	2-2.92x2.57	36-5500	34.5	8.50	IL	OC	78.7	46.8	46.8	141.7	59.2	49.2	1171	1-SD	A	HS	4	Hk	CS	CS	H	N	6.6	
Opel.....Olympia	Fr	4-3.15x2.91	56-4700	90.8	7.00	IL	IH	100.0	49.6	50.0	174.0	63.8	58.7	1922	1-SD	W	HS	3	Hk	CS	LS	H	8	10.6	
.....Kapitan	Fr	6-3.35x3.01	90-4300	159.0	7.00	IL	IH	110.2	54.3	54.1	189.9	71.3	59.5	2773	1-SD	W	HS	3	Hk	CS	LS	H	10½	14.5	
Porsche.....356B/1600	R	4-3.25x2.91	60-4500	96.5	7.50	Op	OC	82.7	51.4	50.1	157.7	65.6	51.7	1900	1-DD	A	HS	4	TT	TB	TB	H	N	13.7	
.....356B/1600S	R	4-3.25x2.91	75-5000	96.5	8.50	Op	OC	82.7	51.4	50.1	157.7	65.6	51.7	1900	1-DD	A	HS	4	TT	TB	TB	H	N	13.7	
.....356B/1600S-90	R	4-3.25x2.91	90-5500	96.5	9.00	Op	OC	82.7	51.4	50.1	157.7	65.6	51.7	1900	1-DD	A	HS	4	TT	TB	TB	H	N	13.7	
Volkswagen.....11	R	4-3.03x2.52	36-3700	72.7	6.50	Op	IH	94.5	51.4	50.7	160.6	60.6	50.1	1545	1-SD	A	HS	4	SB	TB	TB	H	N	10.6	
.....14	R	4-3.03x2.52	36-3700	72.7	6.60	Op	IH	94.5	51.4	50.7	163.0	64.3	52.4	1720	1-SD	A	HS	4	SB	TB	TB	H	N	10.6	
.....22	R	4-3.03x2.52	36-3700	72.7	6.60	Op	IH	94.5	53.9	53.5	168.9	68.9	76.5	2380	1-SD	A	HS	4	SB	TB	TB	H	N	10.6	
Wartburg.....Four Door Sedan	Fr	3-2.76x3.07	43-4500	55.0	6.80	IL	N	96.5	47.0	49.5	169.3	62.0	58.0	2120	1-SS	W	HS	4	.....	LS	LS	H	8	10.0	
.....Two Door Sedan	Fr	3-2.76x3.07	43-4500	55.0	6.80	IL	N	96.5	47.0	49.5	169.3	62.0	58.0	2120	1-SS	W	HS	4	.....	LS	LS	H	8	10.0	
.....Station Wagon	Fr	3-2.76x3.07	43-4500	55.0	6.80	IL	N	96.5	47.0	49.5	169.3	62.0	58.0	2120	1-SS	W	HS	4	.....	LS	LS	H	8	10.0	
.....Sport Roadster	Fr	3-2.76x3.07	55-4500	55.0	6.80	IL	N	96.5	47.0	49.5	171.7	63.4	53.2	2134	1-SS	W	HS	4	.....	LS	LS	H	8	10.0	
HOLLAND																									
DAF.....600	Fr	2-3.00x2.50	22-4000	36.0	7.10	Op	IH	61.0	46.5	46.5	142.0	57.0	54.5	1365	1-SD	A	Au	.....	LS	CS	H	N	6.3		
IRELAND																									
Heinkel.....T154	R	1-2.50x2.40	10-5500	11.8	6.80	S	IH	68.8	47.8	8.6	100.0	53.8	51.8	4.40/10	626	1-SD	A	HS	4	Ch	CS	CS	HM	N	3.3
ITALY																									
Abarth.....500	R	2-2.83x2.73	33-5200	30.4	8.60	IL	IH	71.7	43.7	43.7	125.0	51.5	44.1	5.00/12	925	1-SD	A	HS	4	.....	LS	CS	.....	N	4.5
.....750 Custom	R	4-2.38x2.50	42-5500	45.6	9.00	IL	IH	78.0	44.9	45.3	128.3	53.7	55.2	5.20/12	1314	1-SD	W	HS	4	.....	LS	CS	.....	N	5.9
.....750 Coupe	R	4-2.38x2.50	44-5800	45.6	9.80	IL	IH	78.0	44.9	45.3	135.8	52.3	46.4	5.20/12	1180	1-SD	W	HS	4	.....	LS	CS	.....	N	6.6
.....750 Spyder	R	4-2.38x2.50	44-5800	45.6	9.80	IL	IH	78.0	44.9	45.3	137.5	54.2	46.7	5.20/12	1215	1-SD	W	HS	4	.....	LS	CS	.....	N	6.6
.....850 Coupe	R	4-2.42x2.69	52-6000	50.8	9.00	IL	IH	78.0	44.9	45.3	140.5	55.4	46.4	5.20/12	1325	1-SD	W	HS	4	.....	LS	CS	.....	N	6.6
.....850S Coupe	R	4-2.42x2.69	57-6500	50.8	9.00	IL	IH	78.0	44.9	45.3	140.5	55.4	46.4	5.20/12	1325	1-SD	W	HS	4	.....	LS	CS	.....	N	6.6
.....850 Monza	R	4-2.44x2.69	73-7000	51.6	9.50	IL	OC	78.0	44.9	45.3	135.4	52.6	44.5	5.20/12	1260	2-SD	W	HS	4	.....	LS	CS	.....	N	6.6
.....2200 Coupe	Fr	6-3.06x2.87	135-6000	131.8	9.50	IL	IH	95.5	52.3	51.0	171.5	63.2	50.3	9.00/14	2320	3-Du	W	HS	4	.....	TB	CS	.....	N	13.2
.....2200 Spyder	Fr	6-3.06x2.87	135-6000	131.8	9.50	IL	IH	95.5	52.3	51.0	179.6	63.4	51.8	9.00/14	2647	3-Du	W	HS	4	.....	TB	CS	.....	N	13.2
Alfa Romeo Giulietta Berlina	Fr	4-2.88x2.92	52-5200	79.8	.....	IL	OC	92.8	50.4	49.5	157.2	60.6	54.8	6.00/15	2058	1-SD	W	HS	4	Hk	CS	CS	H	6½	8.8
.....Giulietta TI	Fr	4-2.88x2.92	64-5500	79.8	.....	IL	OC	92.8	50.4	49.5	162.1	60.6	54.8	6.00/15	2062	1-DD	W	HS	4	Hk	CS	CS	H	6½	8.8
.....Giulietta Sprint	Fr	4-2.88x2.92	79-6000	79.8	.....	IL	OC	92.8	50.4	49.5	155.2	59.8	51.5	6.00/15	2000	1-DD	W	HS	4	Hk	CS	CS	H	6½	11.7
.....Giulietta Spider	Fr	4-2.88x2.92	79-6000	79.8	.....	IL	OC	87.7	50.4	49.5	152.0	61.6	62.0	6.00/15	1930	1-DD	W	HS	4	Hk	CS	CS	H	6½	11.7
.....Giulietta Sprint Veloce	Fr	4-2.88x2.92	89-6000	79.8	.....	IL	OC	92.8	50.4	49.5	155.2	59.8	51.5	6.00/15	2010	2-SD	W	HS	4	Hk	CS	CS	H	6½	17.6
.....Giulietta Spider Veloce	Fr	4-2.88x2.92	89-6000	79.8	.....	IL	OC	87.7	50.4	49.5	152.0	61.6	62.0	6.00/15	1945	2-SD	W	HS	4	Hk	CS	CS	H	6½	17.6
.....Giulietta Sprint Special	Fr	4-2.88x2.92	99-6000	79.8	.....	IL	OC	87.7	50.4	49.5	160.5	64.7	50.5	6.00/15	1930	2-SD	W	HS	5	Hk	CS	CS	H	6½	17.6
.....2000 Berlina	Fr	4-3.29x3.43	104-5300	120.5	.....	IL	OC	106.0	54.6	53.4	184.0	66.3	58.6	6.40/15	3015	1-DD	W	HS	5	Hk	CS	CS	H	9½	13.2
.....2000 Spider	Fr	4-3.29x3.43	113-5700	120.5	.....	IL	OC	97.5	54.6	53.4	175.5	64.7	51.7	6.40/15	2647	2-SD	W	HS	5	Hk	CS	CS	H	9½	13.2
Blanchina.....110B	R	2-2.96x2.76	21-4000	29.2	7.00	IL	IH	72.4	44.1	44.7	117.5	52.8	52.0	5.00/12	.....	1-SD	A	HS	4	.....	LS	CS	H	N	5.5
.....110B Sp.	R	2-2.85x2.76	25-4500	30.5	8.60	IL	IH	72.4	44.1	44.7	117.5	52.8	52.0	5.00/12	.....	1-SD	A	HS	4	.....	LS	CS	H	N	5.5
Ferrari.....250 Granturismo Coupe	Fr	12-2.84x2.29	240-7000	180.0	8.80	V	OC	101.5	52.7	52.6	186.0	67.5	51.9	6.00/16	2720	3-DD	W	HS	4	TT	CT	LS	HM	7½	19.8
.....250 Granturismo Cabriolet	Fr	12-2.84x2.29	240-7000	180.0	8.80	V	OC	101.5	52.7	52.6	186.0	67.5	51.9	6.00/16	2650	3-DD	W	HS	4	TT	CT	LS	HM	7½	19.8
.....250 Granturismo	Fr	12-2.84x2.29	280-7000	180.0	9.20	V	OC	93.6	52.7	52.6	186.0	67.5	51.9	6.00/16	2120	3-DD	W	HS	4	TT	CT	LS	HM	7½	19.8
.....Berlinetta	Fr	12-2.84x2.29	280-7000	180.0	9.20	V	OC	93.6	52.7	52.6	186.0	67.5	51.9	6.00/16	2120	3-DD	W	HS	4	TT	CT	LS	HM	7½	19.8
.....250 Granturismo	Fr	12-2.84x2.29	280-7000	180.0	9.20	V	OC	93.6	52.7	52.6	186.0	67.5	51.9	6.00/16	2205	3-DD	W	HS	4	TT	CT	LS	HM	7½	19.8
.....Spider California	Fr	12-2.84x2.29	280-7000	180.0	9.20	V	OC	93.6	52.7	52.6	186.0	67.5	51.9	6.00/16	2205	3-DD	W	HS	4	TT	CT	LS	HM	7½	19.8
Fiat.....500	R	2-2.60x2.76	21-4800	29.2	7.00	IL	IH	72.4	44.1	44.7	121.0	52.0	52.2	4.70/12	1069	1-SD	A	HS	4	SB	LS	CS	HM	N	5.5
.....500 Sport	R	2-2.65x2.76	25-4800	30.5	8.60	IL	IH	72.4	44.1	44.7	121.0	52.0	52.2	4.70/12	1091	1-SD	A	HS	4	SB	LS	CS	HM	N	5.5
.....600 Sedan	R	4-2.38x2.20	29-4900	38.6	7.50	IL	IH	78.8	45.3	45.7	130.5	54.3	55.3	5.20/12	1279	1-SD	W	HS	4	SB	LS	CS	HM	4½	7.1
.....600 Multipla	R	4-2.38x2.20	29-4900	38.6	7.50	IL	IH	78.8	45.2	44.5	141.2	57.1	62.2	5.20/12	1587	1-SD	W	HS	4	SB	CS	CS	HM	7	7.6
.....1100 Family Car	Fr	4-2.68x2.95	48-4800	66.5	7.00	IL	IH	92.1	48.6	47.9	149.1	57.4	58.9	5.80/14	1929	1-SD	W	HS	4	Hy	CS	LS	HM	4½	10.6
.....1100 Standard	R	4-2.68x2.95	48-4800	66.5	7.00	IL	IH	92.1	48.6	47.9	154.3	57.4	58.9	5.20/14	1885	1-SD	W	HS	4	Hy	CS	LS	HM	4½	10.6
.....1100 DeLuxe	R	4-2.68x2.95	55-5200	66.5	7.85	IL	IH	92.1	48.6	47.9	156.0	57.4	58.9	5.20/14	1940	1-DD	W	HS	4	Hy	CS	LS	HM	4½	10.6
.....1200 Granluce	R	4-2.83x2.95	63-5300	74.5	8.25	IL	IH	92.1	48.6	47.9	156.0	57.4	58.9	5.20/14	1995	1-DD	W	HS	4	Hy	CS	LS	HM	4½	10.6
.....1200 Cabriolet	Fr	4-2.83x2.95	63-5300	74.5	8.25	IL	IH	92.1	48.6	47.9	156.7	58.8	51.5	25.20/14	1962	1-DD	W	HS	4	Hy	CS	LS	HM	4½	10.6
.....1800 Station Wagon	Fr	6-2.83x2.89	82-5000	109.5	8.80	IL	IH	104.3	62.0	61.5	175.8	63.3	56.5	6											

FOREIGN PASSENGER CARS—concluded

MAKE AND MODEL	Location in Chassis	ENGINE						CHASSIS										Cooling System Capacity (Qt.)	Fuel Tank Capacity (Gal.)						
		Number of Cylinders Bore and Stroke (In.)	Max. Brake Horsepower at Specified R.P.M.	Piston Displacement (Cu. In.)	Compression Ratio (To - 1)	Cylinder Arrangement	Valve Location	TREAD (In.)		OVERALL DIMENSIONS (In.)					Shipping Weight (Lbs.)	Carburator—No. Used and Type	Cooling Medium			Shifting Method	No. of Forward Speeds	Final Drive Type	Front Suspension Type	Rear Suspension Type	Service Brakes
								Front	Rear	Length Including Bumpers and Bumper Guards	Width	Height—Road to Roof, No Load	Tires (In.)												
ITALY—continued																									
Fiat—Cont'd																									
..... 1800 Station Wagon	Fr	6-3.03x2.89	90-5000	125.3	8.80	IL	IH	104.3	52.8	51.5	175.8	63.8	58.5	6.40/14	2811	1-DD	W	HS	4	Hy	TB	CS	HM	2 1/4	15.9
..... 1800 Sedan	Fr	6-3.03x2.89	95-5000	125.3	8.80	IL	IH	104.3	52.8	51.5	175.8	63.8	57.9	5.90/14	2601	1-DD	W	HS	4	Hy	TB	CS	HM	2 1/4	15.9
..... 2100 Sedan	Fr	6-3.03x2.89	95-5000	125.3	8.80	IL	IH	104.3	52.8	51.5	182.1	64.1	58.7	6.40/14	2756	1-DD	W	HS	4	Hy	TB	CS	HM	2 1/4	15.9
Lancia..... Aspia Saloon	Fr	4-2.67x2.97	48-4900	66.5	7.80	V	IH	98.0	46.5	46.5	158.3	58.3	57.1	6.00/14	1-De	W	HS	4	.....	CS	LS	M	6	10.0	
..... Aspia Coupe	Fr	4-2.67x2.97	54-5200	66.5	8.00	V	IH	98.0	46.4	46.5	161.0	58.7	51.6	6.00/15	1-De	W	HS	4	.....	CS	LS	M	6	10.0	
..... Aspia Convertible	Fr	4-2.67x2.97	54-5200	66.5	8.00	V	IH	98.0	46.4	46.5	163.4	59.8	51.2	6.00/15	1-De	W	HS	4	.....	CS	LS	M	6	10.0	
..... Aspia G. T. E.	Fr	4-2.67x2.97	54-5200	66.5	8.00	V	IH	98.0	46.4	46.5	168.0	55.9	48.8	6.00/15	1-De	W	HS	4	.....	CS	LS	M	6	10.0	
..... Flaminia Saloon	Fr	6-3.15x3.21	100-4800	150.0	7.80	V	OC	113.0	54.0	54.0	191.0	69.0	59.0	6.40/15	1-De	W	HS	4	.....	CS	LS	H	10	15.3	
..... Flaminia Sport	Fr	6-3.15x3.21	119-5200	150.0	8.00	V	OC	99.0	54.0	54.0	177.0	67.0	51.0	6.40/15	1-De	W	HS	4	.....	CS	LS	H	10	15.3	
..... Flaminia GT	Fr	6-3.15x3.21	119-5200	150.0	9.00	V	OC	99.0	54.0	54.0	177.0	65.0	51.0	6.40/15	1-De	W	HS	4	.....	CS	LS	H	10	15.3	
..... Flaminia Coupe	Fr	6-3.15x3.21	119-5200	150.0	9.00	V	OC	108.0	54.0	54.0	187.0	68.5	56.0	6.40/15	1-De	W	HS	4	.....	CS	LS	H	10	15.3	
Maserati..... 3500 GT	Fr	6-3.38x3.94	230-5000	212.7	8.20	IL	OC	102.3	54.7	53.5	.....	.....	6.50/16	.....	3-DS	W	HS	4	.....	CS	LS	H	.....	23.0	
Moretti..... Berlina	Fr	4-2.32x2.57	35-5000	45.6	8.00	IL	IH	93.0	.....	.....	146.0	57.0	54.6	5.20/12	1680	1-DD	W	HS	4	.....	.....	.....	.....	.....	9.6
..... Spyder	Fr	4-2.32x2.57	43-5000	45.6	8.00	IL	IH	93.0	.....	.....	144.0	54.6	48.8	5.20/12	1545	1-DD	W	HS	4	.....	.....	.....	.....	.....	9.6
..... Coupe	Fr	4-2.32x2.57	43-5000	45.6	8.00	IL	IH	93.0	.....	.....	144.0	54.6	51.5	5.20/12	1590	1-DD	W	HS	4	.....	.....	.....	.....	.....	9.6
..... 4 USI	Fr	4-2.32x2.57	35-5000	45.6	8.00	IL	IH	93.5	.....	.....	146.0	57.0	54.6	5.20/12	1720	1-DD	W	HS	4	.....	.....	.....	.....	.....	9.0
..... 1500 GT	Fr	4-3.08x2.92	120-6000	89.6	9.00	IL	IH	93.5	.....	.....	156.0	58.5	54.6	6.40/16	2205	1-DD	W	HS	4	.....	.....	.....	.....	.....	9.0
..... Junior 1100	Fr	4-2.68x2.95	75-6500	66.5	8.50	IL	IH	93.5	50.3	50.3	170.0	62.5	50.0	6.40/16	2430	2-DD	W	HS	4	.....	.....	.....	.....	.....	9.0
..... 1500 Golden Arrow	Fr	4-3.08x2.92	110-6000	89.6	.....	IL	IH	93.5	50.3	50.3	170.0	62.5	50.0	6.40/16	2430	2-DD	W	HS	4	.....	.....	.....	.....	.....	9.0
JAPAN																									
Nissan..... 210-S, L-210-S	Fr	4-2.68x2.32	37-4800	60.2	7.50	IL	IH	87.4	46.1	45.5	152.8	57.7	58.1	5.00/15	2035	1-SD	W	HS	4	Hk	LS	LS	H	5 1/4	8.6
..... PL-310	Fr	4-2.88x2.00	48-4800	72.6	7.50	IL	IH	88.9	47.6	47.0	153.9	58.3	57.5	5.60/13	1874	1-SD	W	HS	3	Hk	CS	LS	H	4 1/4	8.2
Prince..... Skyline ALSI-1	Fr	4-2.95x3.31	60-4400	90.6	7.50	IL	IH	99.8	52.8	54.3	172.0	65.9	60.4	6.40/14	2820	1-DD	W	HS	4	Hk	CS	LS	H	10 1/2	10.6
..... Skyline ALSIEL (A)	Fr	4-2.95x3.31	73-4800	90.6	8.30	IL	IH	99.8	52.8	54.3	171.7	65.9	60.4	5.90/15	2860	1-DD	W	HS	4	Hk	CS	LS	H	10 1/2	10.6
..... Skyline ALSIEL (B)	Fr	4-3.31x3.31	83-4800	113.6	8.50	IL	IH	99.8	52.8	54.3	171.7	65.9	60.4	5.90/15	2890	1-DD	W	HS	4	Hk	CS	LS	H	10 1/2	10.6
Toyota..... Toyopet Corona PT20	Fr	4-2.75x2.56	48-5000	60.8	7.50	IL	IH	94.5	48.4	48.4	156.0	58.3	56.5	5.60/13	2010	1-DD	W	HS	3	Hk	TB	LS	H	7	16.5
..... Toyopet Crown RS20	Fr	4-3.03x3.07	63-4500	88.7	7.50	IL	IH	99.6	52.2	53.9	171.9	66.8	61.0	6.40/15	2640	1-DD	W	HS	3	Hk	TB	LS	H	8 1/4	12.4
..... Toyopet Corona RT20	Fr	4-3.03x3.07	65-4500	88.8	8.00	IL	IH	94.5	48.4	48.4	156.0	58.3	56.5	5.60/13	2120	1-DD	W	HS	3	Hk	TB	LS	H	8 1/4	10.5
..... Toyopet Crown	Fr	4-3.03x3.07	65-4500	88.8	8.00	IL	IH	99.6	52.2	53.9	171.9	66.8	60.2	6.40/13	2590	1-De	W	HS	3	Hk	CS	LS	H	8 1/4	12.4
..... Custom RS22L	Fr	4-3.03x3.07	65-4500	88.8	8.00	IL	IH	99.6	52.2	53.9	171.9	66.8	60.2	6.40/13	2590	1-De	W	HS	3	Hk	CS	LS	H	8 1/4	12.4
SPAIN																									
S. E. A. T..... 600	R	4-2.36x2.20	22-4600	36.7	7.50	IL	IH	76.7	45.0	45.4	127.0	54.5	55.6	5.20/12	.....	1-SD	W	HS	4	TT	LS	CS	H	4 1/4	7.1
..... 1400C	Fr	4-3.21x2.96	58-4800	85.1	7.50	IL	IH	104.3	52.4	51.4	125.8	62.5	56.4	5.90/14	.....	1-DD	W	HS	4	TT	CS	CS	H	9 1/4	15.9
SWEDEN																									
SAAB..... 93F	Fr	3-2.59x2.87	36-5000	46.0	7.30	IL	N	98.0	48.0	48.0	156.0	62.0	58.0	5.00/15	1710	1-De	W	HS	3	.....	CS	CS	H	8	9.5
..... 95	Fr	3-2.78x2.87	42-5000	51.3	7.30	IL	N	98.0	48.0	48.0	162.0	62.0	58.0	5.00/15	1908	1-SD	W	HS	4	.....	CS	CS	H	8	11.4
Volvo..... P-4450B	Fr	4-3.13x3.15	85-5500	97.6	8.20	IL	IH	102.5	51.0	51.8	173.0	63.0	66.0	6.40/15	2315	2-SD	W	HS	4	Hk	CS	LS	H	9	9.3
..... P-5440B	Fr	4-3.13x3.15	85-5500	97.6	8.20	IL	IH	102.5	51.0	51.8	175.0	63.0	61.5	5.90/15	2116	2-SD	W	HS	4	Hk	CS	CS	H	9	9.3
..... P-1220B	Fr	4-3.13x3.15	85-5500	97.6	8.20	IL	IH	102.5	51.0	51.8	175.0	63.0	59.3	5.90/15	2182	2-SD	W	HS	4	Hk	CS	CS	H	9	12.0

ABBREVIATIONS

- Helical gears to differential mounted on side of transmission casing. Short drive shafts to each front wheel.
 - Two constant velocity jointed shafts.
- A—Air.
Au—Automatic.
BG—Bevel gear.
Ch—Chain.
CL—Coil and leaf spring.
CS—Coil springs.
CT—Coil springs and torsion bars.
DD—Dual throat downdraft.

- De—De Dion.
Do—Downdraft.
DS—Dual throat, sidedraft.
Du—Dual throat.
F—"F" head, valves in head and side.
Fb—Four barrel.
Fr—Front.
H—Hydraulic.
HA—Hand shift or automatic.
Hk—Hotchkiss.
HM—Hydraulic and mechanical.
Ho—Horizontal.
HP—Hydro-pneumatic.
HS—Hand shift.
HV—Hydraulic, vacuum assisted.

- Hy—Hypoid.
IH—In head.
IL—In line.
Inj—Injectors.
L—"L" head, valves at side.
LS—Leaf springs.
M—Mechanical.
N—Non.
OC—Overhead camshaft.
Op—Opposed.
Pr—Preselective.
R—Rear.
RS—Rubber cone spring.
RT—Rubber torsion springs.
S—Single cylinder.

- SA—Split axle.
SB—Spiral bevel.
Sd—Sidedraft.
SD—Single throat downdraft.
Sm—Semi-downdraft.
SS—Single throat sidedraft.
SSH—Single throat semi-horizontal.
SSM—Single throat semi-downdraft.
TB—Torsion bars.
TH—Torsion bar and hydro-pneumatic.
TO—Twin overhead camshafts.
TT—Torque tube.
V—"V" type engine.
W—Water.

SMALL GASOLINE ENGINES 1960

MAKE AND MODEL	Designed for Use	Number of Cycles	ENGINE										GOV-ERNOR		FUEL SYSTEM		Fuel Used	Starting Method	
			Type	No. of Cylinders	Bore and Stroke (in.)	Total Displacement (Cu. In.)	Compression Ratio (to 1)	Valve Location	Horsepower			Used	Type	Ignition System Type	FUEL SYSTEM				
									Rated at RPM	Continuous at RPM	Torque—Lb. Ft. at RPM				Weight (Lb.)	Type			Make
AIR COOLED																			
Briggs & Stratton..... 60100	Ha, AC, Lm	4	Hor	1	2 1/2 x 1 1/2	6.65	4.90	L	2.00-3600	1.70-3600	3.14-3000	21	Y	Pn	Mag	SC	Own	G	REV
..... 60300, 60400	General Purpose	4	Hor	1	2 1/2 x 1 1/2	6.65	4.90	L	2.25-3600	1.90-3600	3.42-3200	23	Y	Pn	Mag	FC	Own	G	REV
..... 60500	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	6.65	4.90	L	2.00-3600	1.70-3600	3.14-3000	18 1/2	Y	Pn	Mag	SC	Own	G	REV
..... 60700	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	6.65	4.90	L	2.25-3600	1.90-3600	3.42-3200	19 1/2	Y	Pn	Mag	FC	Own	G	REV
..... 61100	General Purpose	4	Hor	1	2 1/2 x 1 1/2	6.65	4.90	L	2.00-3600	1.70-3600	3.14-3000	21	Y	Pn	Mag	SC	Own	G	REV
..... 61300, 61400	General Purpose	4	Hor	1	2 1/2 x 1 1/2	6.65	4.90	L	2.25-3600	1.90-3600	3.42-3200	23	Y	Pn	Mag	FC	Own	G	REV
..... 61500	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	6.65	4.90	L	2.00-3600	1.70-3600	3.14-3000	18 1/2	Y	Pn	Mag	SC	Own	G	REV
..... 61700	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	6.65	4.90	L	2.25-3600	1.90-3600	3.42-3200	19 1/2	Y	Pn	Mag	FC	Own	G	REV
..... 80100	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.75	5.50	L	2.50-3600	2.10-3600	4.03-3000	21	Y	Pn	Mag	SC	Own	G	REV
..... 80300, 80400	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.75	5.50	L	3.00-3600	2.55-3600	4.81-3100	25	Y	Pn	Mag	FC	Own	G	REV
..... 80500	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	7.75	5.50	L	2.50-3600	2.10-3600	4.03-3000	18 1/2	Y	Pn	Mag	SC	Own	G	REV
..... 80700	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	7.75	5.50	L	3.00-3600	2.55-3600	4.81-3100	19 1/2	Y	Pn	Mag	FC	Own	G	REV
..... 81100	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.75	5.50	L	2.50-3600	2.10-3600	4.03-3000	21	Y	Pn	Mag	SC	Own	G	REV
..... 81300, 81400	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.75	5.50	L	3.00-3600	2.55-3600	4.81-3100	25	Y	Pn	Mag	FC	Own	G	REV
..... 81500	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	7.75	5.50	L	2.50-3600	2.10-3600	4.03-3000	18 1/2	Y	Pn	Mag	SC	Own	G	REV
..... 81700	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	7.75	5.50	L	3.00-3600	2.55-3600	4.81-3100	19 1/2	Y	Pn	Mag	FC	Own	G	REV
..... 9	General Purpose	4	Hor	1	2 1/2 x 2 1/2	8.85	5.40	L	3.35-3600	2.83-3600	5.29-2700	61	Y	Me	Mag	FC	Own	G	Hbp
..... 14	General Purpose	4	Hor	1	2 1/2 x 2 1/2	14.21	5.40	L	5.25-3600	4.20-3600	8.75-2700	70	Y	Me	Mag	FC	Own	G	Hbp
..... 141300	General Purpose	4	Hor	1	2 1/2 x 2 1/2	14.10	5.60	L	4.50-3600	3.80-3600	8.06-2300	43	Y	Pn	Mag	FC	Own	G	Rc
..... 141700	General Purpose	4	Hor	1	2 1/2 x 2 1/2	14.10	5.60	L	5.75-3600	4.90-3600	8.88-3000	44	Y	Pn	Mag	FC	Own	G	Rc
..... 143300	General Purpose	4	Hor	1	2 1/2 x 2 1/2	14.10	5.60	L	5.75-3600	4.90-3600	8.88-3000	44	Y	Pn	Mag	FC	Own	G	Rc
..... 143700	General Purpose	4	Ver	1	2 1/2 x 2 1/2	14.10	5.60	L	5.75-3600	4.90-3600	8.88-3000	40 1/2	Y	Pn	Mag	FC	Own	G	Rc
..... 19	General Purpose	4	Hor	1	3 x 2 1/2	18.54	5.60	L	7.00-3600	5.95-3600	15.50-2200	76	Y	Me	Mag	FC	Own	G	Hbp
..... 23A	General Purpose	4	Hor	1	3 x 3 1/2	22.87	5.90	L	9.00-3600	7.60-3600	15.79-2300	98	Y	Me	Mag	FC	Own	G	RR
..... 80600	Lm	4	Hor	1	2 1/2 x 1 1/2	7.75	5.50	L	2.50-3600	2.10-3600	4.03-3100	21	Y	Av	Mag	SC	Own	G	RR
..... 80800	Lm	4	Hor	1	2 1/2 x 1 1/2	7.75	5.50	L	3.00-3600	2.55-3600	4.03-3000	21	Y	Av	Mag	Car	Own	G	RR
..... 80900	Lm, Til, In	4	Ver	1	2 1/2 x 1 1/2	7.75	5.50	L	3.00-3600	2.55-3600	4.03-3000	22	Y	Av	Mag	Car	Own	G	RR
..... 141400	General Purpose	4	Hor	1	2 1/2 x 2 1/2	14.10	5.60	L	4.50-3600	3.80-3600	8.06-2300	43	Y	Me	Mag	FC	Own	G	Rc
..... 143400	General Purpose	4	Hor	1	2 1/2 x 2 1/2	14.10	5.60	L	5.75-3600	4.90-3600	8.88-3600	44	Y	Me	Mag	FC	Own	G	Rc
Clinton..... VS-400, CVS-400	General Purpose	2	Ver	1	2 1/2 x 1 1/2	5.78	.....	L	2.50-3600	2.00-3600	3.80-2900	14	Y	Pn	Mag	FC	Own	GO	Rr
..... VS-2100	General Purpose	4	Ver	1	2 1/2 x 1 1/2	7.20	.....	L	2.25-3600	1.80-3600	3.57-2900	18 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... VS-4100	General Purpose	4	Ver	1	2 1/2 x 1 1/2	8.30	.....	L	3.00-3600	2.40-3600	4.40-3000	18 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... VS-1000	General Purpose	4	Ver	1	2 1/2 x 1 1/2	7.20	.....	L	2.50-3600	2.00-3600	3.80-3000	20	Y	Pn	Mag	FC	Own	G	Rr
..... VS-3100, AVS-3100	General Purpose	4	Ver	1	2 1/2 x 1 1/2	8.30	.....	L	3.25-3600	2.60-3600	5.00-2600	20	Y	Pn	Mag	FC	Own	G	Rr
..... FV-100, V-100	General Purpose	4	Ver	1	2 1/2 x 1 1/2	7.20	.....	L	2.75-3600	2.20-3600	4.20-3000	30 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... FV, AFV, V, AV-3100	General Purpose	4	Ver	1	2 1/2 x 1 1/2	8.30	.....	L	3.50-3600	2.80-3600	5.30-2800	30 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... V-1100	General Purpose	4	Ver	1	2 1/2 x 2 1/2	9.50	.....	L	3.75-3600	3.00-3600	.....	31	Y	Pn	Mag	FC	Own	G	Rr
..... V-1200	General Purpose	4	Ver	1	2 1/2 x 2 1/2	10.20	.....	L	4.50-3600	3.60-3600	3.25-2900	40	Y	Pn	Mag	FC	Own	G	Rr
..... A-400, A-490	General Purpose	2	Hor	1	2 1/2 x 1 1/2	5.78	.....	L	2.50-3600	2.00-3600	3.50-3100	20	Y	Pn	Mag	FC	Own	GO	Rr
..... A-480	General Purpose	2	Hor	1	2 1/2 x 1 1/2	5.78	.....	L	2.50-633	2.00-633	21.60-515	28	Y	Pn	Mag	FC	Own	GO	Rr
..... A-2100, A-2190	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.20	.....	L	2.25-3600	1.80-3600	3.65-2600	21 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... A-2180	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.20	.....	L	2.25-600	2.00-600	22.00-450	27 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... 4100, 4190	General Purpose	4	Hor	1	2 1/2 x 1 1/2	8.30	.....	L	3.00-3600	2.40-3600	4.40-3000	21 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... 4160	General Purpose	4	Hor	1	2 1/2 x 1 1/2	8.30	.....	L	3.00-600	2.20-600	28.90-450	27 1/2	Y	Pn	Mag	FC	Own	G	Rr
..... 100, 190	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.20	.....	L	2.50-3600	2.00-3600	3.80-3000	23	Y	Pn	Mag	FC	Own	G	Rr
..... 180	General Purpose	4	Hor	1	2 1/2 x 1 1/2	7.20	.....	L	2.50-600	2.00-600	22.00-500	29	Y	Pn	Mag	FC	Own	G	Rr
..... 3100, 3190	General Purpose	4	Hor	1	2 1/2 x 1 1/2	8.30	.....	L	3.25-3600	2.60-3600	5.00-2700	23	Y	Pn	Mag	FC	Own	G	Rr
..... 3160	General Purpose	4	Hor	1	2 1/2 x 1 1/2	8.30	.....	L	3.25-600	2.60-600	31.30-450	29	Y	Pn	Mag	FC	Own	G	Rr
..... D-700, D-790	General Purpose	4	Hor	1	2 1/2 x 1 1/2	6.65	.....	L	2.25-3600	2.00-3600	3.65-2600	41	Y	Fb	Mag	FC	Own	G	Rr
..... D-760	General Purpose	4	Hor	1	2 1/2 x 1 1/2	6.65	.....	L	2.25-600	2.00-600	22.00-467	47	Y	Fb	Mag	FC	Own	G	Rr
..... 900, 990	General Purpose	4	Hor	1	2 1/2 x 1 1/2	8.30	.....	L	3.25-3600	2.60-3600	4.95-2700	41	Y	Fb	Mag	FC	Own	G	Rr
..... 960	General Purpose	4	Hor	1	2 1/2 x 1 1/2	8.30	.....	L	3.25-600	2.60-600	33.00-450	47	Y	Fb	Mag	FC	Own	G	Rr
..... B-1290	General Purpose	4	Hor	1	2 1/2 x 2 1/2	10.20	.....	L	4.00-3600	3.20-3600	6.60-2500	45	Y	Fb	Mag	FC	Own	G	ER
..... A-1600, A-1690	General Purpose	4	Hor	1	2 1/2 x 2 1/2	16.30	.....	L	6.30-3600	5.04-3600	10.50-2100	87	Y	Fb	Mag	FC	Own	G	ER
..... A-1660	General Purpose	4	Hor	1	2 1/2 x 2 1/2	16.30	.....	L	6.30-600	5.04-600	64.50-385	100	Y	Fb	Mag	FC	Own	G	ER
..... B-2500, B-2590	General Purpose	4	Hor	1	3 1/2 x 3 1/2	25.00	.....	L	9.60-3600	7.68-3600	16.50-2500	103	Y	Fb	Mag	FC	Own	G	ER
..... B-2560	General Purpose	4	Hor	1	3 1/2 x 3 1/2	25.00	.....	L	9.60-600	7.68-600	96.00-415	116	Y	Fb	Mag	FC	Own	G	ER
..... B-1260	General Purpose	4	Hor	1	2 1/2 x 2 1/2	10.20	.....	L	4.00-3600	3.20-3600	39.60-420	45	Y	Fb	Mag	FC	Own	G	ER
Continental..... AU7, AU7B	General Purpose	4	(b)	1	2 1/2 x 2	7.10	6.10	L	2.00-3600	1.70-3600	3.02-3000	36	Y	MA	Mag	Car	C-T	G	BR
..... AU7R	General Purpose	4	(b)	1	2 1/2 x 2	7.10	6.10	L	2.00-600	1.70-600	18.10-500	41	Y	MA	Mag	Car	C-T	G	BR
..... AD7, AW7	Lawn Mowers	4	Ver	1	2 1/2 x 2	7.10	6.10	L	2.00-3600	1.70-3600	3.02-3000	41	Y	MA	Mag	Car	C-T	G	BR
..... AU8, AU8B	General Purpose	4	(b)	1	2 1/2 x 2	7.95	6.65	L											

1960 SMALL GASOLINE ENGINES

MAKE AND MODEL	Designed for Use	Number of Cycles	ENGINE										GOV-ERNOR		FUEL SYSTEM		Starting Method			
			Type	No. of Cylinders	Bore and Stroke (in.)	Total Displacement (Cu. in.)	Compression Ratio (to 1)	Valve Location	Horsepower		Torque—Lb. Ft. at RPM	Weight (Lb.)	Used	Type	Ignition System Type	Fuel System				
									Rated at RPM	Continuous at RPM						Type		Make	Fuel Used	
AIR COOLED—continued																				
Jacobson	J-125	Lawn Mowers	2	(k)	1	2 1/4 x 1 1/2	4.70	5.50		2.25-3600	1.80-3600	3.25-3600	22	Y	Av	Mag	Car	Till	GO	ER
	J-175	Lawn Mowers	2	(k)	1	2 1/4 x 1 1/2	6.20	5.50		3.00-3600	2.55-3600	4.38-3600	22	Y	Av	Mag	Car	Till	GO	ER
	J-175V	Lawn Mowers	2	(k)	1	2 1/4 x 1 1/2	6.20	5.50		3.00-3600	2.55-3600	4.38-3600	20	Y	Av	Mag	Car	Till	GO	ER
	J-225V	Lawn Mowers	2	(k)	1	2 1/4 x 2	7.95	5.50		4.00-3600	3.40-3600	5.85-3600	36	Y	Av	Mag	Car	Till	GO	ER
Kohler	K91	General Purpose	4	Her	1	2 3/4 x 2	8.86	6.50	L	4.00-4000	2.46-3600	5.75-2700	40	Y	Fb	Mag	Car	Cart	G, K	Rre
	K161	General Purpose	4	Her	1	2 3/4 x 2 1/2	16.22	6.20	L	7.00-3600	4.68-3600	11.25-2400	67	Y	Fb	BM	Car	Wal	G, K	Rre
	KV161	General Purpose	4	Her	1	2 3/4 x 2 1/2	16.22	6.20	L	7.00-3600	4.68-3600	11.25-2400	65	Y	Fb	Mag	Car	Wal	G	Rre
	K241	General Purpose	4	Her	1	3 1/4 x 2 1/2	23.80	6.00	L	8.00-3600	6.75-3600	16.70-2200	107	Y	Fb	Mag	Car	Cart	G	ER
	K331	General Purpose	4	Her	1	3 3/4 x 3	33.00	6.20	L	12.50-3200	8.47-2800	24.00-1600	178	Y	Fb	BM	Car	Cart	G	ER
	K682	General Purpose	4	Op	2	3 3/4 x 3 1/2	67.20	6.20	L	24.00-3200	16.95-2800	44.50-2250	250	Y	Fb	Mag	Car	CZ	G	HE
Lauson	LV22	Lawn Mowers	4	Ver	1	2 1/4 x 1 1/2	7.35		L	2.25-3600	1.80-3600	3.40-3600	21 1/2	Y	Me	Mag	Car	Wal	G	Rc
	H22, HB22, HR22	GS, AC, Pu, Af, Lm	4	Her	1	2 1/4 x 1 1/2	7.35		L	2.25-3600	1.80-3600	3.40-3600	22 1/2	Y	Me	Mag	Car	Til	G	Rc
	LV28	Lawn Mowers	4	Ver	1	2 1/4 x 1 1/2	7.35		L	2.50-3600	2.00-3600	3.65-3600	21 1/2	Y	Me	Mag	Car	Wal	G	Rc
	H28, HB28, HR28	GS, AC, Pu, Af, Lm	4	Her	1	2 1/4 x 1 1/2	7.35		L	2.50-3600	2.00-3600	3.65-3600	22 1/2	Y	Me	Mag	Car	Til	G	Rc
	LV30	Lawn Mowers	4	Ver	1	2 1/4 x 1 1/2	7.61		L	3.00-3600	2.40-3600	4.40-3600	21 1/2	Y	Me	Mag	Car	Wal	G	Rc
	H30, HB30, HR30	GS, AC, Pu, Af, Lm	4	Her	1	2 1/4 x 1 1/2	7.61		L	3.00-3600	2.40-3600	4.40-3600	22 1/2	Y	Me	Mag	Car	Til	G	Rc
	LV35	Lawn Mowers	4	Ver	1	2 1/4 x 1 1/2	8.90		L	3.50-3600	2.80-3600	5.10-3600	22 1/2	Y	Me	Mag	Car	Wal	G	Rc
	H35, HB35, HR35	GS, AC, Pu, Af, Lm	4	Her	1	2 1/4 x 1 1/2	8.90		L	3.50-3600	2.80-3600	5.10-3600	23 1/2	Y	Me	Mag	Car	Til	G	Rc
	VX45	Lawn Mowers	4	Ver	1	2 1/2 x 2 1/2	11.04		L	4.50-3600	3.80-3600	6.55-3600	33 1/2	Y	Me	Mag	Car	Wal	G	Rc
	V45, V65, V85	Lawn Mowers	4	Ver	1	2 1/2 x 2 1/2	11.04		L	4.50-3600	3.80-3600	6.55-3600	37 1/2	Y	Me	Mag	Car	Wal	G	Rc
	H45, HB45, HR45	GS, AC, Pu, Af, Lm	4	Her	1	2 1/2 x 2 1/2	11.04		L	4.50-3600	3.80-3600	6.55-3600	38 1/2	Y	Me	Mag	Car	Til	G	Rc
	V55, V65, V85	Lawn Mowers	4	Ver	1	2 1/2 x 2 1/2	12.18		L	5.50-3600	4.40-3600	8.10-3600	37 1/2	Y	Me	Mag	Car	Wal	G	Rc
	H55, HB55, HR55	GS, AC, Pu, Af, Lm	4	Her	1	2 1/2 x 2 1/2	12.18		L	5.50-3600	4.40-3600	8.10-3600	40	Y	Me	Mag	Car	Til	G	Rc
Lawn Boy	C-14, C-15	Lawn Mowers	2		1	2 1/4 x 1 1/2	5.32	6.50	Re	2.50-4000	2.13-4000	3.90-3000	17	Y	(d)	Mag	Car	Own	GO	Rre
	C-41	Lawn Mowers	2		1	2 1/4 x 1 1/2	5.32	6.50	Re	2.50-4000	2.13-4000	3.90-3000	16 1/2	Y	(d)	Mag	Car	Own	GO	Rre
	C-50, C-60	Lawn Mowers	2		1	2 1/4 x 1 1/2	5.32	6.50	Re	2.50-4000	2.13-4000	3.90-3000	16 1/2	Y	(d)	Mag	Car	Own	GO	Rre
	C-70, C-71	Lawn Mowers	2		1	1 1/4 x 1 1/2	4.43	4.10	Re	2.00-4000	1.70-4000	2.80-2800	16 1/2	Y	(d)	Mag	Car	Own	GO	Rre
	C-80	Tiller	2		1	2 1/4 x 1 1/2	5.32	6.50	Re	2.50-4000	2.13-4000	3.90-3000	16 1/2	Y	(d)	Mag	Car	Own	GO	Rre
	C-12-AA	Snow Plow	2		1	2 1/4 x 1 1/2	5.32	6.50	Re	2.50-4000	2.13-4000	3.90-3000	19	Y	(d)	Mag	Car	Own	GO	Rre
McCulloch	MAC-35-A	Chain Saws	2	Ver	1	1 3/4 x 1 1/2	3.30	7.20	Re				19	N		Mag	Car	Own	GO	RR
	140, 150	Chain Saws	2		1	2 1/4 x 1 1/2	4.96	6.00	Re				18			Mag	Car	Own	GO	RR
	160	Chain Saws	2		1	2 1/4 x 1 1/2	4.96	6.00	Re				21			Mag	Car	Own	GO	RR
	170	Chain Saws	2		1	2 1/4 x 1 1/2	5.30	7.00	Re				21			Mag	Car	Own	GO	RR
	MC-10	General Purpose	2		1	2 1/4 x 1 1/2	5.30	6.90	Re				25			Mag	Car	Own	GO	RR
	180	Chain Saws	2		1	2 1/4 x 1 1/2	5.30	7.00	Re				28			Mag	Car	Own	GO	RR
	99	Chain Saws	2	Her	1	2 1/4 x 2	9.80	6.50	Re				51	Y	RV	Mag	Car	Own	GO	RR
Mustang	Standard	Motorcycles	4	Ver	1	2 1/4 x 3	19.50	6.00	L	9.50-5000	9.50-5000			N		Mag	Car	Am	G	Pe
	Special	Motorcycles	4	Ver	1	2 1/4 x 3	19.50	7.00	L	10.50-5000	10.50-5000			N		Mag	Car	Am	G	Pe
	Special 3	Motorcycles	4	Ver	1	2 1/4 x 3	19.50	7.00	L	12.00-4800	12.00-4800			N		Mag	Car	Del	G	Pe
Onan	AK	Generator Sets	4	Ver	1	2 1/4 x 2 1/2	12.20	5.50	L	3.76-3600	2.82-3600	5.80-1800	74	Y	(d)	Mag	Car	Cart	G	HE
	AJ	GS, Rf, Af	4	Ver	1	2 1/4 x 2 1/2	14.90	6.25	L	5.50-3600	3.86-3600	8.00-2100	150	Y	(d)	BM	Car	Cart	G	HE
	LK	GS, Rf	4	Ver	1	3 1/4 x 3	25.00	5.50	L	5.10-1800	4.34-1800	14.00-1800	114	Y	(d)	BM	Car	Cart	G	HE
	ACK	Generator Sets	4	Op	2	3 1/4 x 3	38.00	6.25	L	13.00-3600	10.00-3600	20.00-3600	125	Y	Me	Mag	Car	Til	G	Hc
	V8	Generator Sets	4	Ver	2	3 1/4 x 2 1/2	45.70	6.25	L	19.25-3600	16.20-3600	29.80-2500	140	Y	Me	Mag	Car	MS	G	Ele
	CGK	General Purpose	4	Op	2	3 1/4 x 3	50.00	5.50	L	13.60-3600	12.90-2700	27.80-1800	140	Y	(d)	BM	Car	MS	G	HE
	OW	Generator Sets	4	Op	2	4 1/2 x 3 1/2	88.00	5.50	L	20.80-1800	18.50-1800	58.50-1800	420	Y	Fb	Mag	Car	Zen	G	HE
Power Products	AH-47	Chain Saws	2	Her	1	2 1/4 x 1 1/2	4.70	5.75		3.50-5250		3.70-3750	11 1/2	Y	Av	Mag	Car	Till	GO	RR
West Bend	390	General Purpose	2	(k)	1	1 3/4 x 1 1/2	3.90		Re	1.50-3600	1.50-3600	2.30-3600	14 1/2	Y	Av	Mag	Car	Till	G	RA
	700	General Purpose	2	(k)	1	2 1/4 x 1 1/2	7.00		Re	5.00-5500	5.00-5500	5.17-5500	13 1/2	N		Mag	Car	Till	G	Rc
	890	General Purpose	2	(k)	1	2 3/4 x 1 1/2	5.80	5.75	Re	4.00-5500	4.00-5500	4.20-5500	13 1/2	N		Mag	Car	Till	G	RR
Wisconsin	ACN	General Purpose	4	Ver	1	2 1/4 x 2 1/2	14.88	5.88	L	6.00-3600	4.80-3600	9.50-2900	76	Y	Fb	Mag	Car	SZ	G	BP
	BKN	General Purpose	4	Ver	1	2 1/4 x 2 1/2	17.80	5.88	L	7.00-3600	5.60-3600	12.00-2600	76	Y	Fb	Mag	Car	SZ	G	BP
	AEHL	General Purpose	4	Ver	1	3 1/4 x 3	23.00	6.21	L	9.20-3600	7.40-3600	16.50-2300	110	Y	Fb	Mag	Car	Zen	G	BP
	AGND	General Purpose	4	Ver	1	3 1/4 x 4	38.00	6.12	L	12.50-3200	10.00-3200	27.00-1900	180	Y	Fb	Mag	Car	Zen	G	Hc
	TH	General Purpose	4	Ver	2	3 1/4 x 3 1/2	53.90	6.25	L	16.40-2900	13.10-2900	34.00-2000	220	Y	Fb	Mag	Car	Zen	G	Hc
	THD	General Purpose	4	Ver	2	3 1/4 x 3 1/2	53.90	6.25	L	18.00-3200	14.40-3200	34.00-2000	220	Y	Fb	Mag	Car	Zen	G	Hc
WATER COOLED																				
Kohler	L160	Generator Sets	4	Her	1	2 3/4 x 2 1/2	16.22	6.20	L	6.00-3600	4.54-3000	11.50-2300	100	Y	Fb	Mag	Car	Cart	G	ER
Onan	MAJ	Generator Sets	4	Ver	1	2 3/4 x 2 1/2	14.90	6.25	L	3.66-2400	2.75-1800	5.06-2100	170	Y	(d)	Mag	Car	Cart	G	HE
	MTK	Generator Sets	4	Ver	2	3 1/4 x 2 1/2	38.00	5.50	L	6.50-1800			400	Y	(d)	BM	Car	MS	G	HE
	W35	Generator Sets	4	Ver	2	3 1/4 x 2 1/2	38.00	5.50	L	6.50-1800			417	Y	(d)	BM	Car	M-Z	G	HE

ABBREVIATIONS
 **—21 1/4 lbs. for 6.22 model; 19 1/2 lbs. for EZ-6 model.
 ***—Generator set use, 110 lbs.; pump use, 85 lbs.
 (d)—Flyweights on camshaft.
 (g)—Generator set use, 65 lbs.; pump use, 45 lbs.
 (h)—Generator set use, 135 lbs.; pump use, 95 lbs.
 (k)—Available with vertical or horizontal shaft.
 AF—Auxiliary farm implement equipment.
 AC—Air compressors.
 Am—Ammal. Av—Air vane.
 BM—Battery and magneto.
 BP—Belt or pulley.
 Bpe—Belt, pulley or electric.

BR—Belt, pulley or recoil.
 Car—Carburetor.
 Cart—Cart Carburetor Corp.
 Ce—Centrifugal.
 C-T—Carrier or Tillotson.
 CZ—Carrier or Zenith.
 Del—Delco.
 Ele—Electric.
 ER—Electric or rope.
 Fb—Flyball.
 FG—Float feed carburetor.
 G-T—Gasoline and oil mix.
 GS—Generator sets.
 Ha—Home appliances.
 Hbp—Hand crank, belt or pulley.
 Hc—Hand crank.
 HE—Hand crank or electric.
 Hr—Horizontal.
 Hre—Hand crank or recoil.

I—Valves in head.
 In—Industrial.
 K—Kerosene.
 L—"L" head, valves at side.
 Lm—Lawn mowers.
 MA—Mechanical or air vane.
 Mag—Magneto.
 Me—Mechanical.
 Mo—Mobile equipment.
 MS—Marvel-Schebler Carburetor Div.
 M-Z—Marvel-Schebler or Zenith.
 No—No or none.
 Op—Opposed.
 Pe—Pedal.
 Pm—Pneumatic and mechanical.
 Pn—Pneumatic.
 Pu—Pumps.
 R—Rope.
 RA—Rope or automatic rewinding rope.
 Re—Recoil.

Re—Reed valves.
 REW—Recoil, rope, electric or wind up.
 RF—Refrigeration equipment.
 Rn—Rotary.
 Rr—Recoil or rope.
 RR—Rewinding rope.
 Rre—Recoil, rope or electric.
 RV—Rotary valve.
 SC—Suction feed.
 SZ—Marvel-Schebler or Zenith.
 T—"T" head, valves in head and side.
 TB—Tillotson and Brown.
 Til—Tiller.
 Till—Tillotson.
 Vee—"V" type engine.
 Ver—Vertical.
 Wal—Walbro.
 Y—Yes.
 Zen—Zenith Carburetor Div.

1960 OUTBOARD MOTORS

MAKE AND MODEL	Power Head	Number of Cylinders	Cylinder Material	Bore and Stroke (In.)	Piston Displacement (Cu. In.)	Engine Type	O.B.C. Rating	Engine Weight (Lb.) Fully Equipped	Piston Rings—Number and Width	Propeller			Fuel Tank Capacity (Pts.)	Gear			Magneto Make	Carburetor		Spark Plug—Make and Model	Bearings		Cooling System	Reverse	Lubrication—Pints Oil to 1 Gal. Gasoline
										Diameter (In.)	Pitch	Right or Left Hand Rotation		Ratio	Teeth Pitch	Material (S.A.E. No.)		Make	Size (In.)		Starting Mixture Obtained by—	Main				Connecting Rod
American Merc.	3 Port	2	CI	1 1/2 x 1 1/2	18.40	OT	8.0-3500	28	4-1/2	4	7 1/2	RR	5.00	14-21	12.00	4140	N	Wico	CH-110J	A	A	VP	N	3/4	
	Re-2 Port	1	ACS	2 1/2 x 1 1/2	3.78	S	2.0-4000	46	3-1/2	4	7 1/2	RR	20.00	14-21	12.00	4140	Wico	CH-110J	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	11.87	AF	7.5-4000	57	3-1/2	8	8	RR	34.00	14-23	12.00	4140	Wico	CH-110J	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	17.72	AF	12.0-4000	61	3-1/2	8	8	RR	34.00	14-23	10.00	4140	Wico	CH-110J	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	1 1/2 x 1 1/2	5.28	AF	3.0-4000	33	2-1/2	8 1/4	8 1/4	RR	5.00	17-28	15.68	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
Evinrude	Re-2 Port	2	ACS	1 1/2 x 1 1/2	8.26	AF	3.0-4000	33	2-1/2	8 1/4	8 1/4	RR	5.00	17-28	15.68	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
	Re-2 Port	2	ACS	1 1/2 x 1 1/2	8.84	AF	3.5-4000	34	2-1/2	8 1/4	8 1/4	RR	5.00	17-28	15.68	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	16.00	AF	10.0-4500	66	3-1/2	8 1/4	8 1/4	RR	48.00	15-26	12.40	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	22.00	AF	18.0-4500	79	3-1/2	8 1/4	8 1/4	RR	48.00	15-26	12.40	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	43.88	AF	40.0-4500	129	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
Gale	Re-2 Port	2	ACS	3 1/2 x 2 1/2	43.88	AF	40.0-4500	138	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	59.46	VT	75.0-4500	216	3-1/2	10	11	RR	48.00	20-23	8.00	8620	Oven	AC-M44C-4	P	P	Can	Y	3/4		
	Re-3 Port	3	ACS	2 1/2 x 1 1/2	5.32	S	3.0-4000	28	2-1/2	8 1/4	8 1/4	RR	8.00	13-22	13.96	8620	Oven	CH-J4JM	P	P	RP	N	3/4		
	Re-3 Port	3	ACS	2 1/2 x 1 1/2	8.84	AF	3.0-4000	28	2-1/2	8 1/4	8 1/4	RR	48.00	15-26	12.40	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
	Re-3 Port	3	ACS	2 1/2 x 1 1/2	19.94	AF	25.0-4500	117	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
Johnson	Re-2 Port	2	ACS	3 1/2 x 2 1/2	40.50	AF	35.0-4500	124	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	40.50	AF	35.0-4500	124	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
	Re-3 Port	3	ACS	3 1/2 x 2 1/2	70.70	AF	60.0-4500	205	3-1/2	10 1/4	10 1/4	RR	48.00	20-23	8.00	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
	Re-3 Port	3	ACS	3 1/2 x 2 1/2	70.70	AF	60.0-4500	205	3-1/2	10 1/4	10 1/4	RR	48.00	20-23	8.00	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
	Re-3 Port	3	ACS	3 1/2 x 2 1/2	70.70	AF	60.0-4500	205	3-1/2	10 1/4	10 1/4	RR	48.00	20-23	8.00	8620	Oven	CH-J4JM	P	P	Can	Y	3/4		
Mercury	Re-2 Port	2	ACS	1 1/2 x 1 1/2	5.28	AF	3.0-4000	34	2-1/2	8 1/4	8 1/4	RR	4.88	17-28	13.96	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	1 1/2 x 1 1/2	8.84	AF	3.0-4000	34	2-1/2	8 1/4	8 1/4	RR	48.00	15-26	12.40	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	16.00	AF	10.0-4500	67	3-1/2	8 1/4	8 1/4	RR	48.00	15-26	12.40	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	22.00	AF	18.0-4500	77	3-1/2	8 1/4	8 1/4	RR	48.00	15-26	12.40	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	40.50	AF	35.0-4500	126	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
Neptune	Re-2 Port	2	ACS	3 1/2 x 2 1/2	40.50	AF	35.0-4500	127	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	43.88	AF	40.0-4500	139	3-1/2	10 1/4	10 1/4	RR	48.00	12-21	7.50	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	70.70	VT	60.0-4500	217	3-1/2	12 1/2	14	RR	48.00	17-25	8.00	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	70.70	VT	60.0-4500	217	3-1/2	12 1/2	14	RR	48.00	17-25	8.00	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
	Re-2 Port	2	ACS	3 1/2 x 2 1/2	89.46	VT	75.0-4500	216	3-1/2	18	18	RR	48.00	20-23	8.00	8620	Oven	CH-J4JM	P	P	VP	N	3/4		
Offver	Re-2 Port	2	ACS	1 1/2 x 1 1/2	7.20	AF	6.0-6000	46	2-1/2	Var	Var	RR	25.00			8620	TH	CH-77J	A	A	VP	Y	3/4		
	Re-2 Port	2	ACS	1 1/2 x 1 1/2	15.50	AF	10.0-6000	77	2-1/2	Var	Var	RR	48.00			8620	TH	CH-77J	A	A	VP	Y	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	16.50	AF	11.0-6000	83	2-1/2	Var	Var	RR	48.00			8620	TH	CH-77J	A	A	VP	Y	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	22.00	AF	22.0-6000	86	2-1/2	Var	Var	RR	48.00			8620	TH	CH-77J	A	A	VP	Y	3/4		
	Re-2 Port	2	ACS	2 1/2 x 1 1/2	40.00	AF	35.0-6000	123	2-1/2	Var	Var	RR	48.00			8620	TH	CH-77J	A	A	VP	Y	3/4		

TUNE-UP DATA ON 1955-1960 CARS

MAKE AND MODEL	No. of Cylinders, Bore and Stroke (In.)	VALVES		IGNITION						CRANK- PIN		FRONT AXLE					
		Intake Valve Opens Deg. Before or After T.C.	Operating Tappett Clearance (In.)		Spark Plug		Timing		Crankcase Capacity (Qts.)	Cooling System Capacity (Qts.)	Compression Ratio (Standard Head)	Diam (In.)	Length (In.)	Caster (Deg.)	Camber (Deg.)	Toe-In (In.)	Kingpin Inclination (Deg.)
			Intake	Exhaust	Make and Model	Gap (In.)	Spark Occurs— No. Deg. Before or After T.C.	Breaker Gap In.									
AMERICAN MOTORS CORP.																	
NASH																	
5540.....1955	6-3 1/2 x 4 1/2	10B	.015H	.015H	AL-A7A	.030	4A	.022	4	41	7.45	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5560.....1955	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7	.030	4A	.022	6	17	7.60	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5580.....1955	8-3 1/2 x 3 1/2	14B	AA	AA	AL-AG-5	.035	5B	.016	5	20	8.25	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5640.....1956	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7	.030	TC	.016	4	11	7.47	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5660.....1956	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7	.030	4A	.019	6	17	7.60	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5680.....1956	6-4 x 3 1/2	14B	AA	AA	Ch-N-18	.036	5B	.016	5	27	9.55	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5780.....1957	6-4 x 3 1/2	12 1/2 B	AA	AA	AL-A7	.035	5B	.016	5	19	8.00	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
RAMBLER																	
5510.....1955	6-3 1/2 x 4 1/2	10B	.015H	.015H	AL-A7A	.030	4A	.022	4	11	7.30	2 1/2	1 1/2 P to 1 1/4 P	1 1/2 P to 1 1/4 P	0 to 1 1/2	8	8
5610.....1956	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7	.030	TC	.016	4	10	7.47	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5710.....1957	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7 1/2	.035	TC	.016	4	10	8.25	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5720.....1957	6-3 1/2 x 3 1/2	12 1/2 B	.012H	.014H	AL-A7 1/2	.035	5B	.016	5	20	8.00	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5730.....1957	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.014H	AL-A7 1/2	.035	5B	.016	5	19	9.50	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5801.....1958	6-3 1/2 x 4 1/2	10B	.016C	.018C	AL-AL-7 1/2	.035	5B	.016	4	11	8.00	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	8	8
5810.....1958	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7 1/2	.035	5B	.016	4	10	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5820.....1958	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7 1/2	.035	5B	.016	4	20	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5880.....1958	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-A7 1/2	.035	5B	.016	4	20	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5890.....1959	6-4 x 3 1/2	10B	.016C	.018C	AL-AL-7 1/2	.035	5B	.016	4	11	8.00	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	8	8
5910.....1959	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-AL-7 1/2	.035	5B	.016	4	10	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5920.....1959	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.014H	AL-AL-7 1/2	.035	TC	.016	4	20	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
5980.....1959	6-4 x 3 1/2	12 1/2 B	AA	AA	AL-AL-7 1/2	.035	5B	.016	4	19	9.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
6000 Deluxe & Super.....1960	6-3 1/2 x 4 1/2	10B	.016C	.018C	AL-AL-7 1/2	.035	5B	.020	4	11	8.00	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	8	8
6000 Custom.....1960	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-AL-7 1/2	.035	5B	.016	4	10	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
6010.....1960	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.016H	AL-AL-7 1/2	.035	5B	.016	4	10	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
6020.....1960	6-3 1/2 x 4 1/2	12 1/2 B	.012H	.014H	AL-AL-7 1/2	.035	TC	.017	4	20	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
6080.....1960	6-4 x 3 1/2	12 1/2 B	AA	AA	AL-AL-7 1/2	.035	TC	.017	4	19	8.70	2 1/2	0 to 1 1/2	1 1/2 N to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
CHECKER MOTORS CORP.																	
SUPERBA																	
A-10L.....1960	6-3 1/2 x 4 1/2	2B	.014H	.014H	Ch-UO-16	.020	4B	.020	5	12	7.30	2 1/2	1 1/2	1 1/2 P to 1 1/2 P	0 to 1 1/2	7	7
A-10.....1960	6-3 1/2 x 4 1/2	17B	.017H	.023H	Ch-N-8	.033	3 1/2 B	.020	5	12	8.00	2 1/2	1 1/2	1 1/2 P to 1 1/2 P	0 to 1 1/2	7	7
CHRYSLER CORP.																	
CHRYSLER																	
C67.....1955	6-3 1/2 x 3 3/4	15B	AA	AA	AL-45-165	.035	6B	.017	5	24	8.00	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
C68.....1955	6-3 1/2 x 3 3/4	15B	AA	AA	AL-4GS-175	.035	6B	.017	5	25	8.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
C300.....1955	6-3 1/2 x 3 3/4	35B	.015	.024	AL-4GS-200	.035	10B	.017	5	25	8.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
C71.....1956	6-3 1/2 x 3 3/4	5B	AA	AA	AL-AR-52	.035	2B	.017	5	24	8.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
C72.....1956	6-3 1/2 x 3 3/4	15B	AA	AA	AL-AGR-42	.035	6B	.017	5	25	9.00	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
C72-300.....1956	6-3 1/2 x 3 3/4	35B	.015H	.024H	AL (I)	.035	6B	.017	5	25	9.00	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
C75-1.....1957	6-3 1/2 x 3 3/4	13B	AA	AA	AL-AR-42	.035	6B	.017	5	21	9.25	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
C75-2.....1957	6-3 1/2 x 3 3/4	13B	AA	AA	AL-AR-42	.035	6B	.017	5	21	9.25	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
C76.....1957	6-4 x 3 1/2	15B	AA	AA	AL-AGR-42	.035	6B	.017	5	24	9.25	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
C76-300.....1957	6-4 x 3 1/2	35B	.015H	.024H	AL-AGR-42	.035	6B	.017	5	24	9.25	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LC1.....1958	6-3 1/2 x 3 3/4	13B	AA	AA	AL-AR-42	.035	6B	.017	4	21	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LC2.....1958	6-3 1/2 x 3 3/4	13B	AA	AA	AL-AR-42	.035	6B	.017	4	21	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LC3-H.....1958	6-4 x 3 1/2	15B	AA	AA	AL-AR-42	.035	6B	.017	5	24	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LC3-S.....1958	6-4 x 3 1/2	32B	.015H	.024H	AL-AGR-32	.035	6B	.017	5	24	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
MC1.....1959	6-4 1/2 x 3 1/2	15B	AA	AA	AL-A-42	.035	10B	.017	5	17	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
MC2.....1959	6-4 1/2 x 3 1/2	15B	AA	AA	AL-A-42	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
MC3.....1959	6-4 1/2 x 3 1/2	15B	AA	AA	AL-A-42	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
MC3-300E.....1959	6-4 1/2 x 3 1/2	20B	AA	AA	AL-A-32	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
PC1.....1960	6-4 1/2 x 3 1/2	15B	AA	AA	AL-A-42	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
PC2.....1960	6-4 1/2 x 3 1/2	15B	AA	AA	AL-A-42	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
PC3.....1960	6-4 1/2 x 3 1/2	15B	AA	AA	AL-A-42	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
PC3-300P.....1960	6-4 1/2 x 3 1/2	20B	AA	AA	AL-A-32	.035	10B	.017	5	16	10.10	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
DE SOTO																	
S21.....1955	6-3 1/2 x 3 1/2	12B	AA	AA	AL-45-165	.035	4B	.017	5	23	7.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
S22.....1955	6-3 1/2 x 3 1/2	4A	AA	AA	AL-4S-140	.035	10B	.017	5	23	7.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
S23.....1956	6-3 1/2 x 3 1/2	4A	AA	AA	AL-AR-51	.035	8B	.017	4	23	8.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
S24.....1956	6-3 1/2 x 3 1/2	15B	AA	AA	AL-AR-82	.035	4B	.017	4	23	8.50	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
S24 Adventurer.....1956	6-3 1/2 x 3 1/2	35B	AA	AA	AL (I)	.035	6B	.017	4	23	9.25	2 1/2	0 to 2N	1 1/2 N to 1 1/2 P	0 to 1 1/2	5 1/2	5 1/2
S27.....1957	6-3 1/2 x 3 1/2	10B	AA	AA	AL-AR-42	.035	6B	.017	5	20	8.50	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
S25, S26.....1957	6-3 1/2 x 3 1/2	15B	AA	AA	AL-AR-42	.035	6B	.017	5	20	9.25	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
S26 Adventurer.....1957	6-3 1/2 x 3 1/2	30B	AA	AA	AL-AR-42	.035	6B	.017	5	20	9.25	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LS1.....1958	6-4 1/2 x 3 1/2	15B	AA	AA	AL-AR-42	.035	6B	.017	4	16	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LS2, LS3-H.....1958	6-4 1/2 x 3 1/2	15B	AA	AA	AL-AR-42	.035	6B	.017	4	16	10.00	2 1/2	0 to 1 1/2	0 to 1 1/2 P	0 to 1 1/2	6 1/2	6 1/2
LS3-S.....1958	6-4 1/2 x 3 1/2	20B	AA	AA	AL-AR-32	.035	8B	.017	4	16	10.25	2 1/2	0 to 1 1/2	0			

TUNE-UP DATA ON 1955-1960 CARS

MAKE AND MODEL	No. of Cylinders, Bore and Stroke (In.)	VALVES		IGNITION				Crankcase Capacity (Qts.)	Cooling System Capacity (Qts.)	Compression Ratio (Standard Head)	CRANK-PIN		FRONT AXLE			
		Intake Valve Opens Before or After T.C.	Operating Tappet Clearance (In.)		Spark Plug		Timing				Diam. (In.)	Length (In.)	Caster (Deg.)	Camber (Deg.)	Toe-In (In.)	Kingpin Inclination (Deg.)
			Intake	Exhaust	Make and Model	Gap (In.)										
D500.....1956	6-3 1/2 x 3 1/2	128	.012H	.022H	AL-4S-250	.035	28	.029	5	20	9.25	2 1/2	0 to 2N	1/2 N to 1/2 P	1/2	5 1/2
D72.....1957	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-51	.035	TC	.029	5	13	8.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
D66, D67, D70, D71.....1957	6-3 1/2 x 3 1/2	100	AA	AA	AL-AR-42	.035	68	.017	5	20	8.50	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
D500.....1957	6-3 1/2 x 3 1/2	100	AA	AA	AL-AR-32	.035	68	.017	5	20	9.25	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
D501.....1957	6-3 1/2 x 3 1/2	358	.015H	.024H	AL-AR-32	.035	68	.017	5	21	10.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
LD1.....1958	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-51	.035	28	.020	5	13	8.00	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
LD2.....1958	6-3 1/2 x 3 1/2	100	AA	AA	AL-AR-42	.035	68	.017	5	20	9.00	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
LD3.....1958	6-4 1/2 x 3 1/2	158	AA	AA	AL-AR-42	.035	68	.017	4	16	10.00	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
D500.....1958	6-4 1/2 x 3 1/2	148	AA	AA	AL-AR-32	.035	68	.017	4	16	10.00	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
MD1.....1959	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-51	.025	21-8	.020	5	13	8.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
MD2.....1959	6-3 1/2 x 3 1/2	148	AA	AA	AL-AR-42	.035	108	.017	5	20	9.20	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
MD3.....1959	6-4 1/2 x 3 1/2	158	AA	AA	AL-A-42	.035	108	.017	5	16	10.10	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
D600.....1959	6-4 1/2 x 3 1/2	208	AA	AA	AL-A-42	.035	108	.017	5	16	10.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
PD1.....1960	6-4 1/2 x 3 1/2	118	AA	AA	AL-A-42	.035	108	.017	5	16	10.00	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
PD2.....1960	6-4 1/2 x 3 1/2	158	AA	AA	AL-A-42	.035	108	.017	5	16	10.00	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
DODGE-DART																
PD3.....1960	6-3 1/2 x 3 1/2	TC	.010H	.020H	AL-AG-42	.035	21-8	.020	4	13	8.50	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
PD4L & M.....1960	6-3 1/2 x 3 1/2	178	.010H	.018H	AL-A-42	.035	58	.017	5	20	9.00	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
PD4H.....1960	6-3 1/2 x 3 1/2	138	.010H	.018H	AL-A-42	.035	108	.017	5	20	9.00	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
D500.....1960	6-4 1/2 x 3 1/2	208	AA	AA	AL-A-3	.035	71-8	.017	5	16	10.00	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
D500.....1960	6-4 1/2 x 3 1/2	208	AA	AA	AL-A-3	.035	71-8	.017	5	16	10.00	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	5 1/2
IMPERIAL																
C68.....1955	6-3 1/2 x 3 1/2	158	AA	AA	AL-4GS-175	.035	68	.017	5	25	8.50	2 1/2	0 to 2N	1/2 N to 1/2 P	0 to 1/2	5 1/2
C70.....1955	6-3 1/2 x 3 1/2	158	AA	AA	AL-4GS-175	.035	68	.017	5	25	8.50	2 1/2	0 to 2N	1/2 N to 1/2 P	0 to 1/2	7
C73.....1956	6-3 1/2 x 3 1/2	158	AA	AA	AL-AR-42	.035	48	.017	5	25	9.00	2 1/2	0 to 2N	1/2 N to 1/2 P	1/2	7
1M1.....1957	6-4 1/2 x 3 1/2	158	AA	AA	AL-AR-42	.035	68	.017	5	24	9.25	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
LY1.....1958	6-4 1/2 x 3 1/2	158	AA	AA	AL-AR-42	.035	68	.017	5	24	10.00	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
MY1.....1959	6-4 1/2 x 3 1/2	158	AA	AA	AL-A-42	.035	108	.017	5	16	10.10	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
PY1.....1960	6-4 1/2 x 3 1/2	158	AA	AA	AL-A-42	.035	108	.017	5	16	10.10	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
PLYMOUTH																
P-26.....1955	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-4S-140	.035	28	.020	5	13	7.40	2 1/2	0 to 2N	1/2 N to 1/2 P	0 to 1/2	5 1/2
P-27.....1955	6-3 1/2 x 3 1/2	148	AA	AA	AL-4S-168	.035	48	.018	5	19	7.50	1 1/2	0 to 2N	1/2 N to 1/2 P	0 to 1/2	5 1/2
P-27.....1955	6-3 1/2 x 3 1/2	148	AA	AA	AL-4S-168	.035	48	.018	5	19	7.50	1 1/2	0 to 2N	1/2 N to 1/2 P	0 to 1/2	5 1/2
P-28.....1956	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-50	.035	20	.020	5	13	7.50	2 1/2	0 to 2N	1/2 N to 1/2 P	1/2	5 1/2
P-29.....1956	6-3 1/2 x 3 1/2	148	AA	AA	AL-AR-52	.035	48	.017	5	19	8.00	1 1/2	0 to 2N	1/2 N to 1/2 P	1/2	5 1/2
P-29.....1956	6-3 1/2 x 3 1/2	148	.012H	.020H	AL-AR-52	.035	48	.017	5	20	8.00	2 1/2	0 to 2N	1/2 N to 1/2 P	1/2	5 1/2
P-29.....1956	6-3 1/2 x 3 1/2	98	.010H	.018H	AK-4S-250	.035	48	.017	5	20	9.25	2 1/2	0 to 2N	1/2 N to 1/2 P	1/2	5 1/2
P-30.....1957	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-51	.035	TC	.020	5	13	8.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
P-31.....1957	6-3 1/2 x 3 1/2	88	.008H	.018H	AL-AR-52	.035	48	.017	5	20	8.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
P-31-2, P-31-3.....1957	6-3 1/2 x 3 1/2	88	.008H	.018H	AL-AR-52	.035	88	.017	5	20	8.50	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
P-31 Fury.....1957	6-3 1/2 x 3 1/2	178	.008H	.018H	AL-AR-32	.035	88	.017	5	20	9.25	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
LP1.....1958	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-51	.035	28	.020	5	13	8.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
LP2.....1958	6-3 1/2 x 3 1/2	88	.012H	.018H	AL-AR-42	.035	108	.017	5	20	9.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
LP2-H Fury.....1958	6-3 1/2 x 3 1/2	178	.018H	.018H	AL-AR-42	.035	108	.017	5	20	9.25	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
Golden Commando.....1958	6-4 1/2 x 3 1/2	158	AA	AA	AL-AR-32	.035	88	.017	4	16	10.00	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	5 1/2
MP1.....1959	6-3 1/2 x 3 1/2	128	.010H	.010H	AL-AR-51	.035	21-8	.020	5	13	8.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
MP2.....1959	6-3 1/2 x 3 1/2	178	.010H	.018H	AL-AR-42	.035	108	.017	5	20	9.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
MP2 Sport Fury.....1959	6-3 1/2 x 3 1/2	138	.010H	.018H	AL-AR-42	.035	108	.017	5	20	9.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
Golden Commando.....1959	6-4 1/2 x 3 1/2	208	AA	AA	AL-A-32	.035	71-8	.017	5	16	10.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
PP1.....1960	6-3 1/2 x 3 1/2	TC	.010H	.020H	AL-AG-42	.035	21-8	.020	4	13	8.50	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
PP2.....1960	6-3 1/2 x 3 1/2	178	.010H	.018H	AL-A-42	.035	58	.017	5	20	9.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
Golden Commando.....1960	6-4 1/2 x 3 1/2	248	AA	AA	AL-A-32	.035	108	.017	5	16	10.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
Golden Commando.....1960	6-4 1/2 x 3 1/2	248	AA	AA	AL-A-32	.035	21-8	.017	5	16	10.00	2 1/2	0 to 1 1/2 N	0 to 1 1/2 P	1/2	5 1/2
VALIANT																
V160, V200.....1960	6-3 1/2 x 3 1/2	88	.010H	.020H	AL-AG-42	.035	58	.020	4	13	8.50	2 1/2	1/2 N to 1 1/2 N	1/2 P to 1 1/2 P	1/2	7 1/2
FORD MOTOR CO.																
COMET																
6.....1960	6-3 1/2 x 2 1/2	158	.016H	.016H	Ch-F14Y	.034	28	.025	3 1/2	8 1/2	8.70	2 1/2	1P to 2P	1/2 P to 1 1/2 P	1/2	7
CONTINENTAL																
60A.....1956	6-4 x 3 1/2	188	AA	AA	Ch-960	.034	58	.015	5	23	9.00	2 1/2	1/2 P to 1 1/2 P	0 to 1 1/2 P	1/2	7 10'
60A.....1957	6-4 x 3 1/2	188	AA	AA	Ch-960	.034	58	.015	5	23	10.00	2 1/2	1/2 P to 1 1/2 P	0 to 1 1/2 P	1/2	7 10'
EDSEL																
Ranger, Pacer.....1958	6-4 1/2 x 3 1/2	228	AA	AA	Ch-F-11-Y	.034	78	.015	5	18 1/2	10.50	2 1/2	0 to 1P	1/2 P to 1P	1/2	7
Corsair, Citation.....1958	6-4 1/2 x 3 1/2	228	AA	AA	Ch-F-11-Y	.034	78	.015	5	22	10.50	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	7
Ranger.....1959	6-3 1/2 x 3 1/2	178	.019H	.019H	Ch-870	.034	48	.025	4	18	8.40	2 1/2	0 to 1P	1/2 P to 1 1/2 P	1/2	8 1/2
Ranger.....1959	6-3 1/2 x 3 1/2	128	.019H	.019H	Ch-F-14Y	.034	38	.015	5	19	8.80	2 1/2	0 to 1P	1/2 P to 1 1/2 P	1/2	8 1/2
Corsair.....1959	6-4 x 3 1/2	228	AA	AA	Ch-F-11Y	.034	38	.015	5	19	8.80	2 1/2	0 to 1P	1/2 P to 1 1/2 P	1	8 1/2
Ranger, Corsair.....1959	6-4 1/2 x 3 1/2	228	AA	AA	Ch-F-11Y	.034	38	.015	5	19	9.60	2 1/2	0 to 1P	1/2 P to 1 1/2 P	1/2	8 1/2
Ranger.....1960	6-3 1/2 x 3 1/2	178	.019H	.019H	Ch-870	.034	48	.025	4	18	8.40	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	8 1/2
Ranger.....1960	6-3 1/2 x 3 1/2	128	.019H	.019H	Ch-F14Y	.034	38	.015	5	19	8.80	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	8 1/2
Ranger.....1960	6-4 x 3 1/2	268	AA	AA	Ch-F14Y	.034	38	.015	5	19	9.60	2 1/2	0 to 1 1/2 P	0 to 1 1/2 P	1/2	8 1/2
FALCON																
6.....1960	6-3 1/2 x 2 1/2	158	.016H	.016H	Ch-F14Y	.034	28	.025	3 1/2	8 1/2	8.70	2 1/2	1P to 2P	1/2 P to 1 1/2 P	1/2	7

TUNE-UP DATA ON 1955-1960 CARS

MAKE AND MODEL	No. of Cylinders, Bore and Stroke (In.)	VALVES		IGNITION				Cooling System Capacity (Qts.)	Compression Ratio (Standard Head)	CRANK-PIN		FRONT AXLE			
		Operating Tappet Clearance (In.)	Spark Plug	Timing	Crankcase Capacity (Qts.)	Diam. (In.)	Length (In.)			Caster (Deg.)	Camber (Deg.)	Toe-In (In.)	Kingpin Inclination (Deg.)		
														Intake Valve Opens Before or After T.C.	Intake	Exhaust	Make and Model	Gap (In.)	Spark Occurs—No. of Degrees After T.C.	Breaker Gap (In.)
FORD															
6.....1955	6-3 1/2 x 3 1/2	13B	.015H .019H	Ch-870	.034 3B	.025 4	15	7.50	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
V8.....1955	8-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 6B	.015 5	19	7.60	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
6.....1956	6-3 1/2 x 3 1/2	24B	.019H .019H	Ch-870	.034 4B	.025 4	14 1/2	8.00	2 1/2	1 1/2	0 to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
6, Main, Cust.....1956	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	19	8.00	2 1/2	1 1/2	0 to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
6, Fairlane.....1956	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	19	8.00	2 1/2	1 1/2	0 to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
6.....1957	6-3 1/2 x 3 1/2	17B	.019H .019H	Ch-870	.034 4B	.025 4	15	8.60	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 1'	
6, Cust., Cust. 300.....1957	6-3 1/2 x 3 1/2	18B	.019H .019H	Ch-870	.034 4B	.025 5	15	8.60	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 1'	
6, Fairlane, Fair. 500.....1957	6-3 1/2 x 3 1/2	18B	.019H .019H	Ch-870	.034 4B	.025 5	19	9.10	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 1'	
6.....1958	6-3 1/2 x 3 1/2	17B	.019H .019H	Ch-870	.034 4B	.025 4	15	8.60	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 7'	
6, Cust. 300, Fair.....1958	8-3 1/2 x 3 1/2	18B	.019H .019H	Ch-F-11-Y	.034 3B	.015 5	19	9.10	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 7'	
6, Cust. 300, Fair, 500.....1958	8-4 x 3 1/2	21B	.025H .026H	Ch-F-11-Y	.034 3B	.015 5	19	9.50	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 7'	
6, Fair. 500.....1958	8-4 x 3 1/2	21B	.026H .026H	Ch-F-11-Y	.034 3B	.015 5	19	10.20	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 7'	
6.....1959	6-3 1/2 x 3 1/2	17B	.019H .019H	Ch-870	.034 4B	.025 4	15	8.40	2 1/2	1 1/2	0 to 1° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1959	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-F-14Y	.034 3B	.015 5	19	8.60	2 1/2	1 1/2	0 to 1° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1959	8-4 x 3 1/2	22B	AA AA	Ch-F-11Y	.034 3B	.015 5	19	8.90	2 1/2	1 1/2	0 to 1° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1959	8-4 x 3 1/2	22B	AA AA	Ch-F-11Y	.034 3B	.015 5	19	9.60	2 1/2	1 1/2	0 to 1° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1960	6-3 1/2 x 3 1/2	17B	.019H .019H	Ch-870	.034 4B	.025 4	15	8.40	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1960	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-F-14Y	.034 3B	.015 5	19	8.60	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1960	8-4 x 3 1/2	22B	AA AA	Ch-F-11Y	.034 3B	.015 5	19	8.90	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	8 1/2'	
6.....1960	8-4 x 3 1/2	26B	AA AA	Ch-F-11Y	.034 6B	.015 5	19	9.60	2 1/2	1 1/2	1° to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	8 1/2'	
LINCOLN															
73A, 73B.....1955	8-3 1/2 x 3 1/2	6B	AA AA	Ch-870	.034 5B	.015 5	23	8.50	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7° 10'	
73A, 73B.....1956	8-4 x 3 1/2	18B	AA AA	Ch-880	.034 5B	.015 5	23 1/2	9.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7° 12'	
57A, B, 58A, B, 60A, B, 70B.....1957	8-4 x 3 1/2	18B	AA AA	Ch-880	.034 5B	.015 5	23	10.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7° 12'	
8.....1958	8-4 x 3 1/2	27B	AA AA	Ch-F-11-Y	.032 6B	.015 5	23	10.90	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7° 26'	
8.....1959	8-4 x 3 1/2	22B	AA AA	Ch-F-11-Y	.032 6B	.015 5	23	10.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7° 26' 10"	
8.....1960	8-4 x 3 1/2	22B	AA AA	Ch-F-11-Y	.034 6B	.015 5	23	10.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7° 26'	
MERCURY															
Custom, Monterey.....1955	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	19	7.60	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Montclair.....1955	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	19	8.50	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
8.....1956	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	19	8.50	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
57A, B, 58A, B.....1957	6-3 1/2 x 3 1/2	18B	.019H .019H	Ch-880	.034 3B	.015 5	20	9.70	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
75A.....1957	8-4 x 3 1/2	18B	AA AA	Ch-880	.034 3B	.015 5	23	9.70	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Montclair.....1958	8-4 x 3 1/2	34B	AA AA	Ch-880	.034 4B	.015 5	20 1/2	10.50	2 1/2	2	2	1° to 1 1/2° N	0 to 1 1/2° N	1/4 to 1/2	7°
Montclair.....1958	8-4 x 3 1/2	34B	AA AA	Ch-880	.034 7B	.015 5	20 1/2	10.50	2 1/2	2	2	1° to 1 1/2° N	0 to 1 1/2° N	1/4 to 1/2	7°
Park Lane.....1958	8-4 x 3 1/2	27B	AA AA	Ch-880	.034 7B	.015 5	20 1/2	10.50	2 1/2	2	2	1° to 1 1/2° N	0 to 1 1/2° N	1/4 to 1/2	7°
Montclair.....1959	6-3 1/2 x 3 1/2	18B	.019H .019H	Ch-F-14-Y	.032 3B	.015 5	20	8.75	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Montclair.....1959	8-4 x 3 1/2	22B	AA AA	Ch-F-11-Y	.032 6B	.015 5	21	10.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Park Lane.....1959	6-4 x 3 1/2	22B	AA AA	Ch-F-11-Y	.032 6B	.015 5	21	10.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Montclair.....1960	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-F-14-Y	.034 3B	.015 5	20	8.90	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Montclair.....1960	8-4 x 3 1/2	22B	AA AA	Ch-F-11-Y	.034 6B	.015 5	21	8.50	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
Montclair, Park Lane.....1960	8-4 x 3 1/2	22B	AA AA	Ch-F-11-Y	.034 6B	.015 5	21	10.00	2 1/2	1 1/2	0 to 1 1/2° N	0 to 1 1/2° P	1/4 to 1/2	7°	
THUNDERBIRD															
40A, 40B.....1955	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 6B	.015 5	20	8.10	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
40A, 40B.....1956	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	21	8.40	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
40A, 40B.....1956	6-3 1/2 x 3 1/2	12B	.019H .019H	Ch-870	.034 3B	.015 5	21	(k)	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7° 7'	
40A, 40B.....1957	6-3 1/2 x 3 1/2	18B	.019H .019H	Ch-870	.034 3B	.015 5	20	9.10	2 1/2	1 1/2	0 to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 7'	
40A, 40B.....1957	6-3 1/2 x 3 1/2	18B	.019H .019H	Ch-870	.034 3B	.015 5	20	9.70	2 1/2	1 1/2	0 to 1 1/2° P	0 to 1 1/2° P	1/4 to 1/2	7° 7'	
63A.....1958	8-4 x 3 1/2	21B	.025H .026H	Ch-F-11Y	.034 4B	.015 5	19 1/2	10.20	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
63A, 76A.....1959	8-4 x 3 1/2	22B	AA AA	Ch-F-11Y	.034 4B	.015 5	19 1/2	9.60	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
63A, 76A.....1959-60	10 1/2 x 3 1/2	22B	AA AA	Ch-F-11Y	.034 4B	.015 5	22 1/2	10.00	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
63A, 76A.....1960	8-4 x 3 1/2	26B	AA AA	Ch-F-11Y	.034 4B	.015 5	19	9.60	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	8 1/2'	
GENERAL MOTORS CORP.															
BUICK															
40.....1955	6-3 1/2 x 3 1/2	25B	AA AA	AC-44-5	.033 5B	.015 6	16 1/2	8.40	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	0 to 1/4	0	
50, 60.....1955	8-4 x 3 1/2	25B	AA AA	AC-44-5	.033 5B	.015 6	16 1/2	9.00	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	0 to 1/4	0	
70.....1955	8-4 x 3 1/2	25B	AA AA	AC-44-5	.033 5B	.015 6	16 1/2	9.00	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	0 to 1/4	0	
40.....1956	8-4 x 3 1/2	25B	AA AA	AC-44	.033 5B	.015 6	17 1/2	8.90	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
50, 60, 70.....1956	8-4 x 3 1/2	30B	AA AA	AC-44	.033 5B	.015 6	17 1/2	9.50	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
40.....1957	8-4 x 3 1/2	25B	AA AA	AC-44	.033 5B	.015 5	16 1/2	9.50	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
50, 60, 70.....1957	8-4 x 3 1/2	34B	AA AA	AC-44	.033 5B	.015 5	16 1/2	10.00	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
40.....1958	8-4 x 3 1/2	25B	AA AA	AC-44	.033 5B	.015 5	16 1/2	9.50	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
50, 60, 70, 700.....1958	8-4 x 3 1/2	34B	AA AA	AC-44	.033 5B	.015 5	16 1/2	10.00	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
4400.....1959	8-4 x 3 1/2	25B	AA AA	AC-44S	.033 5B	.015 4	16 1/2	8.50	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
4600, 4700, 4800.....1959	8-4 x 3 1/2	33B	AA AA	AC-44S	.033 12B	.015 4	16 1/2	10.50	2 1/2	1 1/2	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7	
4400.....1960	8-4 x 3 1/2	25B	AA AA	AC-44S	.033 5B	.015 4	17	8.50	2 1/2	1 1/2	2N	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7
4600, 4700, 4800.....1960	8-4 x 3 1/2	33B	AA AA	AC-44S	.033 12B	.015 4	17	10.25	2 1/2	1 1/2	2N	1° to 1 1/2° P	1° to 1 1/2° P	1/4 to 1/2	7
CADILLAC															
60, 62, 75.....1955	8-3 1/2 x 3 1/2	19B	AA AA	AC-44-5	.035 2 1/2-B	.019 5	18	9.00	2 1/2	1 1/2	0 to				

TUNE-UP DATA ON 1955-1960 CARS

MAKE AND MODEL	No. of Cylinders, Bore and Stroke (In.)	VALVES		IGNITION				CRANK-PIN		FRONT AXLE							
		Intake Valve Opens Before or After T.C.	Operating Tapsett Clearance (In.)	Spark Plug		Timing		Crankcase Capacity (Qtz.)	Cooling System Capacity (Qtz.)	Compression Ratio (Standard Head)	Diam. (In.)	Length (In.)	Caster (Deg.)	Camber (Deg.)	Toe-In (In.)	Kingpin Inclination (Deg.)	
				Intake	Exhaust	Make and Model	Gap (In.)										Spark Occurs—No. Deg. Before or After T.C.	Breaker Gap (In.)
1500, 2100, 2400.....1957	8-3 1/2 x3	18B	AA	AA	AC-44	.036	4B	.....	3 1/2	16	8.00	2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	4
1500, 2100, 2400.....1957	8-3 1/2 x3	12 1/2 B	AA	AA	AC-44	.036	4B	.....	3 1/2	16	8.50	2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	4
1100, 1500, 1700.....1958	6-3 1/2 x3 1/2	10 1/2 B	AA	AA	AC-44	.035	TC	.019	5	16	8.25	2 1/2	1	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1200, 1600, 1800.....1958	8-3 1/2 x3	12 1/2 B	AA	AA	AC-44	.035	4B	.019	4	16	8.50	2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1200, 1600, 1800.....1958	8-4 1/2 x3 1/2	18 1/2 B	AA	AA	AC-44	.035	4B	.019	4	22	9.50	2 1/2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1100, 1500, 1700.....1958	6-3 1/2 x3 1/2	10 1/2 B	AA	AA	AC-44	.036	5B	.019	5	17 1/2	8.25	2 1/2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1200, 1600, 1800.....1958	8-3 1/2 x3	12 1/2 B	AA	AA	AC-44	.035	4B	.019	4	17 1/2	8.50	2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1200, 1600, 1800.....1958	8-4 1/2 x3 1/2	18 1/2 B	AA	AA	AC-44	.035	4B	.019	4	21	9.50	2 1/2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1100, 1300, 1500, 1700.....1960	6-3 1/2 x3 1/2	10 1/2 B	AA	AA	AC-44	.036	5B	.019	5	17 1/2	8.25	2 1/2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1200, 1400, 1600, 1800.....1960	8-3 1/2 x3	18B	AA	AA	AC-44	.036	4B	.019	4	17 1/2	8.50	2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
1200, 1400, 1600, 1800.....1960	8-4 1/2 x3 1/2	18 1/2 B	AA	AA	AC-44N	.036	5B	.019	4	21	9.50	2 1/2	1 1/2	1 1/2	0 to 1P	1/2 to 3/4	7 1/2
CORVAIR																	
800, 700.....1960	8-3 1/2 x2 1/2	43B	AA	AA	AC-44FF	.035	4B	.019	4	CB	8.00	1 1/2	2 1/2	P to 3P	0 1P	3/8 to 1/2	7
CORVETTE																	
2900.....1955	8-3 1/2 x3 1/2	19 1/2 B	.010H	.020H	AC-44-5	.036	2A	.016	5	17 1/2	8.00	2 1/2	1	0 to 1P	0 to 1P	0 to 3/8	4
2900.....1956	8-3 1/2 x3	21 1/2 B	.008H	.018H	AC-C-43	.036	4B	.019	5	16	9.25	2	1	0 to 1P	0 to 1P	0 to 3/8	4
2900.....1957	8-3 1/2 x3	12 1/2 B	AA	AA	AC-44	.036	4B	.016	5	16	9.50	2	1	0 to 1P	0 to 1P	0 to 3/8	4
867.....1958	8-3 1/2 x3	12 1/2 B	AA	AA	AC-46	.035	4B	.016	5	16	9.50	2	1 1/2	1 1/2	0 to 1/2	4	4
867.....1959	8-3 1/2 x3	12 1/2 B	AA	AA	AC-46	.036	4B	.016	5	18 1/2	9.50	2	1 1/2	1 1/2	0 to 1/2	4	4
878.....1960	8-3 1/2 x3	12 1/2 B	AA	AA	AC-44	.036	4B	.019	5	15 1/2	9.50	2	1 1/2	1 1/2	0 to 1/2	4	4
OLDSMOBILE																	
88, 98.....1955	8-3 1/2 x3 1/2	13 1/2 B	AA	AA	AC-44-5	.030	5B	.016	5	20 1/2	8.50	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	5° 51'	5° 51'
88, 98.....1956	8-3 1/2 x3 1/2	11 1/2 B	AA	AA	AC-44	.030	5B	.016	5	20 1/2	9.25	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	5° 51'	5° 51'
88, 98.....1957	8-4 1/2 x3 1/2	13 1/2 B	AA	AA	AC-44	.030	5B	.016	5	20	9.50	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	7	7
88, 98.....1958	8-4 1/2 x3 1/2	16B	AA	AA	AC-44	.030	5B	.016	4	20	10.00	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	6 to 7	7
Dynamic 88.....1959	8-4 1/2 x3 1/2	16B	AA	AA	AC-44	.030	5B	.016	4	20	9.75	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	6 to 7	10
Super 88, 98.....1959-60	8-4 1/2 x3 1/2	16B	AA	AA	AC-44	.030	5B	.016	4	20	9.75	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	6 to 7	10
Dynamic 88.....1960	8-4 1/2 x3 1/2	14B	AA	AA	AC-45	.030	5B	.016	4	20	8.75	2 1/2	3/4	0 to 1N	1/2 N to 1/2 P	6 to 7	10
PONTIAC																	
27, 28.....1955	8-3 1/2 x3 1/2	22B	AA	AA	AC-44-5	.036	5B	.016	5	24	8.00	2 1/2	3/4	1N to 1 1/2 N	0 to 1P	0 to 3/8	1° 20'
27, 28.....1956	8-3 1/2 x3 1/2	22 1/2 B	AA	AA	AC-44	.036	5B	.016	5	22 1/2	8.90	2 1/2	3/4	1N to 1 1/2 N	0 to 1P	0 to 3/8	1° 20'
27, 28.....1957	8-3 1/2 x3 1/2	22B	AA	AA	AC-45	.036	6B	.016	5	21 1/2	8.50	2 1/2	3/4	1N to 1 1/2 N	0 to 1P	0 to 3/8	1° 20'
28, 27, 28.....1958	8-4 1/2 x3 1/2	22B	AA	AA	AC-45	.036	6B	.016	5	21 1/2	8.00	2 1/2	3/4	1N to 1N	0 to 1P	0 to 3/8	4° 50'
21, 24, 27, 28.....1959	8-3 1/2 x3 1/2	22B	AA	AA	AC-46	.036	6B	.016	5	21 1/2	8.00	2 1/2	3/4	1N to 2N	1/2 N to 1/2 P	0 to 4° 50'	4° 50'
21, 23, 24.....1960	8-4 1/2 x3 1/2	14B	AA	AA	AC-45S	.036	6B	.016	5	22	8.00	2 1/2	3/4	1N to 2N	1/2 N to 1/2 P	0 to 4° 50'	4° 50'
27, 28.....1960	8-4 1/2 x3 1/2	30B	AA	AA	AC-45S	.036	6B	.016	5	22	8.00	2 1/2	3/4	1N to 2N	1/2 N to 1/2 P	0 to 4° 50'	4° 50'
STUDEBAKER-PACKARD CORP.																	
PACKARD																	
5540.....1955	8-3 1/2 x3 1/2	14B	AA	AA	(g)	.035	6B	.016	5	26	8.50	2 1/2	3/4	1N to 1 1/2 N	1/2 N to 1/2 P	0 to 5° 50'	5° 50'
5540, 5580.....1955	8-4 1/2 x3 1/2	14B	AA	AA	Ch-H-10	.035	6B	.016	5	26	8.50	2 1/2	3/4	1N to 1 1/2 N	1/2 N to 1/2 P	0 to 5° 50'	5° 50'
5640, 5660.....1956	8-4 1/2 x3 1/2	14B	AA	AA	Ch-N-1E	.035	5B	.016	5	26	9.50	2 1/2	3/4	1N to 1 1/2 N	1/2 N to 1/2 P	0 to 5° 50'	5° 50'
5680, 5688.....1956	8-4 1/2 x3 1/2	14B	AA	AA	Ch-N-1E	.035	10B	.016	5	26	10.00	2 1/2	3/4	1N to 1 1/2 N	1/2 N to 1/2 P	0 to 5° 50'	5° 50'
57L.....1957	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-10	.036	4B	.016	5	17	7.50	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
57L.....1958	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18Y	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
58L-K.....1958	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18Y	.036	4B	.016	5	17	7.80	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
STUDEBAKER																	
16G6.....1955	8-3 1/2 x3 1/2	15B	.016C	.016C	Ch-J-7	.031	2B	.020	5	10	7.50	1 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
16G8 (Early).....1955	8-3 1/2 x2 1/2	19B	.024H	.024H	Ch-H-11	.035	6B	.013	5	17 1/2	7.50	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
16G8 (Late), 6H.....1955	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-11	.036	4B	.016	5	17 1/2	7.50	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
56B.....1956	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-11	.036	4B	.016	5	17	7.80	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
56C.....1956	8-3 1/2 x3 1/2	15B	.016C	.016C	Ch-J-7	.031	2B	.020	5	11	7.80	1 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
56H.....1956	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-11	.036	4B	.016	5	17	7.80	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
56J.....1956	8-4 1/2 x3 1/2	14B	AA	AA	Ch-N1987B	.036	5B	.015	5	25	8.00	2 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
57D.....1957	8-3 1/2 x3 1/2	15B	.016C	.016C	Ch-J-7	.031	2B	.020	5	11	7.80	1 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
57B.....1957	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-11	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
57H.....1957	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-11	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
57H Golden Hawk.....1957	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-11	.036	4B	.016	5	17	7.50	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
58C.....1958	8-3 1/2 x3 1/2	15B	.016C	.016C	Ch-J-7	.031	2B	.020	5	11	7.80	1 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
58B.....1958	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18-Y	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	1N to 1P	1/2 to 3/4	6
58H.....1958	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18-Y	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
58H Golden Hawk.....1958	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18-Y	.036	4B	.016	5	17	7.80	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
59S.....1959	8-3 1/2 x3 1/2	15B	.016C	.016C	Ch-J-7	.031	2B	.020	5	11	8.30	1 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
59V.....1959	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18-Y	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1N	1/2 to 3/4	6
60S.....1960	8-3 1/2 x3 1/2	15B	.016C	.016C	Ch-J-7	.031	2B	.020	5	11	8.30	1 1/2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
60V.....1960	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18Y	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6
60V Hawk.....1960	8-3 1/2 x3 1/2	11B	.024H	.024H	Ch-H-18Y	.036	4B	.016	5	17	8.30	2	3/4	1N to 2 1/2 N	0 to 1P	1/2 to 3/4	6

FOOTNOTES AND ABBREVIATIONS

- †—Adjustment automatic with hydraulic valve lifters when equipped with Powerglide transmission.
- ‡—10 1/2 B when equipped with Powerglide transmission.
- Le Mans Dual Jet engine which is available at extra cost.
- ‡‡—3B when equipped with Hydramatic transmission.
- ¶—10 1/2 when equipped with Dynaflo transmission.
- ¶¶—Eldorado uses AC-43-S.

- 44—18B when equipped with Powerglide transmission.
- 25 when equipped with air conditioning.
- 26 1/2 B with Powerglide; 31 1/2 B with two four-barrel carburetors.
- §—1008 with two four-barrel carburetors.
- ¶—016 with two four-barrel carburetors.
- 350 with optional camshaft for std. transmission.
- 6B with automatic transmission.
- 27B with Hydramatic.

- 1—Left side only; right side, 1/2 N to 1/2 P.
- 2—Early cars only; late cars, 18B.
- 3—Or Ch-H-10-64D.
- 4—SB with Mercromatic transmission.
- 5—Or Ch-H-10 or AC-45L.
- 6—Or Ch-H-10.
- 7—Or Al-ARL-82, Ch-H-16, Ch-H-18Y.
- 8—Left side only; right side, 1/2 N to 1/2 P.
- (g)—AL-AC-5 or Ch-H-10.
- (i)—AGR-41 or AGR-31.
- (j)—AR-31 or AS-250.
- (k)—8.40 with overdrive; 9.00 with Fordomatic.

- A—After.
- AA—Automatic adjustment with hydraulic valve lifters.
- AC—A. C. Spark Plug Div.
- AL—The Electric

Small Gasoline Engine Service Data

MAKE AND MODEL	IGNITION SYSTEM				VALVE SYSTEM								FUEL SYSTEM		PISTON		RINGS		Connecting Rod Bearing Type	Pistons Removed From			
	Breaker Point Gap (In.)	Spark Plug	Make and Model	Size (Mm.)	Tappet Clearance		Seat Angle	Valve Timing				Timing Mark Location	Fuel Pump Make	Fuel Screen Location	Carburetor Float Level Height (In.)	Compression		Oil					
					Intake (In.)	Exhaust (In.)		Intake (Deg.)	Exhaust (Deg.)	Opens (Deg. B.T.C.)	Closes (Deg. A.B.C.)					Opens (Deg. B.B.C.)	Closes (Deg. A.T.C.)				Number	Size (In.)	
																							Number	Size (In.)
BRIGGS & STRATTON																							
9	1950-60	.020	Ch-J8	14	.025	.008	.015	45	45	24	47	57	14	CC	Own	FF	PCB	2	1/8	1	1/8	A	C
14, 19	1950-60	.020	Ch-J8	14	.025	.008	.015	45	45	27	50	60	17	CC	Own	FF	PCB	2	1/8	1	1/8	A	C
23A	1950-60	.020	Ch-J8	14	.025	.008	.018	45	45	28	51	61	18	CC	Own	FF	PCB	2	1/8	1	1/8	A	C
80100, 80500, 81100, 81500, 80100,	1950-60	.020	Ch-J8	14	.025	.006	.010	45	45	27	50	60	17	CC	N	...	...	2	1/8	1	1/8	A	C
80300, 80400, 80700, 81300, 81400,	1950-60	.020	Ch-J8	14	.025	.006	.010	45	45	27	50	60	17	CC	Own	...	...	2	1/8	1	1/8	A	C
81700, 80300, 80400, 80700, 81300,	1950-60	.020	Ch-J8	14	.025	.006	.010	45	45	27	50	60	17	CC	Own	...	...	2	1/8	1	1/8	A	C
81400, 81700	1950-60	.018	Ch-J8	14	.025	.006	.010	45	45	27	50	60	17	CC	N	...	...	2	1/8	1	1/8	A	C
141300, 143300, 141700, 143700	1950-60	.018	Ch-J8	14	.025	.006	.010	45	45	27	50	60	17	CC	N	...	...	2	1/8	1	1/8	A	C
80560, 80860, 80900	1960	.020	Ch-J8	14	.025	.006	.010	45	45	27	50	60	18	CC	N	GT	F	2	1/8	1	1/8	A	C
141400, 143400	1960	.018	Ch-J8	14	.025	.006	.010	45	45	27	50	60	17	CC	N	...	...	2	1/8	1	1/8	A	C
CLINTON																							
100 Series, VS2100 Series, VS3100	1950-60	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
Series	1950-60	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
VS200 Series	1950-60	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
D700 Series, 1200 Series	1950-60	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
900 Series	1950-60	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	Own	GT	.160	2	1/8	1	1/8	A	C
V1100 Series, VS4100 Series	1950-60	.020	Ch-H10	14	.025	.011	.011	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
1600, 2500 Series, 2700 Series	1950-60	.020	Ch-H10	14	.025	.011	.011	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
V100, VS100, V3100, VS4100	1960	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
VS400 Series	1960	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	...	GT	.160	2	1/8	1	1/8	A	C
V1100, V1200	1960	.020	Ch-H10	14	.025	.010	.010	44	44	26	44	56	14	CCr	Own	GT	.160	2	1/8	1	1/8	A	C
A1600, B2500	1960	.020	Ch-H10	14	.025	.011	.011	44	44	26	44	56	14	CCr	Own	GT	.160	2	1/8	1	1/8	A	C
CONTINENTAL																							
AUT0, AUT0R	1950-60	.019	Ch or AL	14	.025	.014	.014	45	45	20	50	50	6	...	...	FTV	...	2	1/8	1	1/8	A	...
AU7, AU7B, AU7R, AD7, AW7,	1950-60	.019	Ch or AL	14	.025	.014	.014	45	45	...	...	...	...	N	N	FTV	...	2	1/8	1	1/8	A	...
AU8, AU8B, AU8R, AD8, AW8,	1950-60	.019	Ch or AL	14	.025	.014	.014	45	45	...	...	...	...	N	N	FTV	...	2	1/8	1	1/8	A	...
AU8S, AU8SR, AD8S, AW8S	1950-60	.019	Ch or AL	14	.025	.014	.014	45	45	...	...	...	...	N	N	FTV	...	2	1/8	1	1/8	A	...
CUSHMAN																							
M6, M7, M8, M9	1950-60	.020	Ch-F14V	18	.025	.015	.015	45	45	20	60	63	17	...	N	GSD	1 1/2	2	1/8	1	1/8	A	In
GLADDEN																							
40, 50, 75	1950-60	.020	Ch-J9	14	.025	.008	.010	45	45	15	45	45	15	N	N	FF	...	2	1/8	1	1/8	A	In
GRAVELY																							
L	1950-60	.015	AL-TT10	3/8	.033	.008	.012	45	45	TC	60	60	20	ICG	...	GT	1 1/2	2	1/8	1	1/8	B	In
HOMELITE																							
15	1950-60	.015	Ch	14	.025	...	...	...	...	...	...	...	...	...	...	...	...	3	1/8	...	...	...	BN
8, 20	1950-60	.020	Ch	14	.025	...	...	...	...	...	...	...	...	...	...	...	...	3	1/8	...	...	...	BN
35	1950-60	.020	Ch	14	.025	...	...	...	...	...	...	...	...	...	N	...	...	3	1/8	...	...	...	BN
23	1950-60	.020	Ch	14	.025	...	...	...	...	...	...	...	...	...	N	...	...	3	1/8	...	...	...	BN
24	1950-60	.020	Ch	14	.025	...	...	...	...	...	...	...	...	...	N	...	...	3	1/8	...	...	...	BN
4-20	1950-60	.015	Ch-HO8A	14	.025	...	...	...	...	...	...	...	...	...	Br	TP	...	2	1/8	...	...	...	RB
7-19, 7-21	1950-60	.015	Ch-HO8A	14	.025	...	...	...	...	...	...	...	...	...	T-B	TP	...	2	1/8	...	...	...	RB
5-30N, 8-20	1950-60	.020	Ch-HO3	14	.025	...	...	...	...	...	...	...	...	...	T-B	TP	...	3	1/8	...	...	...	RB
36	1950-60	.020	Ch	14	.025	...	...	...	...	...	...	...	...	...	Til	...	...	2	1/8	...	...	...	BN
37	1950-60	.020	Ch	14	.025	...	...	...	...	...	...	...	...	...	Til	...	...	2	1/8	...	...	...	BN
Zip	1950-60	.015	Ch-J6	14	.025	...	...	...	...	...	...	...	...	...	T-B	TP	...	2	1/8	...	...	...	RB
JACOBSEN																							
J100	1950-60	.020	Ch-UJ12	14	.025	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J125	1950-60	.020	Ch-UJ12	14	.025	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J175	1950-60	.020	Ch-UJ12	14	.025	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J175V	1950-60	.020	Ch-J8J	14	.025	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J225	1950-60	.020	Ch-UJ12	14	.025	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J125V	1960	.020	Ch-UJ12	14	.035	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J125V	1960	.020	Ch-UJ12	14	.035	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J175V	1960	.020	Ch-J8J	14	.035	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
J225V	1960	.020	Ch-J8J	14	.035	...	...	...	...	...	...	...	...	...	...	TVC	...	3	1/8	N	...	A	FB
KOHLER																							
K91	1950-60	.020	Ch or AC	14	.025	.006	.012	44	44	16	36	38	14	Fly	N	GT	1 1/2	2	1/8	1	1/8	A	C
K161	1950-60	.020	Ch or AC	14	.025	.006	.017	44	44	16	36	40	12	Fly	SC	TP	1 1/2	2	1/8	1	1/8	A	C
L160	1950-60	.020	Ch or AC	14	.025	.008	.012	44	44	16	36	40	12	Fly	SC	FP	1 1/2	2	1/8	1	1/8	A	C
K241	1950-60	.020	Ch-H10	14	.025	.010	.020	44	44	20	52	40	12	Fly	BI	GT	1 1/2	2	1/8	1	1/8	A	C
K331	1950-60	.020	Ch or AC	14	.025	.008	.020	44	44	20	52	52	20	Fly	SC	TP	1 1/2	2	1/8	1	1/8	A	In
K662	1950-60	.015	Ch or AC	14	.025	.008	.015	44	44	20	52	52	20	Fly	SC	FP	1 1/2	2	1/8	1	1/8	A	In
KV161	1960	.020	Ch or AC	14	.025	.008	.017	44	44	16	36	40	12	Fly	SC	GT	.160	2	1/8	1	1/8	A	A

Small Gasoline Engine Service Data

MAKE AND MODEL	IGNITION SYSTEM				VALVE SYSTEM								FUEL SYSTEM		PISTON RINGS			Pistons Removed From Connecting Rod Bearing Type				
	Breaker Point Gap (in.)	Spark Plug	Make and Model	Size (MM/m.)	Tapset Clearance		Seat Angle		Valve Timing				Fuel Pump Make	Fuel Screen Location	Carburetor Float Level Height (in.)	Compression			Oil			
					Intake (in.)	Exhaust (in.)	Intake (Deg.)	Exhaust (Deg.)	In- take		Ex- haust					Number	Size (in.)		Number	Size (in.)		
									Opens (Deg. B.T.C.)	Closes (Deg. A.B.C.)	Opens (Deg. B.B.C.)	Closes (Deg. A.T.C.)										
LAUSON																						
H20, HR20, H25, HB25, HR25, H30, HB30, HR30, V25, V27, 1959-60	.020	Ch-J8	14	.030	.010	.010	45	45	16	48	48	10	CG	N	GT	.100	2	3/8	1	3/8	A	C
H35, HB35, HR35, H45, HB45, HR45, H55, HB55, HR55, V22, V30, V35, VC85, VX35, V40, VC40, VX40, V45, VC45, VX45, V55, VC55, VX55, 1959	.020	Ch-J8	14	.030	.010	.010	45	45	16	48	48	10	CG	N	GT	.100	2	3/8	1	3/8	A	C
LV22, H22, HB22, HR22, LV25, H25, HB25, HR25, LAV30, LV30, H30, HB30, HR30, LV35, H35, HB35, HR35, 1960	.020	Ch-J8	14	.030	.010	.010	45	48					CG		GT	3/8	2	3/8	1	3/8	A	C
VX45, V45, VC45, H45, HB45, HR45, V55, VX55, VC55, H55, HB55, HR55, 1960	.020	Ch-H10	14	.030	.010	.010	45	45					CG		GT	3/8	2	3/8	1	3/8	A	C
MC CULLOCH																						
MAC D30, D36, Super 44A																						
Super 55A, 1959	.018	Ch-J8J	14	.025										Own	TL		2	3/8	1	3/8	AB	
MAC 35, 1959	.018	Ch-J8	14	.025					56		78			Fly	GT		2	3/8	1	3/8	B	NR
MAC 35A, 1959-60	.018	Ch-J8J	14	.025					56		78			Fly	GT		2	3/8	1	3/8	B	NR
39, 1959	.018	Ch-J8J	14	.025					56		78			Fly	GT		2	3/8	1	3/8	B	NR
D44, 1959	.018	Ch-J8	14	.025					50		79			Fly	Til	FP	2	3/8	1	3/8	AB	NR
49, 77, 1959	.018	Ch-J8J	14	.025					50		79			Fly	Own	PC	2	3/8	1	3/8	AB	NR
Super 44, Super 55, 1959	.018	Ch-J8J	14	.025					50		79			Fly	Til	FP	2	3/8	1	3/8	AB	NR
99, 1959	.018	Ch-J8J	14	.025										N	Car		2	3/8	1	3/8	AB	NR
140, 150, 160, 170, 180, 1960	.018	Ch-J8J	14	.025													2	3/8	1	3/8	NR	NR
MC10, 1960	.018	Ch-J8J	14	.027													2	3/8	1	3/8	NR	NR
99, 1960	.018	Ch-J8J	14	.025													3	3/8	1	3/8	NR	NR
MUSTANG																						
Standard, 1959-60	.020	Ch-J9	14	.025	.008	.010	30	45	15	45	45	15	N	N	N	FF	2	3/8	1	3/8	A	In
Special, 1959-60	.020	Ch-J9	14	.025	.008	.010	30	45	26	60	60	26	N	N	N	FF	2	3/8	1	3/8	A	In
Special 8, 1960	.020	Ch-J7	14	.025	.008	.010	30	45	40	60	60	40	N	N	N	FF	2	3/8	1	3/8	A	In
ONAN																						
AK, 1959-60	.020	Ch-H8	14	.025	.014	.014	45	45	10	58	58	22	TGC	Own	FF	FF	2	3/8	1	3/8	A	A
AJ, 1959-60	.022	Ch-H8	14	.025	.011	.011	45	45	10	58	48	22	TGC	Own	FF	FF	2	3/8	1	3/8	A	C
LK, 1959-60	.020	Ch-H8	14	.025	.011	.011	45	45	10	48	48	10	TGC	Own	FF	FF	2	3/8	1	3/8	A	C
ACK, 1959-60	.020	Ch-H8	14	.035	.007	.007	44	44	22	58	46	10		Own	GT	GT	2	3/8	1	3/8	A	In
VB, 1959-60	.015	Ch-H8	14	.030	.008H	.008H	45	45	15	63	61	17	Fly	Own	GT	GT	2	3/8	1	3/8	A	In
CK, 1959-60	.020	Ch-H8	14	.025	.015	.015	45	45	10	48	48	10	CGC	Own	FF	FF	2	3/8	1	3/8	A	C
CW, 1959-60	.020	Ch-K15J	14	.025	.012	.012	45	45	10	58	58	10	Fly	Own	FF	FF	2	3/8	1	3/8	A	C
MAJ, 1960	.022	Ch-H8	14	.025	.011	.011	45	45	10	58	48	22	TGC	Own	FF	FF	2	3/8	1	3/8	A	C
MTK, 1960	.020	Ch-K15J	18	.025	.009	.011	45	45	10	48	48	10	Fly	AC	FP	FP	3	3/8	1	3/8	B	C
W35, 1960	.020	Ch-K15J	18	.025	.009	.011	45	45	10	48	48	10	Fly	AC	FP	FP	3	3/8	1	3/8	B	C
POWER PRODUCTS																						
AH47, 1959	.020	Ch-J8J	14	.025									CS		GT		2	3/8	1	3/8	B	NB
AH81, 1959	.020	Ch-J8J	14	.025											GT		2	3/8	1	3/8	B	NB
V47, 1959	.020	Ch-J8J	14	.025									CS		GT		2	3/8	1	3/8	B	NB
AH47, 1960	.015	Ch-J8J	14	.030											Car		2	3/8	1	3/8	B	In
WEST BEND																						
300 Series, 1959-60	.020	Ch-H12J	14	.040										N	GT		2	3/8	1	3/8	B	B
510 Series, 845 Series, 1959	.020	Ch-H12J	14	.040										Til	TCr		2	3/8	1	3/8	B	B
700 Series, 1959-60	.020	Ch-H8J	14	.030										Til	TCr		2	3/8	1	3/8	B	B
580 Series, 1960	.020	Ch-H8J	14	.020											Car		2	3/8	1	3/8	B	NB
WISCONSIN																						
ACN, 1959-60	.015	Ch or AC	18	.030	.008	.014	45	45	5	80	45	10	CGM		ST		2	3/8	1	3/8	A	C
BKN, 1959-60	.015	Ch or AC	18	.030	.008	.014	45	45	5	80	45	10	CGM		ST		2	3/8	1	3/8	A	C
AGNL, 1959-60	.015	Ch or AC	18	.030	.008	.016	45	45	20	60	60	20	FFS		ST		2	3/8	1	3/8	A	C
AGN, AGND, 1959-60	.015	Ch or AC	18	.030	.008	.016	45	45	20	60	60	20	FFS		ST		2	3/8	1	3/8	A	C
TH, THD, 1959-60	.015	Ch or AC	18	.030	.008	.016	45	45	25	75	115	35	FFS	B-A	SP		2	3/8	1	3/8	A	C

ABBREVIATIONS

1—With magneto ignition only; with battery ignition, .035.
 1—From top of float to edge of bowl cover, without gasket.
 1—Or Auto-Lite.
 A—Above.
 AB—Above or below.
 AC—A.C. Spark Plug Div.
 AL—Electric Auto-Lite Co.
 B—Below.
 B-A—Blackstone or A.C.
 BI—Blackstone.
 BN—Ball and needle bearings.
 Br—Brown.

C—Cast.
 Car—Carburetor.
 CC—Crankshaft and camgear.
 CCR—Camshaft and crankshaft.
 CG—Cam gear.
 CGC—Camshaft gear cover.
 CGM—Camshaft gear and magneto gear cover.
 Ch—Champion Spark Plug Co.
 CI—Cast or Insert.
 CS—Crankcase and stator plate.
 F—Fixed.
 FB—Forged bronze.
 FF—Fuel filter.

FFS—Face of magneto gear teeth, flywheel and shroud.
 FP—Flywheel.
 FV—Fuel pump.
 FTV—Fuel tank valve.
 GSB—Gas tank and sediment bowl.
 GT—Gasoline tank.
 H—Hat.
 ICG—Idler and cam gears.
 In—Insert.
 N—None.
 NB—Needle bearings.
 NF—No float.
 PC—Fuel pump and carburetor.

PCB—Float parallel to carburetor upper body.
 RB—Roller bearings.
 RN—Roller and needle bearings.
 SC—Stan Test Corp.
 SP—Fuel strainer attached to pump.
 ST—Strainer below fuel tank.
 T-B—Tillotson or Brown.
 TC—Top center.
 TCR—Fuel tank and carburetor.
 TGC—Timing gear cover.
 Til—Tillotson Mfg. Co.
 TL—Fuel tank and line.
 TP—Fuel tank and fuel pump.
 TVC—Tank valve and carburetor float cover.

1959-60 Imported Car Service Data

MAKE AND MODEL	IGNITION SYSTEM							VALVE TIMING				WHEEL ALIGNMENT				
	Spark Plug		Breaker Point Gap (in.)	Cam Angle (Deg.)	Timing (Deg.)	Timing Mark Location	Firing Order	Location of No. 1 Cylinder	Tapet Clearance		Timing Mark Location	Intake Opens (Deg. B.T.C.)	Caster (Deg.)	Camber (Deg.)	Toe-In (in.)	
	Make	Gap (in.)							Intake (in.)	Exhaust (in.)						
A.C.—Britain																
Ace, Aceca	1959	Le	.017	.015	...	12 1/2	B	(1)	...	.020H	.020H	Fly FCC	12 1/4	5	1P to 2P	1/4
Ace, Aceca	1959	K	.019	.015	...	8B	...	(1)	...	.012H	.012H	...	...	5	1P to 2P	1/4
ALFA-ROMEO—Italy																
Giulietta	1959	Mar	...	.015	...	6B	Fly	(2)	F	.016	.022	Fly	22	...	0	1/4
Giulietta Veloce	1959	Mar	...	.015	...	8B	Fly	(2)	F	...	...	...	34	...	0	1/4
2000	1959	Mar	.017	.015	...	10B	Fly	(3)	F	.017	.019	Fly	31 1/4	...	2 1/4 to 4	1/4
Giulietta Berlina & TI	1960	M-L	.025	.015	...	8B	Fly	(2)	F	.019	.021	Fly	25 1/2	...	0	1/4
Giulietta Sprint & Spider	1960	Le	.024	.015	...	8B	Fly	(2)	F	.019	.021	Fly	25 1/2	...	0	1/4
Giulietta Veloce Sprint & Spider	1960	Le	.024	.015	...	8B	Fly	(2)	F	.015	.021	Fly	34	...	0	1/4
Giulietta Sprint Special	1960	Le	.024	.015	...	8B	Fly	(2)	F	.011	.019	Fly	46	...	0	1/4
2000 Berlina	1960	M-L	.018	.015	...	10B	CP	(3)	F	.017	.019	CP	31 1/4	...	1 to 1 1/4	1/4
2000 Spider	1960	Le	.024	.015	...	7B	CP	(3)	F	.017	.021	CP	31 1/4	...	1 to 1 1/4	1/4
ALPINE—France																
A108-850	1960	Ma	...	...	...	7B	...	(2)	...	.020	.030	...	7	...	...	...
A108-904	1960	Ma	...	...	...	7B	...	(2)	...	.020	.030	...	20	...	...	...
ALVIS—Britain																
TD-21	1959	Ch	.025	.014	...	8B	Fly	(1)	F	.009H	.009H	Fly	13	1 1/4	1	1/4
TD-21	1960	Ch	.025	.014	...	8B	Fly	(1)	F	.009H	.012H	Fly	13	1 1/4	1	1/4
ARMSTRONG-SIDDELEY—Britain																
Sapphire 346	1959	Le	.031	.015	...	5B	CP	(1)	F	.016	.014	CC	8	1 1/4	2	1/4
Star Sapphire	1959	Le	.030	.015	...	4B	Fly	(1)	F	.008	.008	CC	12	1 1/4	0 to 1 1/4	1/4
Star Sapphire & Limousine	1960	Le	.028	.015	...	4B	Fly	(1)	F	.008	.008	CC	12	1/4 P to 3/4 P	1/4 to 1 1/4	0 to 1/4
ASTON-MARTIN—Britain																
DB3	1959	K	.022	.012	...	10B	Fly	(1)	F	.010C	.010C	Fly	17 1/4	2 1/4	2 1/4	1/4
DB4	1959	K	.022	.012	...	11B	CD	(1)	F	.011C	.013C	CD	28 1/2	1 to 1 1/4	1	1/4
DB4	1960	K	.022	.015	36	11B	CD	(1)	F	.010C	.011C	CD	28	1 to 1 1/4	1/4 to 1 1/4	1/4
DB4GT	1960	K	.016	.015	36	7B	CD	(1)	F	.010	.011	CD	47 1/2	1 to 1 1/4	1/4 to 1 1/4	1/4
AUSTIN—Britain																
A35, A40	1959	Ch	.024	.014	...	5B	CP	(2)	F	.012	.012	Ca	5	3	1	1/4 to 1 1/4
A55	1959	Ch	.024	.014	...	5B	CP	(2)	F	.012	.012	Ca	5	1 1/4	0 to 1 1/4	1/4
A95, A105	1959	Ch	.024	.014	...	TC	CP	(1)	...	.012	.012	Ca	5	1 1/4	1/4 to 1	0 to 1/4
7	1960	Ch	.025	.015	60	TC	Fly	(2)	...	.012	.012	CC	5	1 1/4	1	1/4
A55	1960	Ch	.025	.015	60	5B	CP	(2)	F	.015	.015	CC	TC	5	1/4 to 1	1/4 to 1 1/4
A99	1960	Ch	.025	.015	35	TC	CP	(1)	F	.012	.012	CC	5	1 1/4	1	1/4
AUSTIN-HEALEY—Britain																
Sprite	1959	Ch	.025	.015	...	5B	...	(2)	...	.012	.012	CC	5	3	1	1/4 to 1 1/4
100-6	1959	Ch	.025	.015	...	6B	FC	(1)	...	.012	.012	CC	5	2	1	1/4 to 1 1/4
3000	1960	Ch	.025	.015	35	5B	CP	(1)	F	.012	.012	CC	5	2	1	1/4 to 1 1/4
AUTO UNION—Germany																
1000, 1000S	1960	BBC	.025	.015	142	20 1/2	B	...	(4)	R	...	...	...	1/4 to 3/4	1 to 2	0 to 1/4
1000Sp	1960	B-C	.025	.015	142	18 1/4	...	(4)	R	...	...	...	...	1/4 to 3/4	1 to 2	0 to 1/4
BENTLEY—Britain																
Continental	1959	Ch	.025	.020	44	2B	Fly	(5)	...	.006	.012	Fly	TC	1 1/4 N to 1P	0	1/4 to 1/4
S	1959	Ch	.025	.020	44	2B	Fly	(5)	...	.016	.012	Fly	TC	1N to 1 1/2 P	0	1/4 to 1/4
Continental, S2	1960	L-C	.026	.020	44	2B	Fly	(6)	FR	...	...	Fly	200	0	1/4 to 1/4	1/4 to 1/4
BERKELEY—Britain																
Twosome	1959	K	...	...	...	...	...	(4)	...	...	...	...	...	...	...	...
Q95, Q105	1960	K	.018	.015	...	...	CC	(7)	...	...	...	...	306	...	2 1/4	...
Q95, Q105	1960	K	.022	.015	...	...	CC	(7)	...	...	...	...	306	...	2 1/4	...
BIANCHINA—Italy																
110B	1960	Mar	.022	.022	...	10B	FP	...	...	.018	.015	TG	90	1	9	0 to 1/4
110B Special	1960	Mar	.022	.020	...	10B	FP	...	...	.015	.015	TG	258	1	9	0 to 1/4
BMW—Germany																
600	1959	Bo	.022	...	...	9B	...	(7)	...	.006	.006	Fa	22	16	1 1/4	1/4 to 1/4
2.0, 2.0 Luxus, 3.2	1959	Bo	.025	...	...	8B	VD	(6)	FR	.010	.010	VD	2	1	1	1/4
3.2 Super	1959	Bo	.025	...	...	8B	VD	(6)	FR	.010	.010	VD	10	1	1	1/4
503	1959	Bo	.025	...	...	8B	VD	(6)	FR	.010	.010	VD	10	1	1	1/4
507	1959	Bo	.025	...	...	8B	VD	(6)	FR	.010	.010	VD	19 1/2	3 1/4	1	1/4
600	1960	Bo	.028	.018	...	160	90	...	(7)	...	.006	.006	Fa	22	16	1 1/4
2.0, 2.0 Luxus, 3.2	1960	Bo	.035	.016	30	8B	VD	(6)	FR	.010	.010	VD	2	1	1	1/4
3.2 Super	1960	Bo	.035	.016	30	8B	VD	(6)	FR	.010	.010	VD	10	1	1	1/4
503	1960	Bo	.035	.016	30	8B	VD	(6)	FR	.010	.010	VD	10	1	1	1/4
507	1960	Bo	.035	.016	30	8B	VD	(6)	FR	.010	.010	VD	19 1/2	3 1/4	1	1/4
700	1960	Bo	.028	.016	160	100	Fa	...	...	.006	.006	Fa	35	14	1/4	1/4 to 1/4
BORGWARD—Germany																
Isabella	1959	Bo	.029	.016	90	TC	Fly	(2)	R	.008	.008	TG	18	2 1/4 to 3 1/4	0 to 1	0
Isabella TS	1959	Bo	.029	.016	90	4A	Fly	(2)	R	.008	.008	TG	18	2 1/4 to 3 1/4	0 to 1	0
Combi, Isabella	1960	B-B	.025	.016	...	7C	Fly	(2)	R	.006	.006	TG	18	2 1/4 to 3 1/4	0 to 1	0
Coupe, Isabella TS	1960	B-B	.025	.016	...	4A	Fly	(2)	R	.008	.008	TG	18	2 1/4 to 3 1/4	0 to 1	0
CITROEN—Britain																
2CV	1959	Ch	.027	.015	...	120	Fly	(7)	F	.008H	.008H	Cap	3	1/4	1 1/4	1/4 to 1/4
1D18P	1959	Ch	.028	.011	...	160	Fly	(2)	F	.008C	.010C	CC	3	1 1/4	0 to 1 1/4	1/4 to 1/4
DS19	1959	Ch	.026	.011	...	8B	Fly	(2)	F	.008C	.010C	CC	3	1 1/4	0 to 1 1/4	1/4 to 1/4
1D19	1960	Ch	.026	.012	...	10	Fly	(2)	F	.008C	.010C	CC	3	1 1/4	0 to 1 1/4	1/4 to 1/4
DS19	1960	Ch	.026	.012	...	8	Fly	(2)	F	.008C	.010C	CC	3	1 1/4	0 to 1 1/4	1/4 to 1/4
CITROEN—France																
2CV	1959	Ma	.020	...	...	...	...	...	...	...	...	...	...	1 to 1 1/4	0 to 1 1/4	1/4 to 1/4
1D18P	1959-60	M-A	.021	.016	...	120	...	(2)	F	.008C	.010C	...	1 1/4	1 to 1 1/4	0 to 1 1/4	1/4 to 1/4
DS19	1959-60	M-A	.021	.016	...	100	...	(2)	F	.008C	.010C	...	1 1/4	1 to 1 1/4	0 to 1 1/4	1/4 to 1/4
2CV	1960	Ma	.021	.016	...	120	...	...	...	.008H	.008H	...	1 1/4	1 to 1 1/4	0 to 1 1/4	1/4 to 1/4
1D18F	1960	M-A	.021	.016	...	120	...	(2)	F	.008C	.010C	...	1 1/4	1 to 1 1/4	0 to 1 1/4	1/4 to 1/4

1959-60 Imported Car Service Data

MAKE AND MODEL	IGNITION SYSTEM								VALVE TIMING			WHEEL ALIGNMENT			
	Spark Plug	Gap (in.)	Breaker Point Gap (in.)	Cam Angle (Deg.)	Timing (Deg.)	Timing Mark Location	Firing Order	Location of No. 1 Cylinder	Tappet Clearance		Timing Mark Location	Intake Opens (Deg. B.T.C.)	Caster (Deg.)	Camber (Deg.)	Toe-In (in.)
									Intake (in.)	Exhaust (in.)					
DAF—Holland															
600.....1959	Bo	.028	.015	...	5B	Fly	(7)	NI	.004C	.004C	CC	14A	0	1	1/4
600.....1960	Bo	.028	.015	...	5B	CP	(7)	NI	.004C	.004C	CC	14A	1	1	1/4
DAIMLER—Britain															
Majestic.....1959	Le	.025	.014	60	10B	VD	(1)	F	.013	.013	TCC	13	3	1 1/4	1/4
DK400.....1959	Le	.025	.014	60	7D	Fly	(1)	F	.015	.015	TCC	13	2	1 1/4	1/4
SP280.....1960	Ch	.025	.016	48	10B	VD	...	F	.012	.012	CC	13	2	2	1/4
DENZEL—Germany															
1300.....1960	Bo	.027	.014	...	10B	FP	(8)	...	.008C	.008C	...	15	1/2 to 3/4	2 1/4	0 to 1/2
DEUTSCH-BONNET—France															
Le Mans, HBR-5.....1960	Ma	.026	.012	60	44B	...	(7)	F	.010	.010	No	32	1	1	1/4 to 1/2
DKW—Germany															
Junior.....1960	B-B	.020	.010	142	10 1/4	...	(4)	R	...	...	...	...	3/4	3/4	0 to 1/2
FACEL VEGA—France															
Facelia.....1960	...	...	...	...	...	...	(2)	...	...	...	...	...	...	...	...
FAIRTHORPE—Britain															
Atometa.....1959	Ch	.020	.015	...	6B	TCC	(7)	F	.010	.010	...	30	3/4 P	2	1/4
Electron Minor.....1959	Ch	.025	.015	...	5B	CP	(2)	F	.010	.010	CC	10	3/4 P	2	1/4
Electron.....1959	Ch	.019	.015	...	5B	CP	(2)	F	.006	.006	Fly	20	3/4 P	2	1/4
Atom Major.....1960	Ch	.025	.015	...	6B	CP	(2)	F	.010	.010	CP	10	3/4 P	2	1/4
Zeta.....1960	Ch	.025	.015	...	...	...	...	F	...	...	...	...	...	2	1/4
FERRARI—Italy															
250 Gran Turismo.....1959	Ch	...	.027	...	26B	Fly	(9)	FR	.022	.023	Fly	24	8	1	1/4
410 Superamerica.....1959	Ma	...	.027	...	26B	Fly	(9)	FR	.022	.023	Fly	24	8	1	1/4
FIAT—Italy															
500.....1959	Ch	.029	.018	...	10B	CT	(7)	...	.006	.006	CC	9	8 to 10	1 1/4 to 1 1/2	0 to 1/4
500.....1960	Ch	.029	.018	...	10B	CT	(2)	...	.006	.006	CC	4	8 to 10	1 1/4 to 1 1/2	0 to 1/4
500 Multipla.....1959	Mar	.021	.018	...	10B	CT	(2)	...	.004	.004	CC	10	8 1/2 to 9 1/2	1 1/4 to 1 1/2	0 to 1/4
1100/103D.....1959	Ch	.029	.018	...	7C	CT	(2)	...	.004	.004	CC	16	1 1/4	1 1/4	0 to 1/4
1200, 1200 Granluxe.....1959	Ch	.029	.018	...	3A	CT	(2)	...	.004	.004	CC	16	1 1/4	1 1/4	0 to 1/4
1400B.....1959	Mar	.024	.018	...	7C	CT	(2)	...	.004	.004	CC	15	2 to 2 1/2	1 1/4 to 1 1/2	0 to 1/4
1900B.....1959	Mar	.024	.018	...	7C	CT	(2)	...	.006	.006	CC	10	2 to 2 1/2	1 1/4 to 1 1/2	0 to 1/4
500.....1960	Ch	.029	.020	78	10B	CT	(7)	...	.015	.015	CC	9	9	1	0 to 1/4
500 Sport.....1960	Ch	.026	.026	78	10B	CT	(7)	...	.015	.015	CC	25	9	1	0 to 1/4
500 Sedan.....1960	Ch	.026	.018	50	10B	CT	(2)	...	.006	.006	CC	4	9	1	0 to 1/4
500 Multipla.....1960	Ch	.026	.018	50	10B	CT	(2)	...	.006	.006	CC	4	9	1	0 to 1/4
1100 Family Car, Standard & Deluxe.....1960	Ch	.026	.018	60	7C	CT	(2)	F	.004	.004	CC	16	2 1/4	2 1/4	0 to 1/4
1200 Granluxe & Cabriolet.....1960	Ch	.026	.018	50	3A	CT	(2)	F	.004	.004	CC	16	2 1/4	2 1/4	0 to 1/4
1900 Sedan & Station Wagon.....1960	Ch	.026	.018	40	10B	CT	(1)	F	.006	.006	CC	20	1 1/2	1	0 to 1/4
1900 Cabriolet.....1960	Ch	.022	.018	50	7C	Ch	(2)	F	.012	.014	Ofc	20	1 1/2	1	0 to 1/4
2100 Sedan.....1960	Ch	.026	.016	40	5B	CT	(1)	F	.006	.006	CC	20	1 1/2	1	0 to 1/4
FMR—Germany															
KR200, KR201.....1960	Bo	...	...	...	30B	...	...	...	...	...	...	...	8	0 to 1	1/4
TC900.....1960	Bo	...	...	...	TC	...	...	...	...	...	...	...	7	0 to 1	1/4
FORD—Britain															
Popular.....1959	Ch	.021	.015	60	4B	TCC	(3)	F	.013C	.016C	CC	9 1/2	4 1/2	1 1/4	1/4 to 1/2
Excort, Squire, Anglia, Prefect.....1959	Ch	.025	.015	60	5B	CP	(2)	F	.013C	.013C	CC	3 1/2	1 to 2 1/2	1 1/4 to 2 1/4	1/4 to 1/2
Consul Mk. II.....1959-60	Ch	.032	.015	55	6B	CP	(3)	F	.014C	.014C	CC	17	1 1/2 to 2 1/2	1 1/4 to 2 1/4	1/4 to 1/2
Zephyr, Zodiac.....1959-60	Ch	.032	.015	32	6B	CP	(1)	F	.014C	.014C	CC	17	1 1/2 to 2 1/2	1 1/4 to 2 1/4	1/4 to 1/2
Excort.....1960	Ch	.025	.015	60	5B	CP	(3)	F	.013C	.013C	CC	3 1/2	1 to 2 1/2	1 1/4 to 2 1/4	1/4 to 1/2
Prefect.....1960	Ch	.030	.015	55	10B	CP	(3)	F	.008C	.018C	CC	10	1 to 2 1/2	1 1/4 to 2 1/4	1/4 to 1/2
Anglia.....1960	Ch	.030	.015	55	10B	CP	(3)	F	.008C	.018C	CC	10	3	1	1/4 to 1/2
FORD—Germany															
Taurus 12M.....1959	Ch	.029	.018	48	11B	CP	(3)	F	.015	.015	TQ	5	3/4 to 2 1/4	0 to 1	1/4 to 1/2
Taurus 12M.....1959	Ch	.030	.018	49	22B	CP	(3)	F	.011	.014	TQ	26	5	0 to 1	1/4 to 1/2
Taurus 12M.....1960	Ch	.030	.018	49	8B	CP	(3)	F	.015	.015	TQ	5	3/4 to 2 1/4	0 to 1	1/4 to 1/2
Taurus 12M Special.....1960	B-C	.030	.018	49	11B	CP	(3)	F	.012	.014	TQ	26	3/4 to 2 1/4	0 to 1	1/4 to 1/2
Taurus 17M.....1960	Ch	.030	.018	48	22B	CP	(3)	F	.011	.014	TQ	26	0 to 3/4	3/4 to 1 1/4	1/4 to 1/2
FRAZER-NASH—Britain															
Continental.....1960	Bo	.035	.016	...	...	CP	(6)	FR	.010H	.010H	CC	10 1/2	3 1/2	1N to 1P	0
FRISKY—Britain															
Convertible, Coupe.....1959	Le	.022	.020	...	...	Fly	(7)	...	...	...	Fly	...	...	...	1/4
Sprint.....1959	K	.022	.022	...	...	...	(7)	...	...	...	...	...	9	...	1/4
GOGGOMOBIL—Germany															
T3-400.....1959	Ch	.024	.014	...	...	...	...	...	...	...	...	...	9	5	1/4 to 1/2
T-700.....1960	...	.025	.010	...	5B	Fly	...	...	.001C	.001C	Fly	35	4	3	1/4 to 1/2
GOLIATH—Germany															
1100, 1100B Sedan.....1959	Bo	.028	.018	45	4B	Fly	(8)	F	.008H	.008H	...	18	0	1 1/4	0 to 1/4
1100, 1100B Tiger & Empress.....1959	Bo	.028	.018	45	6B	Fly	(8)	F	.008H	.010H	...	18	0	1 1/4	0 to 1/4
1100 (60 HP).....1960	B-C	.027	.018	...	4B	...	(8)	...	...	...	...	...	0	1 1/4	0 to 1/4
1100 (35 HP).....1960	B-C	.027	.018	...	6B	...	(8)	...	...	...	...	...	0	1 1/4	0 to 1/4
HEINKEL—Ireland															
T-154.....1959	L-C	.022	.016	...	9B	Fly	...	...	.008C	.008C	Cap	10 1/2	1 1/4	2	1/4 to 1/2
T-154.....1960	L-C	.022	.016	...	1B 1/2 A	Fly	...	...	.008C	.008C	...	...	1 1/4	2	1/4 to 1/2
HILLMAN—Britain															
Hunkey Ser. I.....1959	Ch	.025	.015	9	6B	CP	(2)	F	.012	.014	CC	10	3	3/4	1/4 to 1/2
Minx Ser. III.....1959	Ch	.025	.015	9	10B	CP	(2)	F	.012	.014	CC	10	1 1/4	3/4	1/4 to 1/2
Minx Ser. IIIA.....1960	Ch	.025	.015	9	7B	CP	(2)	F	.012	.014	CC	10	1 1/4	3/4	1/4 to 1/2

1959-60 Imported Car Service Data

MAKE AND MODEL	IGNITION SYSTEM								VALVE TIMING				WHEEL ALIGNMENT			
	Spark Plug	Gap (In.)	Breaker Point Gap (In.)	Cam Angle (Deg.)	Timing (Deg.)	Timing Mark Location	Firing Order	Location No. 1 Cylinder	Tappet Clearance		Intake Opens (Deg. B.T.C.)	Caster (Deg.)	Camber (Deg.)	Toe-In (In.)		
									Intake (In.)	Exhaust (In.)						
HOLDEN—Australia FC.....	1959-60	AC	.031	.014	38	2B	Fly	(1)	F	.008	.012	CC	6	1 to 2	0 to 2	1/4 to 1/2
HUMBER—Britain HAWK.....	1959	Ch	.025	.015	9	2A	CP	(2)	F	.007	.009	CC	15	0	3/4	1/4
Super Snipe.....	1959-60	Ch	.025	.015	...	6B	CP	(1)	F	.014	.014	CC	20	1/2 N	1/4	1/4
Hawk.....	1960	Ch	.025	.015	...	1A	CP	(2)	...	.007	.009	CP	15	1/2	3/4	1/4
ISETTA—Germany 300.....	1959	Be	.020	...	...	7 1/8 B	Fa	...	...	.006	.008	Fa	6	12	1 1/4	1/4
300.....	1960	Be	.024	.016	160	7 1/8 B	Fa	...	...	.006	.008	Fa	6	12	1 1/2	1/4
JAGUAR—Britain 2.4 Litre.....	1959	Ch	.030	.015	35	6B	Fly	(1)	R	.004C	.006C	Fly	10	1 1/2 N to 1N	1 1/2 P to 1P	0 to 1/4
XK150.....	1959	Ch	.025	.015	35	6B	VD	(1)	R	.004C	.006C	VD	15	1 1/2 P to 2P	1 1/2 P to 1P	0 to 1/4
3.4 Litre.....	1959	Ch	.025	.015	35	2B	Fly	(1)	R	.004C	.006C	Fly	15	1 1/2 N to 1N	1 1/2 P to 1P	0 to 1/4
Mark VIII.....	1959	Ch	.022	.015	35	6B	VD	(1)	R	.004C	.006C	VD	15	1 1/2 P to 1P	1 1/2 P to 1P	1/4 to 1/2
XK150.....	1959	Ch	.025	.015	35	9B	VD	(1)	R	.004C	.006C	VD	15	1 1/2 P to 2P	1 1/2 P to 1P	0 to 1/4
Mark IX.....	1959-60	Ch	.025	.015	35	4B	VD	(1)	R	.004C	.006C	VD	15	1 1/2 N to 1 1/2 P	1 1/2 P to 1 1/2 P	1/4 to 1/2
Mark 2 Saloon 2.4 Litre.....	1960	Ch	.025	.015	35	8B	Fly	(1)	R	.004C	.006C	Fly	10	0	1 1/2 P to 1P	0 to 1/4
Mark 2 Saloon 3.4 Litre.....	1960	Ch	.025	.015	35	2B	Fly	(1)	R	.004C	.006C	Fly	15	0	1 1/2 P to 1P	0 to 1/4
Mark 2 Saloon 3.8 Litre.....	1960	Ch	.025	.015	35	4B	VD	(1)	R	.004C	.006C	VD	15	0	1 1/2 P to 1P	0 to 1/4
Mark IX Saloon 3.8 Litre.....	1960	Ch	.025	.015	35	4B	VD	(1)	R	.004C	.006C	VD	15	1 1/2 N to 1 1/2 P	1 1/2 P to 1 1/2 P	1/4 to 1/2
XK150 Roadster 3.4 Litre.....	1960	Ch	.025	.015	35	6B	Fly	(1)	R	.004C	.006C	Fly	15	1 1/2 P	1 1/2 P to 1P	0 to 1/4
XK150 Roadster 3.8 Litre.....	1960	Ch	.025	.015	35	9B	VD	(1)	R	.004C	.006C	VD	15	1 1/2 P	1 1/2 P to 1P	0 to 1/4
JENSEN—Britain 541R & Deluxe.....	1959-60	Ch	.035	.012	...	...	Fly	(1)	F	.012	.012	Fly	10	1 1/4	1/2	1/4
LANCIA—Italy Flaminia Saloon.....	1959	Ch	.022	.018	...	11B	Fly	(10)	FL	.006	.010	Fly	11	1	2	1/4 to 1/2
Flaminia Sport, G.T. & Coupe.....	1959	Ch	.022	.018	...	11B	Fly	(10)	FL	.006	.010	Fly	13	1	2	1/4 to 1/2
Apia.....	1960	C-A	.025	.018	...	15D	Fly	(2)	FR	.006	.008	...	15	0	1	1/4 to 1/2
Flaminia Saloon.....	1960	Ch	.022	.018	...	11B	Fly	(10)	FL	.006	.010	Fly	11	1	2	1/4 to 1/2
Flaminia Coupe.....	1960	Ch	.022	.018	...	11B	Fly	(10)	FL	.006	.010	Fly	13	1	2	1/4 to 1/2
Flaminia G.T. & Sport.....	1960	Ch	.022	.018	...	11B	Fly	(10)	FL	.006	.010	Fly	13	3	2	1/4 to 1/2
LLOYD—Germany Alexander.....	1959-60	Ch	.029	.016	...	3B	Fly	(7)	...	.006	.006	CS	35	0	2	0 to 1/4
Alexander TS.....	1959-60	Ch	.029	.016	...	7C	Fly	(7)	...	.006	.006	CS	32	0	2	0 to 1/4
Arabella.....	1960	Ch	.029	.018	...	5B	CP	(6)	...	.006	.006	CC	37	0	2	1/4 to 1/2
LOTUS—Britain Seven.....	1960	...	.025	.014	...	...	TG	(11)	F	...	...	...	5	5	1P	1/4
Elite (85 HP).....	1960	Ch	.018	.015	...	2 1/8 B	Fly	(2)	...	.006	.006	CC	30	7	1 1/4 to 1 1/2	1/4 to 1/2
Elite (75 HP).....	1960	Ch	.018	.015	...	2 1/8 B	Fly	(2)	...	.006	.006	CC	12	7	1 1/4 to 1 1/2	1/4 to 1/2
MAICO—Germany 500.....	1959	Be	.025	.016	...	30B	...	(7)	F	...	...	...	...	3	1 1/2	0 to 1 1/2
MERCEDES-BENZ—Germany 180, 190.....	1959	Be	.037	.018	50	8B	VD	(2)	F	.004	.006	Cam	12	3 to 4	0 to 1P	0 to 1/4
180D.....	1959	...	...	...	...	...	VD	(2)	F	.007	.007	TG	6 1/2	3 to 4	0 to 1P	0 to 1/4
190D.....	1959	...	...	...	...	...	VD	(2)	F	.006	.011	Cam	11	3 to 4	0 to 1P	0 to 1/4
190SL.....	1959	Be	.029	.018	50	9B	VD	(2)	F	.004	.006	Cam	17 1/2	3 to 4	0 to 1P	0 to 1/4
219.....	1959	Be	.029	.014	36	1A	VD	(1)	F	.005	.008	Cam	10	3 to 4	0 to 1P	0 to 1/4
220S.....	1959	Be	.029	.014	36	2B	VD	(1)	F	.005	.008	Cam	10	3 to 4	0 to 1P	0 to 1/4
220SE.....	1959	Be	.023	.014	36	2B	VD	(1)	F	.005	.008	Cam	29	3 to 4	0 to 1P	0 to 1/4
300 Automatic.....	1959	Be	.023	.014	36	...	CV	(1)	F	.004	.006	Cam	9	2 to 3	0 to 1P	0 to 1/4
300SL.....	1959	Ch	.022	.013	...	...	CV	(1)	F	.004	.006	Cam	20	3 1/4 to 4 1/4	0 to 1P	0 to 1/4
180, 190.....	1960	Be	.037	.018	...	4B	VD	(2)	F	.004	.006	Cam	12	3 1/2	0 to 1/4	0 to 1/4
180D.....	1960	...	...	...	...	...	VD	(2)	F	.006	.006	TG	6 1/2	3 1/2	0 to 1/4	0 to 1/4
190D.....	1960	...	...	...	...	...	VD	(2)	F	.005	.012	Cam	11	3 1/2	0 to 1/4	0 to 1/4
190SL.....	1960	Be	.033	.018	...	8B	VD	(2)	F	.004	.006	Cam	17 1/2	3 1/2	0 to 1/4	0 to 1/4
220.....	1960	Be	.029	.014	...	1A	VD	(1)	F	.003	.006	Cam	10	2 1/2	0 to 1/4	0 to 1/4
220S.....	1960	Be	.029	.014	...	2B	VD	(1)	F	.003	.006	Cam	10	2 1/2	0 to 1/4	0 to 1/4
220SE.....	1960	Be	.022	.014	...	...	VD	(1)	F	.003	.006	Cam	10	2 1/2	0 to 1/4	0 to 1/4
300 Automatic.....	1960	Be	.023	.014	...	4A	CV	(1)	F	.003	.006	Cam	...	2 to 3	0 to 1/4	0 to 1/4
300SL.....	1960	Ch	.022	.013	...	...	CV	(1)	F	.003	.006	Cam	...	4 to 5	1/2	1/4 to 1/2
METROPOLITAN—Britain	1959	Ch	.024	.014	...	6B	CP	(2)	...	.012	.012	Ca	5	...	...	...
MG—Britain Magnette.....	1959	Ch	.020	.015	60	4B	CC	(2)	F	.012H	.012H	CP	5	3	1	0
MGA.....	1959	Ch	.020	.015	60	7B	CC	(2)	F	.017H	.017H	CP	16	4	1	0
MGA Twin Cam.....	1959	Ch	.025	.015	60	7C	CP	(2)	F	.017C	.017C	CC	20	4	1	0
Magnette III.....	1960	Ch	.025	.015	60	7C	FP	(2)	...	.015	.015	CC	TC	1 1/2	1/4 to 1	1/4 to 1/2
MGA 1600.....	1960	Ch	.025	.015	60	7C	TCC	(2)	...	.015	.015	CC	5	4	1/2 to 1 1/2	0
MORGAN—Britain Plus 4.....	1959	Ch	.032	.012	...	4B	CC	(2)	...	.012	.012	CC	15	4	2	1/4 to 1/2
4/4 Series II.....	1959-60	Ch	.025	.014	...	4B	CC	(3)	...	.012	.012	CC	3 1/2	4	2	1/4 to 1/2
Plus 4.....	1960	Le	.025	.012	...	4B	CC	(2)	...	.012	.012	CC	15	4	2	1/4 to 1/2
MORRIS—Britain Minor 1000.....	1959	Ch	.025	.015	60	7C	CC	(2)	F	.012C	.012C	CP	5	3	1	1/4
Oxford, Cowley.....	1959	Ch	.025	.015	60	5B	CC	(2)	F	.015H	.015H	CP	5	3	1	1/4
Mini-Minor.....	1960	Ch	.025	.015	60	7C	FP	(2)	...	.012	.012	CC	5	1 1/2	0	1/4
Oxford Ser. V.....	1960	Ch	.025	.015	60	7C	FP	(2)	...	.015	.015	CC	TC	1 1/2	1/4 to 1	1/4 to 1/2
NISSAN—Japan 210-S, L210-S.....	1959	...	.026	.020	53	17B	CP	(2)	...	.014	.014	CC	13	3	1 1/4	1/4
PL-310.....	1960	...	.030	.020	53	20	CP	(2)	F	.015	.015	CC	10	2 1/2	1 1/2	1/4

1959-60 Imported Car Service Data

MAKE AND MODEL	IGNITION SYSTEM								VALVE TIMING				WHEEL ALIGNMENT			
	Spark Plug		Breaker Point Gap (In.)	Cam Angle (Deg.)	Timing (Deg.)	Timing Mark Location	Firing Order	Location of No. 1 Cylinder	Tapet Clearances		Timing Mark Location	Intake Opens (Deg. B.T.C.)	Caster (Deg.)	Camber (Deg.)	Toe-In (In.)	
	Make	Gap (In.)							Intake (In.)	Exhaust (In.)						
NOBLE—Britain	200.....1960	Bo	.026	.018	...	...	...	...	...	...	...	...	...	3 to 4	1/4 to 1/2	
NSU—Germany	Prinz II, 30 & Sport.....1959-60	Bo	.027	.014	...	TC	...	...	.004C	.004C	No	...	8	2	1/4 to 1 1/2	
OPEL—Germany	Olympia.....1959	Bo	.037	.018	...	TC	Fly (2)	F	.008	.012	Fly	27	1 1/2 to 2	1/4 to 1 1/2	1/4 to 1/2	
	Kapitan.....1959	Bo	.037	.014	...	TC	Fly (1)	F	.008	.012	Fly	27	1 to 2	1/4 to 1 1/2	1/4 to 1/2	
	Olympia.....1960	A-B	.037	.014	...	TC	Fly (2)	F	.008	.012	Fly	27	1 to 2	1/4 to 1 1/2	1/4 to 1/2	
	Kapitan.....1960	A-B	.037	.014	...	TC	Fly (1)	F	.008	.012	Fly	30	1 to 2	1/4 to 1 1/2	1/4 to 1/2	
PANHARD—France	PL-17.....1960	Ma	.024	.016	...	SB	Fly (7)	...	.006C	.006C	...	27 1/2	1 1/2	1	1/4 to 1/2	
PEERLESS—Britain	GT2.....1959	Ch	.032	.015	...	4B	...	(2)	F	.010	.010	...	15	4	1 1/2	1/4
	GT2.....1960	Le	.025	.015	...	4B	CP	(2)	F	.010	.010	Fly	15	4	1 1/2	1/4
PEUGEOT—France	403.....1959	AC	.024	.015	57	9 1/2 B	Fly	(2)	R	.004C	.008C	...	TC	1 to 3	1/4 to 1P	1/4
	403.....1960	A-M	.024	.015	57	9 1/2 B	Fly	(2)	R	.004C	.008C	...	TC	2	1	1/4
PORSCHE—Germany	356A-1600.....1959	Bo	.028	.018	...	SB	CP	(2)	F	.013	.013	CP	27	4 1/2 to 5 1/2	1/4 to 1 1/2	1/4 to 1/2
	356A-1600S.....1959	Bo	.028	.018	...	SB	CP	(2)	F	.013	.013	CP	44	4 1/2 to 5 1/2	1/4 to 1 1/2	1/4 to 1/2
	356A-1600CS.....1959	Bo	.016	.014	23 1/2	24B	CC	(2)	F	.006	.006	CP	38	4 1/2 to 5 1/2	1/4 to 1 1/2	1/4 to 1/2
	356B-1600.....1960	BBC	.028	.016	90	SB	CP	(2)	F	.004	.006	...	5	4 1/2 to 5 1/2	1/4 to 1 1/2	1/4 to 1/2
	356B-1600S.....1960	BBC	.028	.016	90	SB	CP	(2)	F	.006	.004	...	15	4 1/2 to 5 1/2	1/4 to 1 1/2	1/4 to 1/2
	356B-1600S-90.....1960	BBC	.028	.016	90	SB	CP	(2)	F	.006	.004	...	15	4 1/2 to 5 1/2	1/4 to 1 1/2	1/4 to 1/2
PRINCE—Japan	ALSI-1.....1959	NTT	.028	.014	61	SB	Fly	(2)	F	.010	.010	Fly	19 1/2	1 1/2	1	0 to 1/4
	ALSIEL (A).....1960	NTT	.028	.014	61	10B	Fly	(2)	F	.010	.010	Fly	19 1/2	1 1/2	1	1/4 to 1/2
	ALSIEL (B).....1960	NTT	.028	.014	61	SB	Fly	(2)	F	.010	.010	Fly	19 1/2	1 1/2	1	1/4 to 1/2
RENAULT—France	R-1062.....1959	AC	.030	.018	...	2B	CP	(2)	F	.004	.008	Cap	6	10	1 1/2	1/4 to 1 1/2
	R-1060.....1959	AC	.023	.018	...	2B	CP	(2)	F	.008	.012	Cap	6	10	1 1/2	1/4 to 1 1/2
	R-1091.....1959	AC	.023	.018	...	7B	CP	(2)	F	.008	.012	Cap	6	10	1 1/2	1/4 to 1 1/2
	R-1103.....1959	AC	.030	.018	...	SB	CP	(2)	R	.005	.007	Cap	6	2 to 4	1/4 to 1 1/2	1/4 to 1/2
	R-1104.....1959	AC	.030	.018	...	SB	CP	(2)	R	.005	.007	Cap	10	2 to 4	1/4 to 1 1/2	1/4 to 1/2
	R-1062.....1960	AC	.024	.018	...	2B	CP	(2)	F	.004	.008	Cap	6	10	1 1/2	1/4 to 1 1/2
	R-1090.....1960	AC	.024	.018	...	TC	CP	(2)	F	.004	.008	Cap	6	10	1N to 1P	1/4 to 1 1/2
	R-1091.....1960	AC	.024	.018	...	3B	CP	(2)	F	.006	.009	Cap	7	10	1N to 1P	1/4 to 1 1/2
	R-1092.....1960	AC	.024	.018	...	3B	CP	(2)	F	.006	.009	Cap	7	10	1N to 1P	1/4 to 1 1/2
	R-1103.....1960	AC	.024	.018	...	SB	CP	(2)	R	.005	.007	Cap	6	2 to 4	1/4 to 1 1/2	1/4 to 1/2
	R-1104.....1960	AC	.024	.018	...	SB	CP	(2)	R	.005	.007	Cap	10	2 to 4	1/4 to 1 1/2	1/4 to 1/2
RILEY—Britain	1.5.....1959	Ch	.025	.015	60	TC	CC	(2)	F	.015H	.015H	CP	5	3	2 to 5	0
	2.6.....1959	Ch	.025	.015	35	4 1/2 B	CC	(1)	F	.012H	.012H	CP	5	1 1/2	1	0 to 1/4
	4-68.....1960	Ch	.025	.015	60	TC	FP	(2)	...	.015	.015	CC	TC	1 1/2	1/4 to 1	1/4 to 1/2
ROLLS-ROYCE—Britain	Silver Cloud.....1959	Ch	.025	.020	44	2B	Fly	(5)	...	.006	.012	Fly	TC	1N to 1 1/2 P	0	1/4 to 1/2
	Silver Wraith.....1959	Ch	.025	.020	44	2B	Fly	(5)	...	.006	.012	Fly	TC	1/2 N to 1 1/2 P	0 to 1P	1/4 to 1/2
	Silver Cloud II, Phantom V.....1960	L-C	.025	.020	44	2B	Fly	(6)	FR	...	...	Fly	20B	0	0	1/4 to 1/2
ROVER—Britain	60.....1959	Le	.031	.015	60	10B	Fly	(2)	F	.006	.012	Fly	9	2	2	0 to 1/4
	75, 90.....1959	Le	.031	.015	35	10B	Fly	(1)	F	.006	.012	Fly	9	2	2	0 to 1/4
	105.....1959	Le	.031	.015	35	3B	Fly	(1)	F	.006	.012	Fly	9	2	2	0 to 1/4
	3 Litre.....1959	Le	.031	.015	35	3B	Fly	(1)	F	.006	.010	Fly	17 1/2	0 to 1	0 to 2	0 to 1/4
	60.....1960	Le	.031	.015	60	6B	Fly	(2)	F	.010	.010	Fly	6	2	2	0 to 1/4
	105.....1960	Le	.031	.015	35	10B	Fly	(1)	F	.006	.010	Fly	17 1/2	2	2	0 to 1/4
	3 Litre.....1960	Ch	.031	.015	35	3B	Fly	(1)	F	.006	.010	Fly	17 1/2	0	2	0 to 1/4
SAAB—Sweden	93B, 93F.....1959-60	Ch	.028	.014	...	SB	...	(4)	R	...	...	...	...	1 1/2 to 2 1/2	1/2 to 1	1/4 to 1/2
	95.....1960	Bo	.029	.014	...	SB	FP	(4)	R	...	...	...	...	2	1/4	1/4 to 1/2
SEAT—Spain	1400.....1959	Fir	.022	.016	30	10B	...	(2)	F	.030	.030	...	15	2 to 2 1/2	1/4 to 1	1/4
	600.....1959-60	Fir	.022	.016	30	10B	CP	(2)	F	.030	.030	CC	10	9	1	1/4
	1400C.....1960	Fir	.022	.016	30	TC	CP	(2)	F	.030	.030	CC	15	2	1/4	1/4
SIMCA—France	Aronde.....1959	Ma	.027	.047	56	4B	CP	(2)	F	.004C	.008C	Tgh	12	1 1/2 to 2 1/2	1 to 1 1/2	1/4
	Aronde.....1959	Ma	.027	.047	56	4B	CP	(2)	FR	.004C	.008C	Tgh	12	1 1/2 to 2 1/2	1 to 1 1/2	1/4
	Vedette.....1959	AC	.023	.015	28	3B	CP	(6)	F	.011	.011	TG	5	1 1/2 to 2 1/2	1P to 1 1/2 P	1/4 to 1/2
	Aronde Etoile & Flash.....1960	Ma	.028	.016	56	4B	...	(2)	F	.004	.006	...	12	1 1/2 to 2 1/2	1 to 1 1/2	1/4 to 1/2
	Aronde Flash Special.....1960	Ma	.028	.016	56	TC	...	(2)	F	.004	.006	...	12	1 1/2 to 2 1/2	1 to 1 1/2	1/4 to 1/2
	Aronde.....1960	Ma	.027	.016	56	4B	...	(2)	F	.004	.006	...	12	1 1/2 to 2 1/2	1 to 1 1/2	1/4 to 1/2
	Vedette.....1960	AC	.023	.015	28	2B	...	(6)	FR	.011	.011	...	5	1 1/2 to 2 1/2	1/4 to 1 1/2	1/4 to 1/2
SINGER—Britain	Gazelle Ser. III.....1959	Ch	.025	.015	9	10B	CP	(2)	F	.012	.014	CC	10	1 1/2 to 3	1/4	1/4
	Gazelle Ser. IIIA.....1960	Ch	.025	.015	...	7B	CP	(2)	F	.012	.014	CC	10	1 1/2	1/4	1/4
SKODA—Czechoslovakia	Octavia.....1959	...	.024	.016	16	23B	T-F	(2)	F	.006	.006	T-F	13 1/2	3 1/4	1	1/4
	Octavia Super.....1959	...	.024	.016	16	23B	T-F	(2)	F	.006	.006	T-F	13 1/2	3 1/4	1	1/4
	Felicia.....1959	...	.024	.016	4	23B	T-F	(2)	F	.006	.006	T-F	29	3 1/4	1	1/4
	Octavia.....1960	...	.024	.016	16	23B	TCC	(2)	F	.006	.006	FCC	13 1/2	3 1/4	1	1/4
	Octavia Super.....1960	...	.024	.016	16	23B	TCC	(2)	F	.006	.006	FCC	13 1/2	3 1/4	1	1/4
	Felicia.....1960	...	.024	.016	4	23B	TCC	(2)	F	.006	.006	FCC	29	3 1/4	1	1/4

1950-60 Imported Car Service Data

MAKE AND MODEL	IGNITION SYSTEM								VALVE TIMING				WHEEL ALIGNMENT			
	Spark Plug		Breaker Point Gap (In.)	Cam Angle (Deg.)	Timing (Deg.)	Timing Mark Location	Firing Order	Location of No. 1 Cylinder	Tappet Clearance		Timing Mark Location	Intake Opens (Deg. B.T.C.)	Caster (Deg.)	Camber (Deg.)	Toe-In (In.)	
	Make	Gap (In.)							Intake (In.)	Exhaust (In.)						
STANDARD—Britain																
8.....1959	Ch	.032	.015	110	88	TCC	(2)	F	.010	.010	TCC	10	1 3/4	2	0 to 1/8	
10, Pennant.....1959-60	Ch	.032	.015	110	108	TCC	(2)	F	.010	.010	TCC	10	1 3/4	2	0 to 1/8	
Ensign.....1959-60	Ch	.032	.015	110	38	TCC	(2)	F	.010	.010	TCC	12	1 1/2	2	0 to 1/8	
Vanguard III.....1959-60	Ch	.032	.015	110	128	TCC	(2)	F	.010	.010	TCC	12	1 1/2	2	0 to 1/8	
STEYR-PUCH—Austria																
2000, 2300 Sport.....1959	Bo	.035	.016	...	4B	...	(2)	---	.008	.008	...	18	2 to 2 1/2	1/4 to 1	1/8 to 1/4	
500.....1960	Bo	.027	.016	...	...	...	(7)	---	.006	.006	...	...	0	1/4 to 1 1/4	0 to 1/8
SUNBEAM—Britain																
Rapier Ser. II.....1959	Ch	.025	.015	90	88	CP	(2)	F	.012	.014	CC	11	1 3/4	3/4	1/8 to 1/4	
Rapier Ser. III.....1960	Ch	.025	.015	...	88	CP	(2)	F	.012	.014	CC	14	1 3/4	3/4	1/8	
Alpine.....1960	Ch	.025	.015	...	88	CP	(2)	F	.012	.014	CC	14	4 3/4	3/4	1/8	
TALBOT—France																
Largo.....1959	Ma	.027	.020	...	...	...	(8)	FR	.028C	.028C	...	...	3	10 to 15	1/8
TATRA—Czechoslovakia																
803.....1959	---	.020	.020	...	108	CP	(12)	FL	.004	.006	FC	4A	0	1	1/8	
803.....1960	---	.027	.008	...	138	CP	(13)	FR	.004	.006	CP	15	0	1 1/2	1/8	
TOYOTA—Japan																
Corona ST10.....1959	---	.027	.018	54	7B	CP	(3)	F	.008	.010	CP	8	1	1	1/8	
Toyopet Crown RS20.....1959-60	---	.032	.018	45	88	Fly	(3)	F	.008	.014	Fly	11	1	1	1/8	
Crown Custom RS22L.....1959-60	---	.032	.018	45	88	Fly	(3)	F	.008	.014	Fly	11	1	1	1/8	
Toyopet Corona PT20.....1960	---	.032	.018	45	108	CP	(2)	F	.008	.014	CP	11	2	1	1/8	
Toyopet Corona RT20.....1960	---	.032	.018	45	88	CP	(3)	F	.008	.014	Fly	11	2	1	1/8	
TRIUMPH—Britain																
Sedan.....1959	Ch	.032	.015	110	108	TCC	(2)	F	.010	.010	TCC	10	1 3/4	2	0 to 1/8	
Sports TR3.....1959	Ch	.025	.015	110	48	TCC	(2)	F	.010	.010	TCC	17	0	1 1/4	1/8	
Sedan & Station Wagon.....1960	Lo	.025	.015	110	108	TCC	(2)	F	.010	.010	TCC	12	2	0 to 1/8		
Herald Sedan.....1960	Lo	.025	.015	110	108	TCC	(2)	F	.010	.010	TCC	12	4	2	0 to 1/8	
Herald Coupe.....1960	Lo	.025	.015	110	128	TCC	(2)	F	.010	.010	TCC	18	4	2	0 to 1/8	
TR3.....1960	Lo	.025	.015	110	48	TCC	(2)	F	.010	.010	TCC	17	0	2	1/8	
UNICAR—Britain																
T.....1959	K	.018	.020	...	...	...	---	---	---	---	---	---	0	1	3/8
VAUXHALL—Britain																
FD, FW.....1959-60	AC	.029	.020	36	98	Fly	(2)	F	.013	.013	Fly	10 1/2	3/4 P to 1 1/4 P	3/4 P to 1 P	1/8 to 3/8	
VOLKSWAGEN—Germany																
11, 14.....1959	Bo	.026	.016	42	7 1/2 B	FP	(8)	---	.004C	.004C	FP	2 1/2	2 1/2	3/4	1/8 to 1 1/8	
22.....1959	Bo	.026	.016	42	7 1/2 B	FP	(8)	---	.004C	.004C	FP	2 1/2	0	3/4	1/8 to 2	
11, 14.....1960	Bo	.026	.016	42	7 1/2 B	FP	(8)	---	.004C	.004C	FP	2 1/2	2 1/2	3/4	1/8 to 1 1/8	
22.....1960	Bo	.026	.016	42	7 1/2 B	FP	(8)	---	.004C	.004C	FP	TC	0	3/4	1/8 to 1 1/8	
VOLVO—Sweden																
P4450L, P5440L.....1959	Ch	.030	.018	50	4B	Fly	(2)	F	.020	.020	Fly	32	3/4 N to 1 1/4 P	3/4 N to 1 1/4 P	0 to 1/8	
P4450L, P5440L.....1960	Ch	.030	.018	50	4B	Fly	(2)	F	.020	.020	Fly	32	3/4 N to 1 1/4 P	3/4 N to 1 1/4 P	0 to 1/8	
P1220L.....1960	Ch	.030	.018	50	4B	Fly	(2)	F	.020	.020	Fly	32	0 to 1 P	0 to 1 1/4 P	0 to 1/8	
WARTBURG—Germany																
Sedan, Coupe, Station Wagon, Roadster.....1960	Bo	.026	.016	No	22	CP	(14)	---	---	---	---	---	0	2	0 to 1/8	
WOLSELEY—Britain																
1500.....1959	Ch	.025	.015	80	88	CC	(2)	F	.015H	.015H	CP	5	3	2	0	
15-50.....1959	Ch	.025	.015	80	58	CC	(2)	F	.015H	.015H	CP	5	2 1/4	3/4	0	
6-90.....1959	Ch	.025	.015	35	3 1/2 B	CC	(1)	F	.012H	.012H	CP	5	3	1 1/4	0	
15-60.....1960	Ch	.025	.015	80	TC	FP	(2)	F	.015	.015	CC	TC	1 1/2	3/4 to 1	1/8 to 1/8	
6-99.....1960	Ch	.025	.015	35	7B	FP	(1)	F	.012H	.012H	CC	5	1 1/4	1	1/8	

1-1, 5, 3, 1, 2, 4.
2-1, 3, 4, 2.
3-1, 2, 4, 3.
4-1, 2, 3.
5-1, 4, 2, 6, 3, 5.
6-1, 5, 4, 8, 6, 3, 7, 2.
7-1, 2.
8-1, 4, 3, 2.
9-1, 12, 9, 4, 5, 8, 11, 2, 3, 10, 7, 6.
10-1, 4, 3, 6, 8, 2.
11-4, 2, 3, 1.
12-1, 3, 6, 2, 7, 5, 4, 8.
13-1, 6, 4, 5, 7, 3, 6, 2.
14-1, 3, 2.
A—After top center.
A-B—A.C. or Bosch.
A-C—A.C. Spark Plug Div.
A-M—A.C. or Marechal.

B—Before top center.
B-B—Bosch or Beru.
BBC—Bosch, Beru or Champion.
B-C—Bosch or Champion.
Be—Beru.
Bo—Bosch.
C—Cold.
Ca—Camshaft chain.
C-A—Champion or A.C.
Cam—Camshaft.
Cap—Camshaft pinion.
CC—Crankshaft and camshaft sprockets.
CD—Crankshaft damper.
Cfb—Camshaft and front bearing cap.
Ch—Champion.
CP—Crankshaft pulley.
CS—Camshaft and sprocket.

CT—Crankshaft pulley and timing gear cover.
CV—Cylinder head and vibration damper.
F—Front.
Fa—Fan.
FC—Flywheel and crankshaft pulley.
FCC—Flywheel and crankshaft and camshaft sprockets.
Fir—Firestone.
Fl—Front left.
Fly—Flywheel.
FP—Fan drive pulley.
FR—Front right.
H—Hot.
K—K.L.G.
L-C—Lodge or Champion.
Lo—Lodge.
Ma—Marchal.

M-A—Marchal or A.C.
Mar—Marelli.
M-L—Marelli or Lodge.
N—Negative.
No—None.
NTT—Nippon Tokyushu Tagigyo.
Ofc—Centrifugal oil cooler body and crankcase front cover.
P—Positive.
R—Rear.
Rl—Right.
TC—Top center.
TCC—Timing chain cover.
T-F—Timing case cover or flywheel.
TG—Timing gears.
Tgh—Timing gear housing.
VD—Vibration damper.

LIGHT TRUCKS

MAKE AND MODEL	WHEEL-BASE		Gross Vehicle Weight for Normal Service (Lb.)	Chassis Weight (Lb.)	TIRE SIZES		Make and Model	ENGINE DETAILS				TRANSMISSION		REAR AXLE				
	Minimum Standard (in.)	Maximum Standard (in.)			D-dual rear S-single rear			No. of Cylinders, Bore and Stroke (in.)	Displacement (Cu. in.)	Comp. Ratio (to 1)	Torque (Lb. Ft.)	Max. Brake H.P. at Given R.P.M.	Make and Model	Forward Speeds	Make and Model	Gear and Type	Drive and Torque	Gear Ratio Range in High
					Standard Front and Rear	Maximum Authorized Tire Size (Duals unless noted)												
Chevrolet...O-1170	119	...	4900	3740	8.00/14S	8.50/14S	O-Hi. Thrift	6-3/4x3 1/2	238	8.3	217	135-4000	Own ¹	3	Own ¹	HS	...	3.08-3.70
.....O-1270	119	...	4900	3760	8.00/14S	8.50/14S	O-Turbo-Fire ²	6-3/4x3 1/2	263	8.6	275	170-4200	Own ¹	3	Own ¹	HS	...	3.08-3.70
.....O-1180	119	...	4900	3680	8.00/14S	8.50/14S	O-Hi. Thrift	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own ¹	HS	...	3.08-3.70
.....O-1280	119	...	4900	3690	8.00/14S	8.50/14S	O-Turbo-Fire ²	6-3/4x3 1/2	263	8.6	275	170-4200	Own ¹	3	Own ¹	HS	...	3.08-3.70
.....OC-143	115	...	5200	3170	6.70/15S	7/17.5S	O-Th. Mas. ³	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own ¹	HF	...	3.38-3.90
(c.f.)...OC-153	127	...	5200	3220	6.70/15S	7/17.5S	O-Th. Mas. ³	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own ¹	HF	...	3.38-3.90
(c.f.)...OP-232	104	...	7000	2740	7/17.5S	8/17.5S	O-Th. Mas. Sp.	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own	HF	H	5.14
(c.f.)...OP-262	125	...	7000	2810	7/17.5S	8/17.5S	O-Th. Mas. Sp.	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own	HF	H	5.14
(c.f.)...OP-262	137	...	7000	2840	7/17.5S	8/17.5S	O-Th. Mas. Sp.	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own	HF	H	5.14
(c.f.)...OP-332	104	...	10000	2920	8/19.5S	8/19.5	O-Th. Mas. Sp.	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	4	Own	HF	H	5.14
(c.f.)...OP-352	125	...	10000	3000	8/19.5S	8/19.5	O-Th. Mas. Sp.	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	4	Own	HF	H	5.14
(c.f.)...OP-362	137	...	10000	3020	8/19.5S	8/19.5	O-Th. Mas. Sp.	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	4	Own	HF	H	5.14
.....OC-253	127	...	7500	3350	7/17.5S	8/19.5S	O-Th. Mas. ³	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	4	Own	HF	H	4.57
.....OC-363	133	...	10000	3350	8/17.5S	8/17.5	O-Th. Mas. ³	6-3/4x3 1/2	236	8.3	217	135-4000	Own ¹	3	Own	HF	H	5.14
Cleveland...LA	107	...	8200	5200	8/17.5S	...	Electric	Motor	...	...	...	...	...	...	...	HF	H	...
.....PBX	89	85	4500	2400	6.40/15S	...	Electric	Motor	...	...	...	...	...	Dana 25	Hy	H	...	
.....LC	120	140	8000	5200	8/17.5S	...	Electric	Motor	...	...	...	...	...	Dana 70	Hy	H	...	
Divco...11	100 ⁴	...	7500	2825	7.00/16S ¹⁰	...	Con F4162	4-3/4x4 ⁶	162	6.1	123	47-2800	WG T9	...	L150	Hy	...	5.57-6.16
.....114	100 ⁴	...	7500	2830	7.00/16S ¹⁰	...	Con F4162S	4-3/4x4 ⁶	162	6.2	123	55-2900	WG T9	...	L150	Hy	...	5.57-6.16
.....12	100 ⁴	...	8000	2865	7.00/16S	...	Con F4162	4-3/4x4 ⁶	162	6.1	123	47-2800	WG T87	...	L150	Hy	...	5.57-6.16
.....124	100 ⁴	...	8000	2800	7.00/16S	...	Con F4162S	4-3/4x4 ⁶	162	6.2	123	55-2900	WG T87	...	L150	Hy	...	5.57-6.16
.....13	100 ⁴	...	7300	2940	7.00/16S ¹⁰	...	Con F4162	4-3/4x4 ⁶	162	6.1	123	47-2800	WG T9	...	L150	Hy	...	5.57-6.16
.....134	100 ⁴	...	7300	2950	7.00/16S ¹⁰	...	Con F4162S	4-3/4x4 ⁶	162	6.2	123	55-2900	WG T9	...	L150	Hy	...	5.57-6.16
.....15	100 ⁴	...	7500	2995	7.00/16S ¹⁰	...	Her QXD3	6-3/4x4 ⁶	236	6.6	173	75-2800	WG T9	...	L150	Hy	...	5.57-6.16
.....154	100 ⁴	...	7500	2990	7.00/16S ¹⁰	...	Her QXD3	6-3/4x4 ⁶	236	6.6	173	75-2800	WG T9	...	L150	Hy	...	5.57-6.16
.....224	127 ^{1/2}	...	8000	3200	7.00/16S ¹⁰	...	Her QXD3	6-3/4x4 ⁶	236	6.6	173	75-2800	WG T87	...	Timken	SB	...	5.14-6.60
.....244	127 ^{1/2}	...	7500	3127	7.00/16S ¹⁰	...	Con F4162S	4-3/4x4 ⁶	162	6.2	123	55-2900	WG T87	...	Timken	SB	...	5.14-6.60
.....344	115	...	7500	3175	7.00/16S ¹⁰	...	Her QXD3	6-3/4x4 ⁶	236	...	...	75-2800	WG T87	...	L150	Hy	...	5.57-6.16
.....364	115	...	6500	3112	7.00/16S	...	Con F4162S	4-3/4x4 ⁶	162	6.2	123	55-2900	WG T87	...	L150	Hy	...	5.57-6.16
.....364HD	115	...	9500	3143	7.50/16S ¹¹	...	Con F4162S	4-3/4x4 ⁶	162	6.2	123	55-2900	WG T87	...	L150	Hy	...	5.57-6.16
.....374	115	...	9500	3232	7.50/16S ¹¹	...	Her QXD3	6-3/4x4 ⁶	236	6.6	173	75-2800	WG T9	...	Timken	SB	...	5.14-6.60
.....42	130	...	7000	3370	7.50/16S	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	L150	Hy	...	5.57-6.16
.....52	117 ^{1/2}	...	7000	3281	7.50/16S	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	L150	Hy	...	5.57-6.16
.....72	182 ^{5/8}	...	10000	4059	7.00/17	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	Int 150	Hy	...	5.57-6.16
.....57	117 ^{1/2}	...	7000	3082	7.00/17	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T87	...	Spi 60	Hy	...	4.88
.....44	130	...	7000	...	7.50/16S	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	Int 150	Hy	...	5.57-6.16
.....45	130	...	9500	...	8.25/16S	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	Int 150	Hy	...	5.57-6.16
.....54	117 ^{1/2}	...	7000	...	7.50/16S	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	Int 150	Hy	...	5.57-6.16
.....55	117 ^{1/2}	...	9500	...	7.50/16S	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	Int 150	Hy	...	5.57-6.16
.....56	117 ^{1/2}	...	7000	...	7/17.5S	...	Con G4193	4-3/4x4 ⁶	193	7.1	172	80-3500	WG T87	...	Spi 60	Hy	...	4.88
.....74	153 ^{5/8}	...	10000	...	7.00/17	...	Own Super 6	6-3/4x4 ⁶	253	7.5	220	102-3400	WG T88	...	Int 150	Hy	...	5.57-6.16
Dodge...P6-D100	108	116	5100	2425	6.70/15S	6.50/16S	Own	6-3/4x4 ⁶	230	7.9	202	120-3600	WG T88E	3	Own D1	Hy	H	4.10-4.89
.....P6-D100	108	116	5100	2575	6.70/15S	6.50/16S	Own	6-2 1/2x4 ⁶	316	8.3	286	200-3900	WG T88E	3	Own D1	Hy	H	4.10-4.89
.....P6-D200	116	116	7500	2600	6.50/16S	6.50/16S	Own	6-3/4x4 ⁶	230	7.9	202	120-3600	WG T88E	3	Spi 60	Hy	H	4.10-4.88
.....P6-D200	116	116	7500	2950	6.50/16S	6.50/16S	Own	6-3/4x4 ⁶	316	8.3	286	200-3900	WG T88E	3	Spi 60	Hy	H	4.10-4.88
.....P6-D300	126	126	9000	2975	7/17.5S	8/17.5	Own	6-3/4x4 ⁶	230	7.9	202	120-3600	NP 420	4	Own D3	Hy	H	4.10-4.89
.....P6-D300	126	126	9000	3125	7/17.5S	8/17.5	Own	6-3/4x4 ⁶	316	8.3	286	200-3900	NP 420	4	Own D3	Hy	H	4.10-4.89
(c.f.)...P6-P300	104	126	9000	2775	7/17.5S	8/19.5S	Own	6-3/4x4 ⁶	230	7.9	202	120-3600	WG T87E	3	Own P3	Hy	H	4.10-4.89
(c.f.)...P6-P300	104	126	9000	2925	7/17.5S	7/17.5	Own	6-3/4x4 ⁶	316	8.3	286	200-3900	WG T87E	3	Own P3	Hy	H	4.10-4.89
Ford...Courier	119	119	4600	3807	8.00/14S	8.00/14	Own EBP ¹²	6-3/4x3 1/2	223	8.4	206	145-4000	Own ¹³	3	Own 3000	HS	H	3.70 ¹²
.....F-100	110	118	5000	2960	6.70/15S	7/17.5	Own EBR ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	Own ¹²	3	Own 3300 ¹²	HS	H	3.70 ¹²
.....F-100-4x4	118	118	5600	3365	6.70/15S	7/17.5	Own EBR ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	Own ¹²	3	Own 3300	HS	H	3.08
.....F-250	118	118	7400	3150	6.50/16S	8/19.5	Own EBR ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	Own ¹²	3	Spi 60 ¹²	HF	H	4.88
.....F-250-4x4	118	118	7400	3470	6.50/16S	8/19.5	Own EBR ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	Own ¹²	3	Spi 60	HF	H	4.88
.....F-350	130	130	9000	3610	8/17.5S	8/19.5	Own EBR ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	WG T88A ¹²	4	Tim B140	HF	H	5.14 ¹²
.....P-350	104	122	9000	2270	7/17.5S	8/19.5	Own EBT ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	Own ¹²	3	Spi 60 ¹²	HF	H	4.88 ¹²
.....P-400	137	137	10000	2515	8/17.5S	8/19.5	Own EBT ¹²	6-3/4x3 1/2	223	8.1	203	139-4200	WG T89C ¹²	3	Spi B140	HF	H	5.14 ¹²
Studebaker...6E5	112	122	5000	2485	7.10/15S	6.50/16S	Own 1E	6-3x4	170	8.0	133	75-3600	WG T90B	3	Spi 2211	Hy	...	4.27-4.56
.....6E6	112	122	5200	2690	7.10/15S	6.50/16S	Own 4E	6-3/4x4 ⁶	245	7.5	196	94-3200	WG T90B	3	Spi 2211	Hy	...	3.73-4.55
.....6E7	112	122	5200	2770	7.10/15S	6.50/16S	Own 3E	6-3/4x4 ⁶	2594									

ABBREVIATIONS

- 1—Overdrive, Powerglide or heavy duty three speed transmissions available.

Hurry! Go for \$150,000

ENTER THE "Miss Autolite" Spark Plug Sweepstakes



VALIANT



LARK



RAMBLER



FALCON

4

1st PRIZES

your choice of compact cars
Rambler, Valiant, Lark or Falcon

OVER 1,000 OTHER PRIZES

including outboard motors • color TV
power tools • transistorized radios
luggage • watches and cameras



JANA DAIL
St. Louis, Mo.

KATHY BONN
Minneapolis, Minn.

PEGGY JACOBSEN
Downey, Calif.

PAT McCLOSKEY
Dublin, Ireland

Here's all you do. Join Dave Garroway and the cast of the NBC-TV TODAY Show in the selection of "Miss Autolite" . . . they've selected the 12 beauties on these pages . . . now *you* select the *one* who will win the "Miss Autolite" title.

Contest is open to all dealers selling or servicing cars, trucks, tractors, marine engines, or farm or industrial equipment.

How dealers win prizes. To be eligible to win prizes your completed entry form must bear a validating stamp supplied by your wholesaler salesman. You can vote as often as you like, but only votes with validating stamps can win prizes. Ask your wholesaler salesman for complete details. Contest closes June 30, 1960.

So—join the fun. Try your hand as a beauty judge! Take a good look at the beauties on the other page. Then fill in the coupon, and mail it today. All entries for the winning "Miss Autolite" bearing validating stamps will be eligible for Sweepstakes prizes. Rating of reasons why a dealer should install Autolite Spark Plugs will be considered in determining final winners. Contest will be administered by an independent contest board and its decisions will be final.

THE ELECTRIC AUTOLITE COMPANY, TOLEDO 1, OHIO

worth of Prizes

ELECT ONE OF THESE 12
BEAUTIES SELECTED BY
DAVE GARROWAY AND
THE CAST OF THE NBC-TV
TODAY SHOW



BONNIE CARLSON
Houston, Tex.



GE GE GALLIGAN
Brooklyn, N. Y.



SUZY SMITH
Concordville, Pa.



RITA MUELLER
Milwaukee, Wis.



JUDY LAWNE
New York City



ANN NEWMAN
Chino, Calif.



NANCY GIORGINI
Bangor, Me.



DODIE MARSHALL
Philadelphia, Pa.



Another "GO" Promotion from

AUTOLITE®

POWER TIP SPARK PLUGS

Now! Autolite sponsors the election news with Dave Garroway on NBC-TV TODAY Show — and "News on the Hour," NBC Radio.

FILL IN THIS COUPON . . . VOTE TODAY!

"Miss Autolite" Spark Plug
SWEEPSTAKES
P. O. Box 7020
St. Louis 77, Missouri

Validating
Stamp
Here

MA-40

I vote for _____ as "Miss Autolite."
(name)

My rating of reasons why I should install Autolite Spark
Plugs is as follows:

- ☐ universal application
- ☐ customer satisfaction
- ☐ Ignition Engineering
- ☐ Power Tip
- ☐ world-famous performance

(Use numbers 1, 2, 3, 4, 5 to designate order of importance)

NAME _____

ADDRESS _____

CITY _____ ZONE _____ STATE _____

WHOLESALE _____

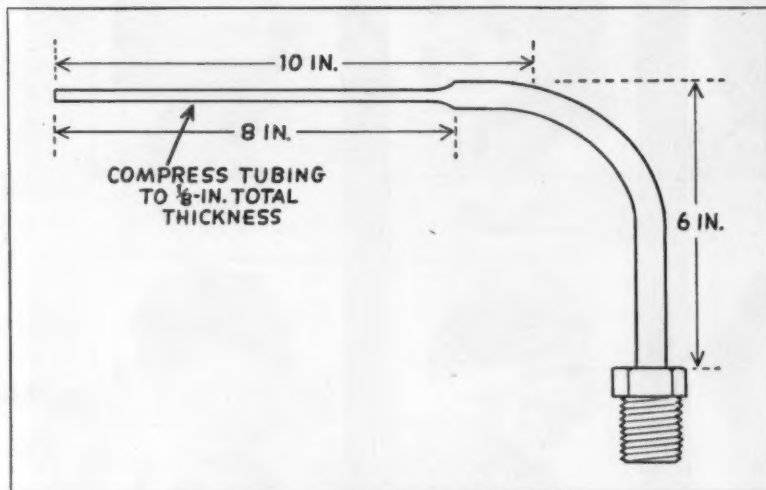
WHOLESALE SALESMAN _____

(Place in envelope and mail today!)

MOTOR AGE CLINIC

for troubleshooting

BY JACK MONTGOMERY, TECHNICAL EDITOR



Torsion bar anchor lubrication

Servicing Front Torsion Bars On Chrysler Built Cars

We are having difficulties getting the squeaks out of torsion bar equipped Chryslers. Is there some method or special grease that will eliminate these annoying squeaks?

L. Kuders
Kuders Garage
Germantown, Pa.

EVERY few thousand miles the torsion bars and rear anchors should be inspected and serviced since they are subjected to a considerable amount of road splash, dust and dirt which often contain salt or other chemicals. To service, use a mallet or soft hammer. Tap the rear anchor with sufficient force to loosen all dirt and scale accumulated between the inside diameter of the rear anchor and torsion bar. Blow out the annular cavity between the torsion bar and rear

anchor. Then rinse out the cavity with methanol type antifreeze. Again blow dry with compressed air. This anchor socket must be clean and dry. Force approximately $\frac{1}{3}$ lbs. of multi-purpose grease into the cavity between the anchor and torsion bar. This can be done by making a special nozzle which can be fitted to a high pressure grease gun. When applying grease, hold a cloth firmly against the mouth of the anchor. Apply enough grease until excess grease is forced out all around the opening. Install a new torsion bar seal package.

1957 Chevy Keeps Fouling Spark Plugs

We have a problem with a 1957 Chevy. It keeps fouling spark plugs. We have checked the engine over completely and everything is all right. We first installed #44 plugs. They lasted

about one week. We then installed #46 plugs. They lasted about two or three months. A few days ago it came back again. This time we installed #48 plugs. And that is as hot as you can go in the Chevy line. Can you give us some idea as to what is causing this car to foul out so many sets of plugs in such a short period of time?

Lou Bouchard,
No. Hollywood, Calif.

YOU do not mention whether this engine is burning excessive amounts of oil, or if it has a lot of miles on it. It possibly needs new piston rings, but before taking this engine apart, take notice when the engine smokes. If it smokes at idles, the oil is probably going down the valve stems, and fouling up the plugs. This can be corrected by installing new valve stem guide seals. Also make sure your breather system is clean, and in good operating condition.

1953 DeSoto Is Hard To Start When Hot

I have a 1953 DeSoto V-8, which is hard starting when the engine is warm. About two months ago the customer had a valve job done at another shop where he bought the car. The engine turns over good when cold and starts. When warm the engine turns over very slowly, and starts after a while. The plugs, points, coil and starter have been checked out okay. The engine
Continued on page 146



A beauty of a buy!

Whisper-quiet . . . extra power . . . wet or dry . . . light weight super capacity . . . you have your choice of all these *plus* in Black & Decker's vacuum cleaner line. The *plus* is a new heavy-duty, high-styled beauty that mounts on a pole, a wall, in bays—so light you can carry it with ease. The new No. 25 is designed especially for motels, hotels, offices, service stations and industry. Quietest commercial vacuum on the market. Priced to make it a real beauty of a buy from

Black & Decker →→→

**Mount 'em
ON POLES**

IN BAYS

ON WALLS

Here's the only vacuum made especially for auto service!



**Extra Capacity
Wet or Dry**



**Whisper-Quiet . . .
Packed with Power**

See Tools-Electric
-Yellow Pages-for
Service & Sales

THE BLACK & DECKER MFG. CO., Dept. 5104
Towson 4, Maryland (In Canada: Brockville, Ont.)

- ☐ Please arrange a demonstration of . . .
☐ Please tell me how I can obtain my Black & Decker promotional aids.

Name Title

Company

Address

City Zone State



☐ Belt Sanders



☐ Impact Wrenches



☐ Drills



☐ Vibro-Centric Drivers

Here's the most versatile vacuum you ever saw—the entirely new Black & Decker Heavy-Duty No. 25—the most compact automotive cleaner obtainable and priced to fit your purse.

Hang it anywhere! It's out of the way on a wall. Between bays, a 360° swivel lets you use its powerful suction in either one. Perfect for car wash cleanups, vacuuming after a lube job or a tire switch. It's so light you can carry it wherever you wish. On a pole, near your air hose, it lets your customers know you're the guy who gives those extra services that bring 'em back again and again. And Black & Decker's exciting new vacuum promotion aids help sell all your services!

No dirt disposal problem either. Just remove the throwaway bag and toss it in your refuse can. No shaking a dust-clogged bag and cleaning up the mess afterwards. See this great new concept soon. Just mail the coupon for a free demonstration.

Yours for as little as 36c a day!

50th YEAR

Leading Jobbers Everywhere Sell

Black & Decker

QUALITY ELECTRIC TOOLS

NEW G-E TOOL-MATE

it's FREE with an
order of 24 (2 cases) of
G-E Sealed Beam
Headlamps

Full-length trough
for small parts

Rugged 26-gauge al-
steel construction with
baked-on enamel finish

Sturdy, fold-down,
pull-type handles

Legs keep tray level
on flat surfaces

Locks onto
radiator cap

**fits radiator cap...
keeps tools handy...saves steps
...saves time...cuts tool losses!**



G-E TOOL-MATE slips onto radiator cap, keeps tools within easy reach



G-E TOOL-MATE keeps tools "on-the-job", saves time, steps and tempers



G-E TOOL-MATE keeps tools together, has trough for small parts, too

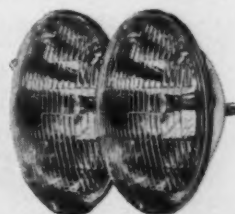
The G-E TOOL-MATE is 2" deep, 6" wide and 16" long—plenty big enough to hold all the tools normally needed for under-hood jobs. And there's a trough running the full length that's perfect for holding nuts, bolts and cotter pins that might otherwise be misplaced.

HERE'S HOW YOU CAN GET A G-E TOOL-MATE—FREE! You just buy 24 General Electric Headlamps (2 cases) at your regular cost. (Any of these five G-E types: 6006—6012—6013—4001—4002.) You pay only for the lamps. The G-E TOOL-MATE is free. And here's a tip that will help sell G-E Headlamps faster:

SELL THEM IN PAIRS. Tell customers with 2-headlamp cars how G-E SUBURBAN Headlamps will give them approximately the same lighting as new 4-headlamp systems. REMEMBER . . . only G-E SUBURBAN Head-

lamps have the low beam filament *on focus* to provide the "spotlight" effect. Motorists see better in spite of oncoming cars' lights.

Order your G-E Headlamps and G-E TOOL-MATES now from your General Electric supplier so you can start using the new G-E TOOL-MATE. General Electric Co., Miniature Lamp Dept. M-09, Nela Park, Cleveland 12, Ohio.



Progress Is Our Most Important Product

GENERAL  ELECTRIC

KEEP UP TO DATE

with this free 1960 ENGINE IDENTIFICATION CHART

It keeps you up to date on GENUINE FORD PARTS

Here's the free key to quick, efficient parts replacement and service with the compliments of your Ford Dealer.

You'll find this easily read, wall-size Model and Engine Identification Guide will instantly give you the exact part you need for the year and model car you're working on.


It tells you how to quickly identify the year and model of any Ford car or truck, too, from 1949 through 1960.

Why not get your Engine Chart today? Just go to your Ford Dealer, he'll be glad to oblige.

While you're there look into the advantages you'll get by using Genuine Ford Parts. They're Super-Quality Parts at Supermarket Prices . . . and your Ford Dealer stocks them in depth. That means you're assured of quick service, anytime.

Remember, Ford Super Quality is now available in universal parts for different-make cars and trucks. You can be sure of their uniform high quality! Ask your Ford Dealer.





Model and Engine

PASSENGER CARS

60		60	
59		59	
58		58	
57		57	
56		56	
55		55	
54		54	
53		53	
52		52	
51		51	
50		50	
49		49	

IMPORTANT TO GET THE BEST

The Passenger Car Power 90 For example, when ordering in an absolute emergency.

Get the

We suggest that you get down to us when you order Ford by making us in quickly.

Important Note: It is recommended that you get the best of the best and the best of the best. We are the only ones who can give you the best of the best.

Important Note: It is recommended that you get the best of the best and the best of the best. We are the only ones who can give you the best of the best.

Genuine Ford Parts

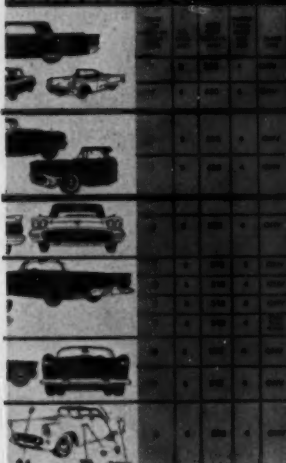
have "other" application, it

FORD

no IDENTIFICATION GUIDE



THUNDERBIRDS



TRUCKS

and Truck Bodying Plans are the only sources for correct information.

It will help us to save you time and credit customer dissatisfaction.

Use the parts you need.

Use the parts you need.

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Use the parts you need.

YOUR FORD PARTS AND SERVICE NEEDS

NOW! GET THIS AUTOLITE

YOU GET THIS TIMING LITE by Autolite

value: \$14⁶⁵

Check these quality features:

- ✓ Flashes only when switch is pressed
- ✓ Bright flash easily visible in lighted areas
- ✓ Lightweight high-impact case with pistol grip
- ✓ Fully insulated . . . uses 115-volt AC
- ✓ Long-lasting . . . trouble-free . . . no moving parts

Now, for a limited time only—here's the finest premium bonus in years! Get this top-quality timing light with the brilliance and the features of lights costing up to three times more.

Be a beauty judge!
Win big prizes!

ENTER THE
"MISS AUTOLITE"
SPARK PLUG SWEEPSTAKES!

ASK YOUR WHOLESALER FOR DETAILS!



THE ELECTRIC AUTOLITE COMPANY • TOLEDO 1, OHIO

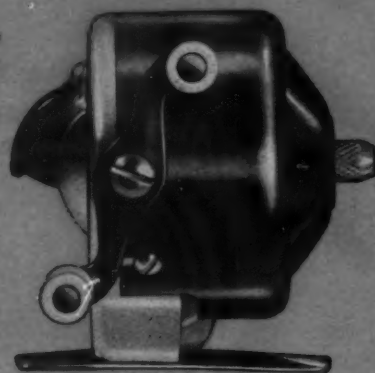


TIMING LITE SPECIAL!

PLUS BRONSON SPIN CAST FISHING REEL

value: **\$5⁹⁵**

The fisherman's favorite. You can enjoy its fun yourself or make it a selling incentive for your men!



PLUS

20 A42 AUTOLITE POWER TIP SPARK PLUGS



value: **\$20⁶⁰**

The fast-moving spark plugs for most General Motors and Chrysler Corporation cars.

Purchase any 30 Autolite Spark Plugs of your choice and the TL20 package assortment is yours for only \$20.60. The timing light and reel are yours at no cost when you sell the 20 spark plugs. See your wholesaler today!

It's all included in this TL20 assortment package.

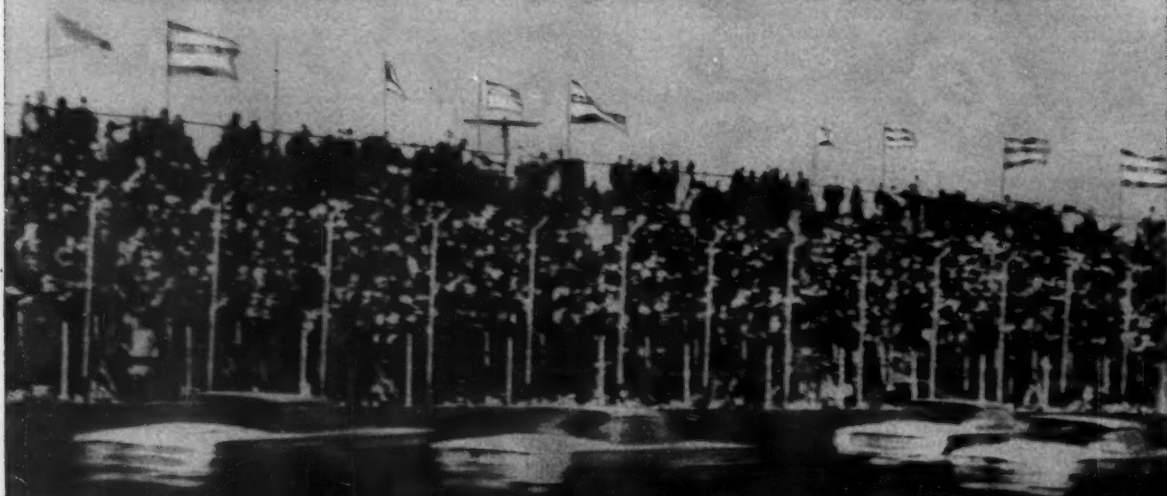


Another "GO" Promotion from

AUTOLITE. POWER TIP SPARK PLUGS

Now! Autolite sponsors the election news with Dave Garroway on NBC-TV TODAY Show — and "News on the Hour," NBC Radio.

GABRIEL DOES IT AGAIN!



WINS THRILL-A-MINUTE 500-MILE STOCK CAR CLASSIC AT DAYTONA!

Chalk up another big NASCAR* victory for Gabriel shock absorbers! For it was a Gabriel-equipped car that roared under the checkered flag in the recent 500-mile stock car championship race at Daytona, just as in the same race the year before . . . and in the big "Southern 500" classic at Darlington last Labor Day.

Remember: these are not Indianapolis-type cars, using special racing equipment. These are stock cars, the kind you service every day . . . using standard equipment, *the same production-line Gabriel shocks you can offer your customers!*

So remind your customers of Gabriel's proven superiority . . . and urge them to get a Gabriel Safety Check now, for smoother driving this spring and summer. You'll find that a little sales effort pays off big with those hard-to-beat, easy-to-sell Gabriel shock absorbers!

*National Association for Stock Car Auto Racing

The Gabriel Company • Cleveland 15, Ohio

Winner Junior Johnson says . . .

"Traveling 150 miles an hour bumper-to-bumper . . . you've got to have perfect control at all times. That's why so many of us NASCAR drivers rely on dependable Gabriel shock absorbers!"



Gabriel
SHOCK ABSORBERS



**WHAT HAVE
YOU GOT
TO SELL?**

Armstrong's famous "fist" dramatically shows how Safety Disc Tires grip the road to stop deadly skids as no other tires can.

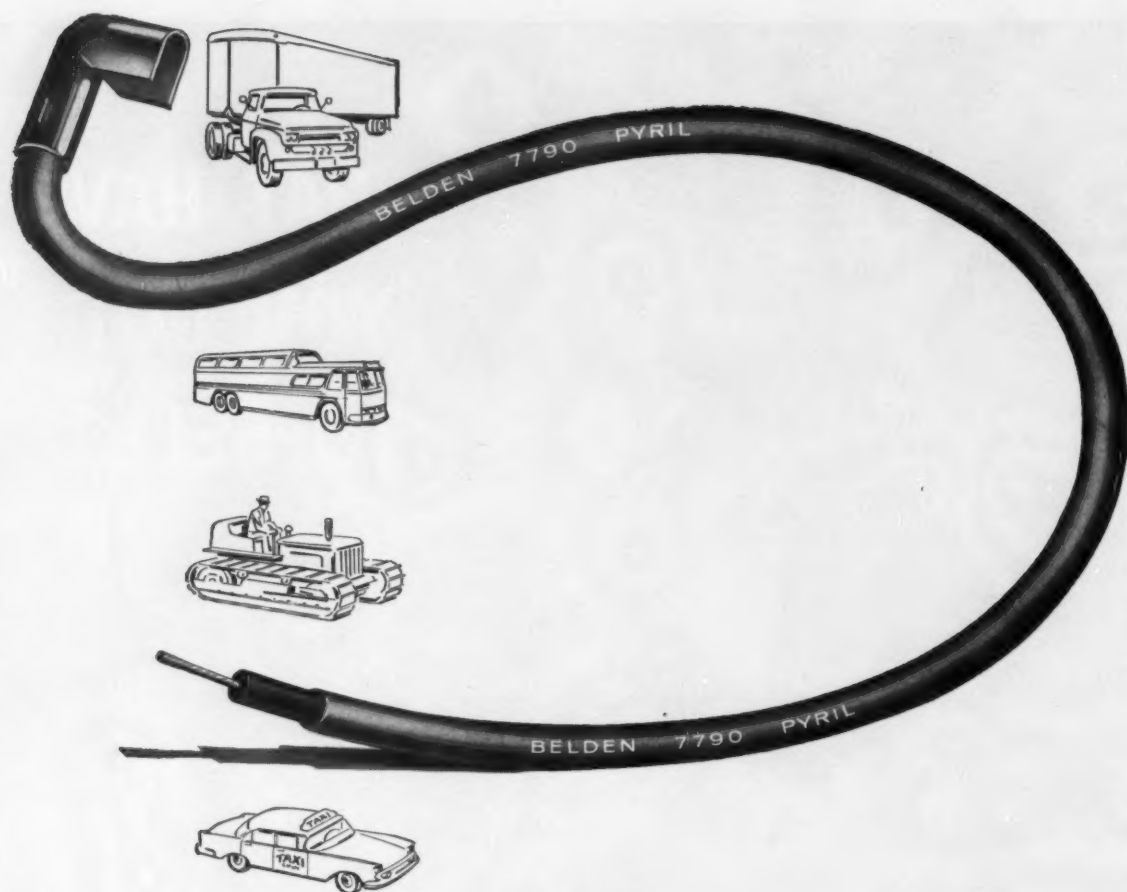
NOTHING!... unless you've got ARMSTRONG Safety Discs

What a difference — when you sell extra safety at no extra cost!

- | | |
|--|--|
| <p>1 Safety Disc tires give you a patented safety exclusive that's easy to see, easy to demonstrate and sell!</p> | <p>4 Safety Disc — and <i>all</i> Armstrong tires — give you more room for profit, tire for tire, than any important brand.</p> |
| <p>2 Safety Disc tires get the most effective advertising backing in the industry — the famous "fist" gripping the road. Includes LIFE, LOOK, POST and on TV!</p> | <p>5 Safety Disc tires carry Armstrong's iron-clad guarantee — longest, strongest in the industry — with famous road hazard feature, another big extra!</p> |
| <p>3 Safety Disc tires come in three "Miracle" grades for trade-up profits. Plus, complete line of Armstrong tires for every purpose, every price.</p> | <p>6 Safety Disc tires are <i>yours</i> to sell. No company store competition. No company "grabs" on commercial, government or truck tire sales — Armstrong <i>helps</i> you sell them!</p> |

You've got the most to sell when you sell Armstrong Tires

ARMSTRONG RUBBER COMPANY, Home Office, West Haven, Conn.



Here's a spark plug wire which
will last longer at higher
temperatures and higher voltages!

Pyril* Spark Plug Wire by Belden

- Pyril withstands extreme heats developed by trucks, buses, and tractors 50% to 200% longer than standard spark plug wires.
- The CH60 jacket of double-extruded Pyril provides greater resistance to external corona than conventional neoprene types . . the firm bonding of insulation to conductor, plus the use of a corona-resistant compound, makes Pyril many times better in resisting internal corona.
- For a longer lasting, high-heat-resistant primary wire . . which will withstand abrasions, and exposure to gasoline, oil, anti-freeze and steam cleaning fluids . . use silicone engine compartment wire by Belden.

Order from your Automotive Jobber

*Belden Trademark



One wire source for all fleet applications

There
is
Only



me

Aero-Seal **JET**

WORM DRIVE HOSE CLAMP



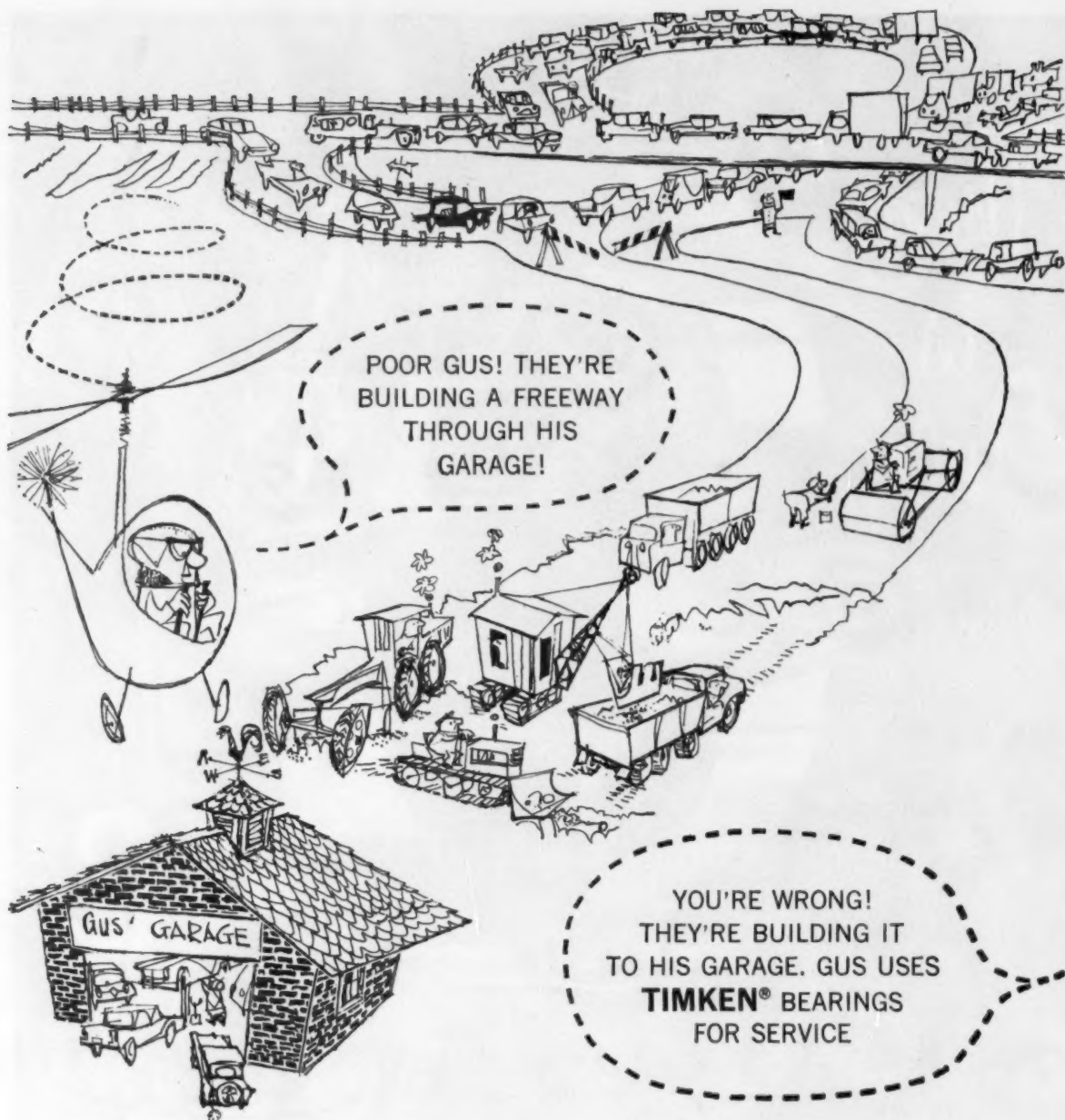
An original product design will always be copied, but the genuine article keeps the confidence of the user by dependability. AERO-SEALS laugh at vibration and corrosion... hang on tightly... never shake loose or snap open. And they won't damage hose. Bands and housings are of 302-18-8 stainless steel. No extra cost for quick-attach Jets. Regular AERO-SEALS are also available.

Complete range of sizes from 7/16" up.



BREEZE CORPORATIONS, INC.

700 Liberty Avenue, Union, New Jersey Cable Address: Breeze, Union, N. J.



You'll line 'em up, too, for bearing replacement jobs, when the word gets around you do quality work and use the best parts money can buy. Tell your customers they're getting Timken® bearings; they recognize and appreciate quality. Timken bearings are America's best-known bearings. The Timken Roller Bearing Company, Canton 6, Ohio. Canadian plant: St. Thomas, Ontario. Cable: "TIMROSCO".



CUSTOMERS LOVE YOU WHEN YOU USE AMERICA'S BEST-KNOWN BEARINGS FOR SERVICE...SO TELL 'EM IT'S TIMKEN

TIMKEN®
tapered roller bearings

Chilton's MOTOR AGE • April 1960

In the automotive service business... **THERE'S NO BUSINESS LIKE CHEVROLET BUSINESS!**

INDEPENDENT GARAGE OWNER:

"Sure appreciate your coming right over to help me out on that collision job last week."

CHEVROLET PARTS MANAGER:

"Well, that's one of our services. Any time we can assist you in estimating a repair job, give me a call and I'll be right over. Thanks for this order—you'll have the parts in a jiffy."

Here's why there's no business like Chevrolet business.

1. Over 17,000,000 Chevrolet cars and trucks on the road . . . more than any other make.
2. Your Chevrolet dealer can be your one-stop source for genuine Chevrolet parts.
3. Genuine Chevrolet parts are built of the same quality of materials and to the same rigid engineering specifications as the originals.
4. Your Chevrolet dealer can offer profit-building service aids to help you serve Chevrolet owners.

CHEVROLET DIVISION OF GENERAL MOTORS, DETROIT 2, MICHIGAN



MAKE YOUR CHEVROLET DEALER YOUR PARTNER IN SERVICE

... HE IS READY, WILLING AND ABLE TO SERVE YOU!

Jump in and start swingin'
JOIN THE EXIDE DEALER

"FREE FOR ALL"



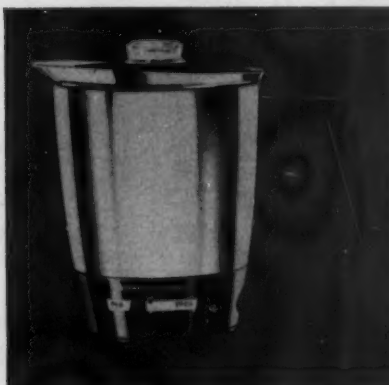
9 KNOCKOUT PRIZES →
EVERYBODY CAN WIN!

Here's your big chance to get in on the free-and-easiest prize fight of the year! Yes, *you* can win any or all of the *nine* knockout gifts in the Exide exciting "Free For All". Here's all you do: buy Exide batteries this Spring—and you automatically start piling up the points that win for you the wonderful "Free For All" prizes on the next page.

Win KODAK Cameras and picnic kits! UNIVERSAL Cordless Shavers and WESTINGHOUSE coffee makers! Your Exide distributor has the details on this free-swinging "Free For All". Call him today! You'll order again and again to get Exide profits *and* prizes! Exide Sales, Automotive Division, The Electric Storage Battery Company, Cleveland 1, Ohio.



WHEN IT'S AN **Exide** YOU START MAKING MONEY!



SCORE 2880 POINTS to win this gleaming 2-to-8 cup WESTINGHOUSE Percolator. Full-range brew control makes cup after cup of delicious coffee. Spoutless. Easy to clean. Retail value: **\$29⁹⁵**



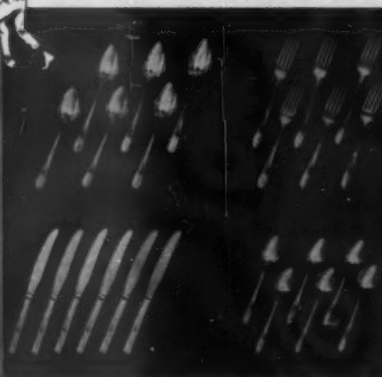
SCORE 2880 POINTS and bring home this 17-piece KODAK Party Kit. Brownie Starlet Camera, Flash Holder, Flashbulbs, Film and 15-volt battery. Convenient gadget bag. Retail value: **\$24⁹⁵**



SCORE 720 POINTS to take home this beautiful two-some... a glamorous gold-finished pin and matching earrings set with cultured pearls for her; cufflinks, tie bar, collar pin and pearl tiptack for yourself.



SCORE 720 POINTS and you'll take sharp black and white or color snapshots with your KODAK "Bulter" Camera. Universal lens, viewfinder, carrying strap and handsome black case. It's easy to use.



SCORE 1440 POINTS and surprise your wife with a 24-piece set of INTERNATIONAL Silverplate in "Bermuda Rose" pattern. 6 knives, forks, teaspoons, soup spoons. Retail value: **\$10⁹⁵**



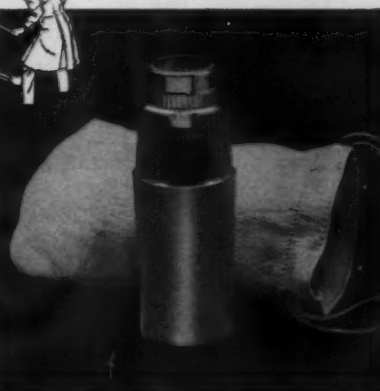
SCORE 2160 POINTS to capture this beautiful, hand-carved Kashmir Carving Set. Sturdy knife and fork lock firmly, store conveniently in a magnificent solid-walnut sheath. Retail value: **\$16⁹⁵**



SCORE 1440 POINTS and enjoy a summerful of outdoor fun with this Picnic Set. Fibreglas-lined, zippered Thermo bag holds half-gallon jug and dinnerware for family of four. Retail value: **\$10⁹⁵**



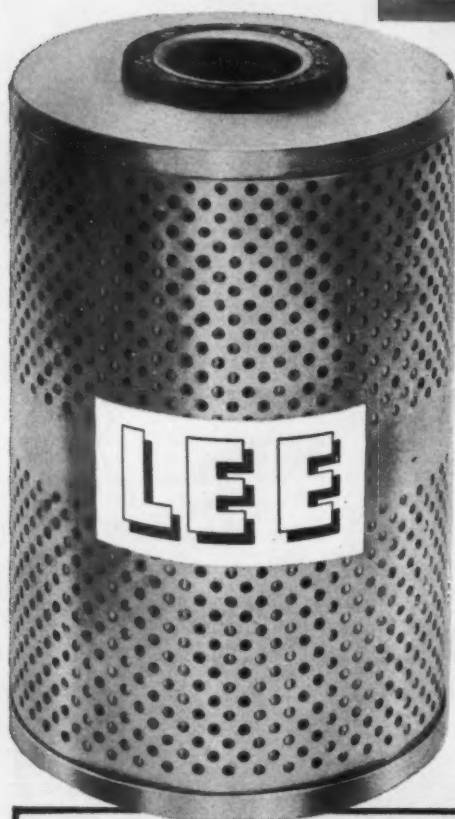
SCORE 720 POINTS to pocket this matched SHEAFFER'S Pen and Pencil Set. Fineline mechanical pencil is handy propel-repel style; popular cartridge pen comes with four long-lasting cartridges.



SCORE 2160 POINTS to get this sensational cordless UNIVERSAL Electric Shaver. Operates on four penlite batteries to give you wonderfully close, clean, smooth shaves. Retail value: **\$16⁹⁵**

CLEAN UP **2** WAYS WITH DUAL-ACTION

LEE FILTERS



1

SAVE LEE FILTER DUAL-ACTION ASSURES GREATER CLEANING ACTION

First, with Feridium Anode, the Lee Resinweld Oil Filter removes all sludge and acids, too...assuring finer engine performance. Lee Gas Filters filter both gas and water. The Lee flame-proof air filter prevents underhood fires. That's why Lee is fast becoming the first name in filters all over the country. Lee is making more customers every day...and Lee holds them for you, with performance.

2

SAVE LEE FILTER GIVES THE DEALER A CHOICE OF 3 GREAT STERLING DEALS

With every 30 Lee Filters you buy Lee gives you a gorgeous Sterling Silver glass lined Salt & Pepper shaker worth \$9. retail.



With every 30 Lee Filters you buy Lee gives you a beautiful Sterling Silver Lighter worth \$9. retail.



With every 30 Lee Filters you buy Lee gives you exquisite Sterling Silver Candlesticks worth \$9. retail.

**HERE'S
THE
DEAL:**

30 LEE FILTERS	LIST 82.00	NET 37.57
YOUR PROFIT \$45.23		

Plus your Sterling Silver bonus worth \$9.

LEE FILTER CORPORATION, Edison, New Jersey. Lee Filter Division, 287 Niagara Street, Toronto, Ontario, Canada
**LEE NATIONAL ADVERTISING IS SELLING 14,000,000 PEOPLE FROM
COAST TO COAST IN SUNDAY SUPPLEMENTS COVERING 27 MAJOR MARKETS!**

INTRODUCING



MOTORCRAFT



MOTORCRAFT...an all-new line





of quality replacement parts now at your jobber's

BE SURE OF QUALITY THAT MEANS CUSTOMER SATISFACTION...INSTALL **MOTORCRAFT** PARTS

Build your service profits with replacement parts that build customer good will. MOTORCRAFT parts are an all-new line of fast-moving parts, specially introduced for service stations and independent garages. They're as close as your telephone . . . call your MOTORCRAFT jobber and see. Ask him, too, about the most complete Quick Reference catalogs available.



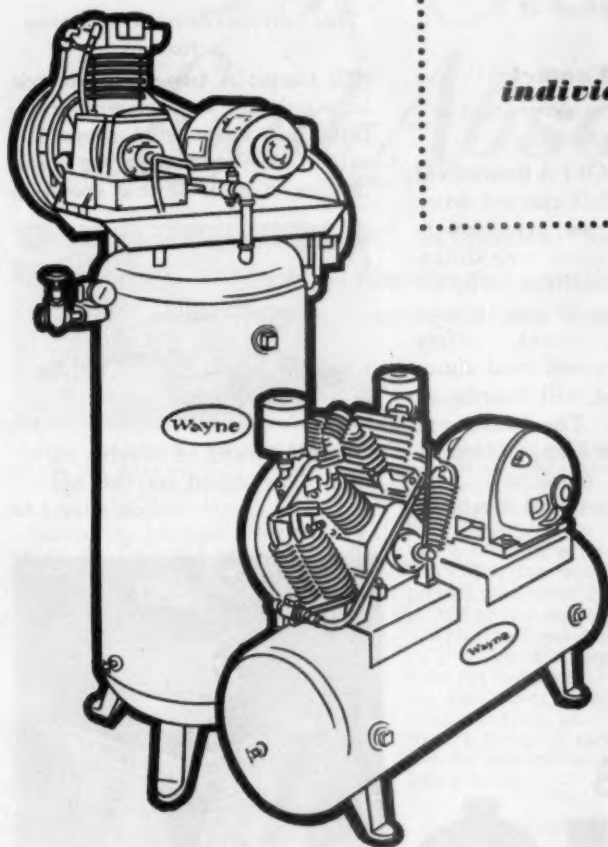
CALL YOUR JOBBER FOR MOTORCRAFT PARTS

**MOTORCRAFT parts are available
only through your jobber—the same
source that supplies many of your other
service needs. Your MOTORCRAFT
jobber assures prompt delivery,
convenience, and helps to keep inventory
investment to a minimum.**



**PRODUCTS OF
FORD
MOTOR COMPANY**

Wayne guarantees efficiency of its 2-stage compressors



**... and offers an
individual performance certificate
with each unit!**

Similarities in appearance and in certain areas of performance can sometimes make compressor selection difficult. How can you be sure of getting the most for your compressor dollar—in terms of operating efficiency, useful life and freedom from repair? Here are two ways Wayne can help you decide:

Usable air output guaranteed—When you buy a Wayne Air Compressor, you are guaranteed an actual usable air output, not just a theoretical displacement rating. Each 2-stage compressor from 1 to 10 hp is tested to determine how much free air it delivers at 175 psi and how much electric current is required. On this basis we guarantee a volumetric efficiency of 75% at 175 psi—and back it with a certificate stating the free air output of each unit. No other manufacturer does this.

Low operating speed—key to compressor life—Accelerated wear and premature failure are often associated with excessive operating speeds. Yet many 2-stage compressors today operate at 800-1000 rpm or more. Speeds of Wayne compressors rated 1 to 10 hp do not exceed 660 rpm—and are comparably low for larger models. The result is less wear, longer service life, less maintenance cost for you.

For a booklet on how to select an air compressor, and data on Wayne compressors—1 or 2-stage, 1/2 to 20 hp—send for Bulletin WPE-085A.



WORLD'S LARGEST
MANUFACTURER OF
GASOLINE PUMPS AND
SERVICE STATION EQUIPMENT

THE WAYNE PUMP COMPANY • Division of Symington Wayne Corporation • Salisbury, Md.

New Products Shopping Center

Crankshaft Regrinder

Cuts set-up and operating time



Van Norman Machine Co.: A new 46 inch crankshaft regrinder combining three major time saving features has been introduced. The model 438 "Rap-O-Matic" regrinder incorporates a 360 degree head and pendulum cross slides for faster work set-up. To further reduce operating time, the new machine also features a foot stack with a built-in, counter balancing device for

main or rod journal grinding and a head stock with a fast positioning counter balance. In addition, the steady rest has a built-in dial indicator. Write: *Van Norman Machine Co., 3640 Main St., Springfield 7, Mass.*

Fluorescent Enamel

Used for safety, decoration or identification

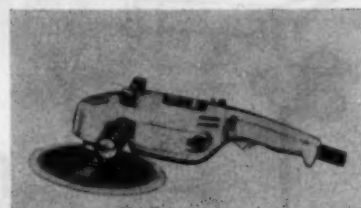
Martin-Senour Co.: A fluorescent spray enamel that can cut down accidents or draw attention to displays and signs was introduced. The enamel comes in 16-ounce spray can. It can be used on airplanes, boats, safety markers, trucks and road signs, house addresses, bill boards, or ornamentation. The fluorescent colors are red or orange. Because the fluorescent pigment is converted ultra-violet light it reflects

more light than the amount hitting it, company states. Write: *Martin-Senour Co., 2500 S. Senour Ave., Chicago, Ill.*

Auto Polisher

Has two speeds with reversing action

Skil Corp.: A two-speed electric auto polisher with reversing action, has been announced. It's said to have four operating possibilities, a high or low speed in



either forward or reverse direction. Low speed on the heavy-

Continued on page 121

BIG WESTINGHOUSE BULB BONUS\$

**\$13⁹⁵ SUN LAMP KIT & 24 WESTINGHOUSE TOWN & HIGHWAY
SAFE-T-BEAM™ HEADLAMPS...YOU SAVE MORE THAN \$5⁰⁰!**

Buy two cases of these extra benefit 6006 and 6012 headlamps and get the deluxe \$13.95 Westinghouse Sun Lamp Kit for that Summer tan the year 'round. UL approved, the kit contains a Westinghouse Sun Lamp, deluxe re-

flector, 6-foot cord. Fits anywhere. Use it at home; get extras for gifts.

REGULAR PRICE \$50.91.....SPECIAL PRICE \$45.18

Call your Westinghouse Lamp Supplier or Write: WESTINGHOUSE LAMP DIVISION, Westinghouse Electric Corporation, Bloomfield, New Jersey.

YOU CAN BE SURE...IF IT'S

Westinghouse

Announcing a revolutionary new air-oil device...greatest advance in ride control since individual wheel suspension!

The Gabriel Load-Absorber

PAT. APPL. FOR

**Completely new ... an ingenious "load sensitive"
suspension member that actually floats a
hydraulic damping unit on a cushion of air**



Today the Gabriel Company proudly unveils the most exciting product development in its 56-year history ... the Load-Absorber.

Backed by intensive research and testing, the Gabriel Load-Absorber is the first and only suspension device that automatically compensates for heavier loads without producing an unpleasantly harsh ride under normal loads.

Such a device urgently needed

Such a device was urgently needed to meet the demands of modern motoring. For while car manufacturers have succeeded in delivering an ever softer ride under normal loads, more and more Americans have been subjecting their cars to extra-heavy loads ... pulling boats or trailers, for example, or loading their trunks with vacation or camping equipment.

Air and oil: suspension of the future

Gabriel engineers found the answer in an entirely new concept in suspension: air and oil ... the suspension of the future. In simplest terms, the Gabriel Load-Absorber is a hydro-pneumatic suspension member that combines the most desirable properties of a shock absorber and an overload spring in a single integrated unit.

What happens is this: the oil, forced

into the outer cylinder on a compression stroke, compresses the air behind a synthetic rubber diaphragm sealed inside the unit. At first the air offers little resistance to compression, so that a soft ride is retained under light loads. But the more the air is compressed, the more resistance it offers, providing full support for heavy loads.

Thus the Load-Absorber is *variable* ... in that it automatically compensates for *heavier* loads without impairing the "luxury ride" under *normal* loads. And it is also *adjustable* ... in that, if the need should ever arise, it can be inflated to handle *extremely* heavy loads.

Thoroughly lab-tested and road-tested

First Gabriel engineers tested the Load-Absorber thoroughly and exhaustively in their laboratories. Then they gave it a "hell on wheels" road test. Month after month, in cold weather and hot, in wet weather and dry, on good roads and bad, the Load-Absorber was given *complete* performance tests.

A typical test course was 200 miles long, and 50 miles of this was unpaved. The remaining 150 miles included both primary and secondary paved roads, with surfaces ranging from smooth to washboard.

No sign of failure

In fact, it was such a grueling grind that, though brand-new cars were used, dozens of chassis and body parts were broken or worn out. But the Gabriel Load-Absorber came through its baptism of fire with *no sign of strain or failure*. More important, the Load-Absorber did the job ... provided a more stable and comfortable ride under *all* conditions ... and rose to the occasion when the roads and loads were worst.

A wide market

What is the market for the Gabriel Load-Absorber? The answer is that *any* car will ride more quietly, more comfortably, more safely, with Load-Absorbers. On any heavily loaded car, Load Absorbers will help prevent bumping on driveways, swaying on curves, and rear-end sag that raises headlights off the road and into the eyes of other drivers. And on any car, Load-Absorbers will deliver a "luxury ride" after the extra load is removed.

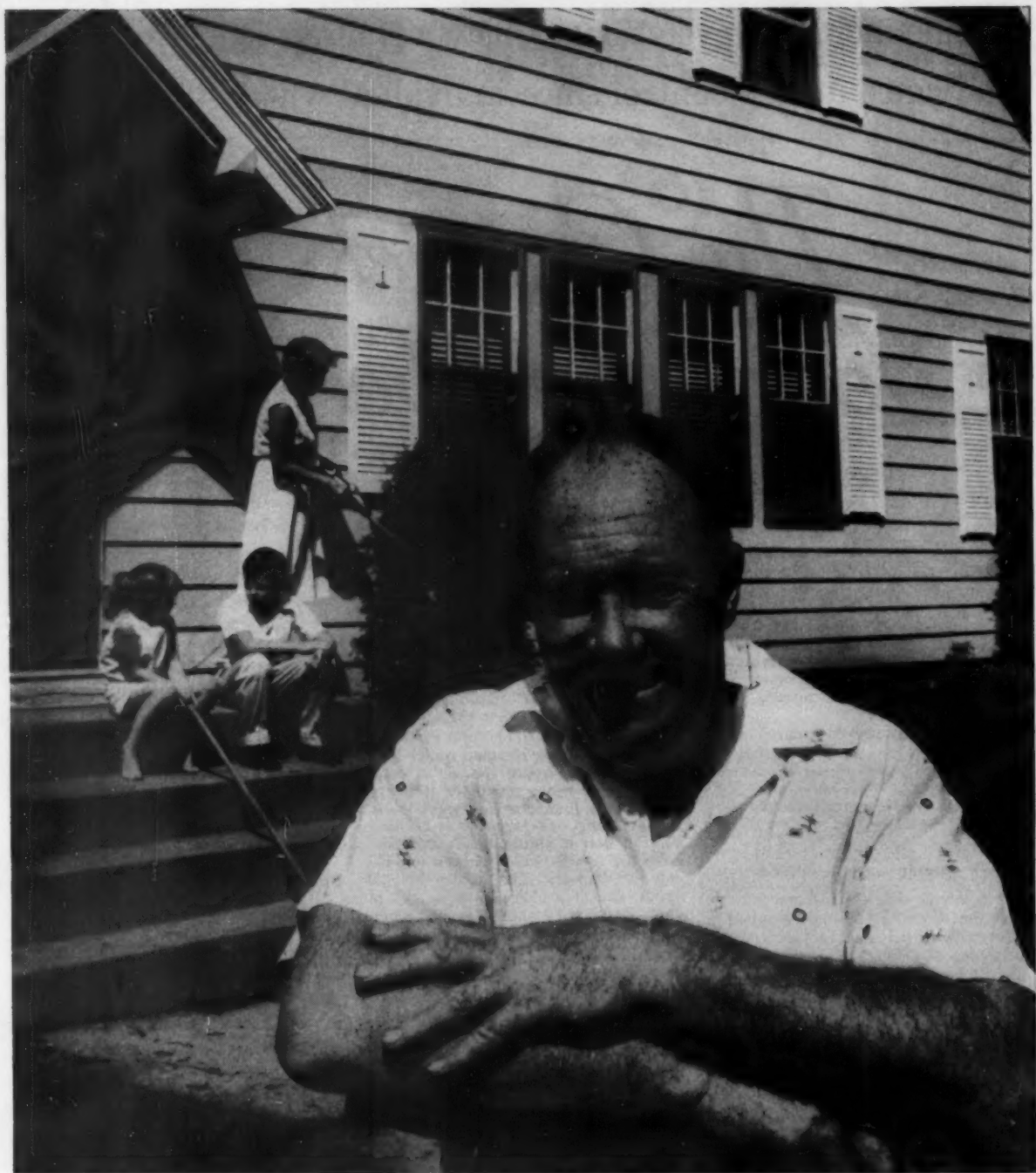
But there are particularly good prospects for the Load Absorber: salesmen, travelers, and owners of pickup trucks, station wagons, or trailers of any kind.

Clearly the market is great ... and growing. The product is right ... and ready.

Order now ... and order plenty!

The Gabriel Company • CLEVELAND 15, OHIO





TEXACO DEALER J. T. TOOHIL AND HIS FAMILY AT THEIR FINE HOME, WORCESTER, MASS., SAYS:

“I switched from my father’s grocery business to the service station business 28 years ago—and selected Texaco. It’s the best move I ever made. Business has increased steadily—about 15% last year. Right now, I’m selling 30,000 gallons a month. Texaco has always been very helpful and co-operative.” Sell the best...sell **TEXACO**



Motorists get neighborly service, and find it pays off, when they drive into Mr. Toohil's service station.

6 reasons why there's a solid future with Texaco

1. **THE BEST** petroleum products, known and accepted by car owners nationwide. Continuous research and development insure that Texaco will always have the best products.
2. **THE BEST** national advertising program, year after year . . . constantly selling Texaco Dealers to car owners everywhere.
3. **THE BEST** point-of-sale and direct mail promotional material to help bring in motorists and bring them back.
4. **THE BEST** customer credit card . . . in fact, the only petroleum credit card honored under one sign nationwide.
5. **THE BEST** retailer policy . . . Texaco helps its Dealers to market nationally-advertised and accepted TBA products.
6. **THE BEST** opportunity to cash in on "touring" business . . . because Texaco customers at home like to stop at Texaco stations when on the road. This means you have more than 40,000 other Texaco Dealers in the U.S. and Canada helping you.

TUNE IN TO THE TEXACO HUNTLEY-BRINKLEY REPORT—MONDAY THROUGH FRIDAY—NBC-TV

MA4

SALES MANAGER, TEXACO INC.,
135 E. 42ND ST., NEW YORK 17, N.Y.

I would like complete information about the possibility of teaming up with Texaco as a: ☐ Dealer, ☐ Consignee, ☐ Distributor. (Please check)

NAME _____

STREET _____

CITY _____

STATE _____

Chilton's MOTOR AGE • April 1960

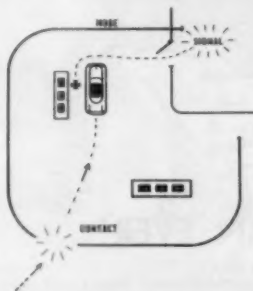
New Products Continued from page 118

duty polisher is recommended for rubbing and cleaning, removing orange peel (spray marks), and for polishing in difficult and confined areas. Low speed also works better with heavy duty polishes on badly oxidized finishes, company reports. Write: Skil Corp., 5033 Elston Ave., Chicago 30, Ill.

Signal Bell

Rings the instant car enters service bay area

Aro Equipment.: A new "Hi-Tone" electric driveway signal unit for automotive service stations is announced. This new signal bell rings the instant a car enters the service bay area. It uses electricity only when ringing and will not continue to ring should car wheels stop on hose. All parts and control switch are enclosed by bell on a 6½ x 6 inch panel. Signal bell mounts in any



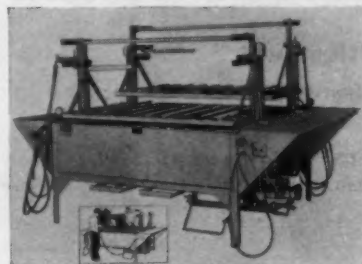
convenient position and need not be exactly level to operate. Write: Aro Equipment Corp., Bryan, Ohio.

Radiator Servicing Equipment

Features 2-in-1 test and repair bench

Inland Mfg. Co.: The newest unit of radiator servicing equipment is company's extra large double-capacity Model 70 dual test and repair bench. It is suitable for auto radiator departments where unusually large

radiators are serviced, or where 2 men are employed during busy season or all year, company states. Two elevators can be



operated simultaneously to accommodate largest radiators, or separately so two operators can work on regular size radiators at the same time. Separate ½ H.P. sealed motors and heavy duty drives are provided, one for each of the two elevators. Write: J. V. Grasso, Mgr. Equip. Div., Inland Mfg. Co., 1108 Jackson St., Omaha 2 Neb., or phone HARney 1108.

Heavy Duty Jack

Offers all-purpose heavy duty lo-jack for new Corvair

Stephenson Industries Inc.: A new all-purpose Step-Draulic Heavy Duty Lo-Jack is now being marketed. In addition to jacking up the Corvair vehicle it removes and replaces the power train as a complete assembly. This one ton capacity, heavy duty jack also services all automatic and conventional cars,



as well as largest truck transmissions and differentials. It is

Continued on page 122

New Products

Continued from page 121

a complete under-car servicing unit when working without hoists. Write: *Stephenson Industries, Inc., 548 E. Fort St., Detroit, Mich.*

Ring Tool

Installs rings on pistons without breakage

Sealed Power Corp.: A tool for use in installing rings on pistons

without breakage or distortion is available. The MT-2 ring tool op-



erates like a reverse-scissors, with pressure on the handles expanding the ring. It can be used

with rings from 2½ to 6¼ inches in diameter. Write: *Sealed Power Corp., 2001 Sanford St., Muskegon, Mich.*

Cord Reel Lights

May be mounted on wall or overhead

Belden Mfg. Co.: A new line of cord reel light has been announced. They are available in two or three conductors with neoprene jacket. This gives resistance to grease, oil, moisture, and dirt. The Cord Reel Light features lengths of 20, 30 and 40 feet to give a varied selection.

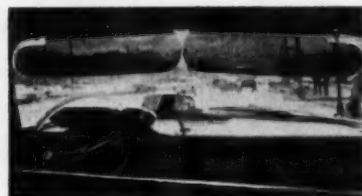


The entire unit may be mounted on a wall or overhead type of installation. The Reel Lights are all UL listed and approved as completed assemblies. Write: *Belden Mfg. Co., 415 S. Kilpatrick St., Chicago 80, Ill.*

Sun Visors

Reduces blinding reflections

The MoPar Division of Chrysler Motors Corp.: New tinted plexiglass sun visors for all 1960 Chrysler Corporation cars, ex-



cept convertible coupes and Imperial models are available. The newly designed visors increase driving safety and comfort by reducing blinding reflections and harmful direct sun rays, it is stated. The visors can be mounted on either the driver or

Continued on page 124

NOW! COAST TO COAST SERVICE



**GENUINE LUCAS
ORIGINAL EQUIPMENT
REPLACEMENT PARTS**

GENERATORS
STARTERS
DISTRIBUTORS
COILS
BATTERIES
LAMPS
WINDSHIELD WIPERS
REGULATORS
HORNS
GIRLING BRAKES
GIRLING
SHOCK ABSORBERS

FASTER THAN EVER!

Wherever you are . . . whatever you need . . . your local Lucas factory branch ships you the genuine Lucas original equipment you want . . . **FAST!** Expanding every day, Lucas is right on the job to help you fill America's growing demand for the world's finest electrical equipment and accessories for British cars.

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JACKSONVILLE 400 S. Edgewood Avenue • EVergreen 8-7607

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LOS ANGELES 5025-5029 W. Jefferson Blvd., P.O. Box 78508
REpublic 1-7211

DOUBLE YOUR PROFIT with the **NEW TWIN PACK**

... it sells two quarts instead of one!





Carton is a handy litter box



Holds supply of sand



Doubles as a highway warning



As a boat-dock night marker



As a useful carryall

This handy, extra-profit Pennzoil package holds two quarts of famous Pennzoil Outboard Motor Oil in cans or bottles. Makes it easy for you to sell every outboard customer *two-to-go*!

This 100% pure Pennsylvania oil keeps plugs, rings and ports clean—for quick starts and smooth power at all speeds. The Tough-Film® in Pennzoil gives engine parts superior protection.

And the TWIN PACK is a valuable extra. The reflectorized carton is easy to carry. Easy to store in boat or car. It makes a convenient take-along supply of Pennzoil outboard oil to increase sales.

Call your Pennzoil distributor today for his Special Profit Offer on performance-proven Pennzoil Outboard Motor Oil. He's listed in the Yellow Pages.

Member Penn. Grade Crude Oil Assn., Permit No. 2, Oil City, Pa.

New Products

Continued from page 122

passenger side. Write: John T. Davis, Adver. Supervisor, Chrysler Motors Corp., MoPar Div., 7000 E. Eleven Mile Rd., Center Line, Mich., or phone JE 9-3000.

"Non Rust" Process

Prevents pipe rusting in storage
The AP Parts Corp.: A success-

ful non-rust treatment for exhaust and tail pipes has been an-



nounced. After three years of development and salt spray tests by

independent testing laboratories in Michigan and Ohio, the new coating is now being applied to all tail and exhaust pipes manufactured by AP in Toledo. Write: H. C. (Skip) Stivers, Sls. Mgr., The AP Parts Corp., 1801 Speilbusch Ave., Toledo 1, Ohio., or phone CH 4-4951.

Shock Absorber Tool

Removes and installs bayonet type shock absorbers

Herbrand Tools, The Bingham-Herbrand Corp.: Tool features a new way to remove and install bayonet type shock absorbers. It is called the No. 2200 shock ab-



sorber tool and is designed for servicing General Motors, Ford and Chrysler cars. The "T" end of the tool fits over the bayonet of the shock, and holds it while the retaining nut is removed or replaced. The threaded stem end screws on the shock over the bayonet so that new shocks can be pulled up through the coil spring. This permits installation of the grommet, cap and retaining nut. Write: Herbrand Tools, Herbrand Div., The Bingham-Herbrand Corp., Corner Stone & Lake Sts., Fremont, Ohio.

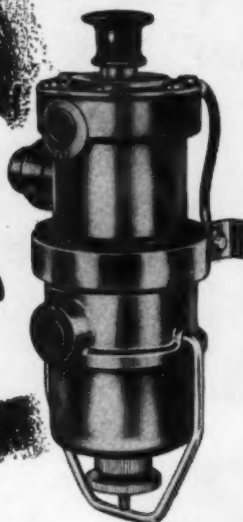
Spray Booths

Offers line of dry type spray booths

Binks Mfg. Company: A line of dry type spray booths to be offered as standard equipment has been introduced. The new line includes filter floor type spray booths, baffle floor type booth, filter bench booths and baffle bench booths. Also offered are an auto spray booth, a truck spray booth and a combination

Continued on page 126

8 reasons to stock and sell the Stewart-Warner electric fuel pump Model 240-A



1. Quick starts—the 240-A starts to work the instant the ignition switch is turned on. Carburetor bowl is full before the starter turns the engine.

2. High volume delivery of up to 60 gph will easily supply the largest truck and bus engines operating at full throttle.

3. No vapor lock because the 240-A operates independently of the engine, quickly forces vapor accumulations from the line.

4. Automatic pressure control prevents over-pressure and flooding—prevents under-pressure and starving. No need for accessory pressure regulator.

5. Adjustable pressure up to 8 psi, more than enough for most truck and bus installations. Pressure adjustment is quick, easy.

6. Built-in giant-size fuel filter doesn't restrict fuel flow. No other fuel line filter is needed.

7. Minimum stock required because only two models—6 and 12-volt—fit all gasoline engines. Not polarity sensitive.

8. Easy field service because simplified construction means mechanics can quickly make complete overhauls. No factory rebuilds!



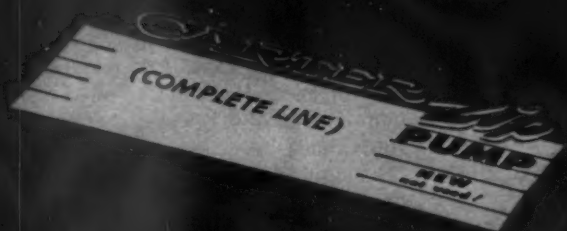
Call your
wholesale supplier today or write:

220 Series Fuel Pump—Economical, long-lasting diaphragm type assures trouble-free service! Delivers up to 20 gph. 6 and 12-volt models.



INSTRUMENT DIVISION
STEWART-WARNER
CORPORATION

Dept. AD-40, 1840 Diversey Parkway, Chicago 14, Illinois



HERE'S WHAT'S IN IT FOR YOU!

MORE SALES—MORE SATISFIED CUSTOMERS
with the only lightweight aluminum fuel pump available today. Carter casts its pumps in this modern metal for greater strength and more heat resistance. The new 'rolling loop' diaphragm provides maximum protection against vapor lock and it's an exclusive feature designed by Carter. These features plus many more make the Carter ZIP-PUMP the most advanced pump line available. Call your Carter supplier and get the complete profit story.

CARTER
CARBURETOR

DIVISION OF INDUSTRIES INCORPORATED
ST. LOUIS 7, MISSOURI

New Products Continued from page 124

auto and truck spray booth. All booths are built of 18 gauge sheet steel sectional panels and are designed for easy erection at the customer's site. Write: *Binks Mfg. Co., 3114 Carroll Ave., Chicago 12, Ill.*

Reversible Scru-Drill

Combines features of a drill and power screwdriver



Black & Decker Mfg. Co.: A double-purpose tool that combines the properties of a drill and power screwdriver in a single, hand-held unit has been announced. As a drill the new

tool, called the "Scru-Drill" can be used with all kinds of bits. A turn of the spindle collar on the tool converts it to a practical equivalent of the company's "Scrugun," with a true screw-driving clutch. As in the "Scrugun," the bit idles for finding the screwhead slot, then forward pressure of the tool engages the mechanism and drives the screw. As soon as the screw is properly seated release of driving pressure disengages the clutch, halting the drive and helping prevent any marring of the work surface. Write: *J. F. Apsey, Jr., Public Relations Dir., Black & Decker, Towson 4, Maryland or phone Valley 3-4400.*

Oil Filter Refiner

Heats oil in winter and cools it in summer

Electro-Refiner Corp.: A new

electrically heated Reclamo oil filter and refiner is being marketed. This new filter, featuring automatic heat control, heats the oil in winter and cools it in the summer. The filter material is a



high grade loose cotton fibre. It not only keeps the oil clean, but by being heated, eliminates the corrosive action of water, company states. The Reclamo can be installed anywhere under the hood. Present filters need not be removed. Write: *Electro Refiner Corp., 6251 West Touhy Ave., Chicago 46, Ill.*

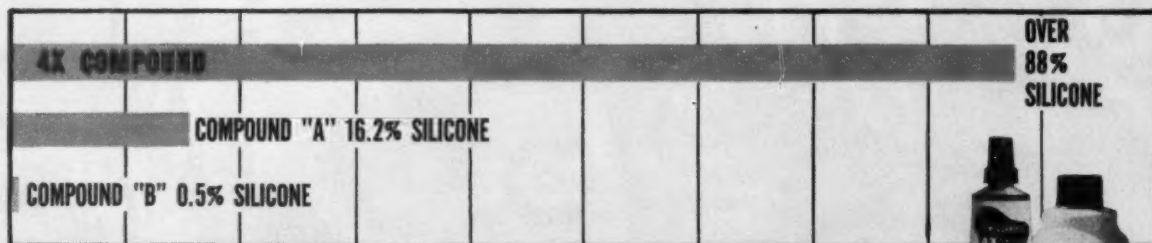
Continued on page 133

USE 4X, THE ALL-SILICONE RUBBER LUBRICANT . . . TO GIVE YOUR CUSTOMERS THE SERVICE THEY EXPECT

- ★ Stops squeaks and sticking of weather-stripping, prolongs life of rubber parts.
- ★ Lubricates radio antennae, improves reception; ideal lubricant for window channels.
- ★ Keeps battery terminals clean, minimizes corrosion build-up, prevents electrical losses.
- ★ Protects ignition systems, keeps moisture out, preserves wiring.

LABORATORY TESTS PROVE:

Dow Corning 4X Compound contains 5 times more silicones . . . lubricates 10 times longer!

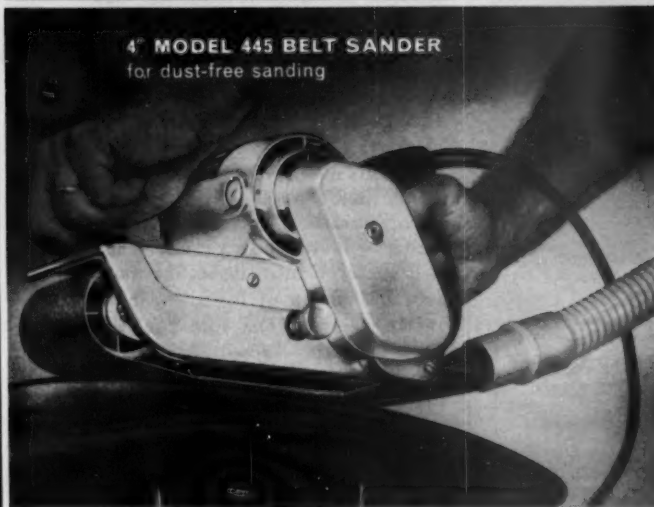


Order 4X Compound or Spray from your jobber today. Write for new brochure containing straight facts about "silicone" lubricants . . . address dept. 1504





7" SUPER DUTY MODEL 853
for continuous, heavy duty sanding



4" MODEL 445 BELT SANDER
for dust-free sanding



7" SPECIAL DUTY MODEL 551
for low cost power sanding

Which Skil sander do you need?

For Continuous, Heavy-Duty Body Sanding . . . Select the Model 853 Skil 7" Super Duty Disc Sander. Power-packed . . . actually delivers 2.48 horsepower! And it's plenty rugged for years of trouble-free use. All ball bearing construction. Remarkably light and easy to handle. Also excellent for wire brushing, disc and cup grinding. Available with lower r.p.m. (Model 852) for safe 9" disc sanding.

For Dust-Free Sanding . . . Choose the new Model 445 Skil 4" Belt Sander.

Has "direct flo" instant dust pick up that removes over 90% of unhealthy fine dust . . . belts stay cleaner, last longer. Keeps shop so clean you can sand and paint in same area. Full 4" belt covers 20% more surface than ordinary 3" sanders. Lightweight, easy-to-handle. Soft rubber pad follows contours, won't gouge surface.

For Low Cost Power Sanding . . . Pick the Model 551 Skil 7" Special Duty Sander. Compare it on all three

counts—power, price and performance! Compact, well balanced, easy to control. Faster, more powerful than many more expensive makes.

Ask your Skil distributor to demonstrate any one—or all—of these fine sanders soon. He's listed under "Tools—Electric" in the Yellow Pages. Or write for complete information: Skil Corporation, 5033 Elston Avenue, Chicago 30, Illinois, Dept. 102D, In Canada: 3601 Dundas Street West, Toronto 9, Ontario.



...and SKILSAW POWER TOOLS

Holley Carburetor Continued from page 39

mixture then moves down another vertical passage. At the bottom of this passage the fuel air mixture divides into two directions. One path to the idle discharge passage, and the other to the idle transfer passage. The fuel flowing past the pointed tip of the idle adjusting needle controls the mixture delivered at idle.

Turning the needle out increases

the volume of this predetermined mixture by allowing a greater volume flow of the fuel air mixture into the cylinders. From the idle adjusting needle chamber, the fuel goes through a short passage in the main body, and then down into the throttle body.

The fuel is then discharged into the throttle bore below the throttle plate. Mixture needle adjustments

located at primary side only. During off idle operation, when the throttle plate is moved, the fuel passes from the metering body to the main body, then into the throttle body passage. As the idle transfer slot is exposed to manifold vacuum, the fuel is discharged into the throttle bore.

As the throttle plate is opened wider, and engine speed increases, the air flow through the carburetor is increased. This increases the velocity of air opposite the transfer slot. This creates an increased vacuum in the venturi, to bring the main metering system into operation. The flow from the idle system tapers off as the main metering system begins to discharge fuel.

The two systems are engineered to provide smooth gradual transition from idle to cruising speeds. As the throttle is opened further, the air passing between the transfer slot and the throttle valve decreases in velocity. This decrease in velocity, decreases the suction at transfer slot.

Opening the throttle increases the velocity and increases the suction at the main metering system discharge nozzle and booster venturi. This new model carburetor incorporates a by-pass idle system.

Air passes through an opening into the main body cavity, where it is metered into the intake manifold by means of an idle air adjusting screw. The idle fuel and idle air are mixed below the throttle body. Where this system is used the throttle plates are against the bores at idle.

The idle air flow can be shut off completely by turning the idle air adjusting screw against the throttle body seat. There is an additional air bleed in the idle transfer passage that adds additional air from the throttle bore.

The accelerating pump system is of the spring loaded diaphragm type. The pump is operated through a pump lever actuated by throttle movement. When the throttle is open the pump linkage actuated by a cam on the primary throttle shaft forces the pump diaphragm up.

As the diaphragm moves up the pressure displaces the fuel in the chamber, and forces the intake check valve closed. The fuel then

Continued on page 129

WANTED

**DEALERS FOR THE NOVI DELUXE
AUTOMOBILE AIR CONDITIONERS**



NOVI No. 1 **QUALITY
PERFORMANCE
PRICE**

**Superior Cooling
Beautiful Design Advanced Engineering
Greatly Reduced Installation Time**

Adapter kits packaged and sold separately for greater flexibility in sales. Five Novi warehouses assure fast delivery and lower inventory investment.

90 DAY WARRANTY ON PARTS

**SPECIAL
DEALER
PRICE**

\$182⁵⁰

Complete with clutch
Federal Excise Tax
included.

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WAREHOUSES: 2501 South Grand Avenue, Los Angeles, Calif. • 2112 West 7th Street, Fort Worth, Texas • 2932 Troost Avenue, Kansas City, Missouri 20830 Coolidge Hwy, Detroit, Mich. • 190 Fourteenth St., N.W., Atlanta, Ga.

For information, write Novi Sales and Service, 20830 Coolidge Hwy, Detroit, Michigan

Holley Carburetor Continued from page 128

flows through a passage in to the pump nozzle, where it is discharged into the venturi throat. Returning the throttle to a closed position, returns the linkage to its normal position.

The diaphragm is then forced down by the spring and fuel then flows through the intake check valve into the pump chamber, filling the chamber with fuel, ready for the next period of acceleration.

Thru Main Jet

In the main metering system at cruising speed, the fuel flows from the float chamber through the main jet. The main jet meters the amount of fuel that flows into the main well.

The fuel in the main well then moves up the well, past the high speed air bleed hole. Air enters through this hole and mixes with the fuel. This air and fuel mixture then continues up the passage, where it is subjected to a measure of vaporization.

The air and fuel mixture then is discharged into the booster venturi, where it is again subjected to vaporization, due to the vacuum present and is dispersed and atomized by the velocity of air passing through the booster venturi.

This air and fuel mixture then travels through the throat of the carburetor, past the throttle valves into the manifold.

Under certain high speed conditions and heavy power demands additional fuel is required. This additional fuel is supplied by a vacuum control power enrichment valve. The valve is located so that the diaphragm of the valve is subjected to manifold vacuum.

The valve has a fuel needle valve and orifice, that control the flow of fuel through the valve. When manifold vacuum drops as is the case under full throttle operation, the spring opens the power enrichment valve, allowing additional fuel to flow from the float bowl.

This fuel enters the main well, in addition to fuel entering through the normal main metering jet. This additional amount of fuel increases the richness of the mix-

ture and supplies adequate fuel quantity to maintain the desired power and speed.

The choke system of this carburetor is of the automatic type, with the control unit mounted on the engine and not on the carburetor. The vacuum piston on this carburetor with mechanical secondaries, is located in the main body of the carburetor.

It is connected to the choke shaft by means of levers and linkage. When the engine is cold, the bi-metal spring closes the choke plate for starting.

When the engine is running the manifold vacuum is applied to the choke piston. In conjunction with the help of air flowing over the off-set choke valve plate, opens the choke valve against the tension of the bi-metal spring. The bi-metal spring is located in the exhaust

Continued on page 130

POWER



BY THE WORLD'S
PIONEER AND LEADER
IN TIRE CHANGERS

EFFORTLESS AIR-POWERED BEAD-BREAKING
AT THE PRICE OF LOW-COST CHANGERS.

NEW DROP-LOCK—self-locks
when simply dropped against
hub. No twist, no adjustment.

2 1/2 TIMES STRONGER Nylon
Air Tube — proved impervious to
grease and oil. Outwears rubber
5 times.

2-in-1 ROLL ON - ROLL OFF
Mount-Demount Tool — "Lift and
turn" action of small ball and
prevents damage in demounting,
while mounting and slides beads
back on, fast and easy with posi-
tive protection of beads.

**AIR
POWER
DOES THE
WORK**

Operator
simply holds
bead-breaker
in place.



Patents
Pending

at a price!

Model LCP

only **\$129.00**

3 YEARS TO PAY ON EASY
PAY AFTER PROFIT PLAN

RATCHET type adjust-
ment to fit all sizes of
tires, 10 thru 19.5 inch
including even boat
trailer tires.

**SAFETY-CONTOUR
BEAD-BREAKER** —
Long, powerful stroke
free beads effortlessly,
safely.



**BUY ON PROOF,
NOT ON CLAIMS**

A Mobile Showroom is ready
to demonstrate the LCP at
your door, on your tires.

*Send Coupon for Free Safety
Facts or Demonstration*

**BIG 4
INDUSTRIES, INC.**

FOSTER, OHIO

West Coast: 8203-S 5. Avalon Blvd.,
Los Angeles 3, Cal.

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Co., Ltd., London, Ont.

BIG FOUR INDUSTRIES, INC.
P. O. Box 444N, Foster, Ohio

- ☐ Send FREE three Fact Folders on 14-Inch Tire Service.
- ☐ Send a van to show us your new Low Cost Power Tire Changer.

Signed _____

Store or Station Name _____

Street _____

City _____ State _____

Holley Carburetor Continued from page 129

manifold body of the engine.

Adjustments

To adjust the float level on the bench, remove the float bowl assembly. Invert the float bowl, then loosen the lock nut and screw sufficient to allow rotation. Turn the adjusting screw, until the base of the float is parallel with the floor

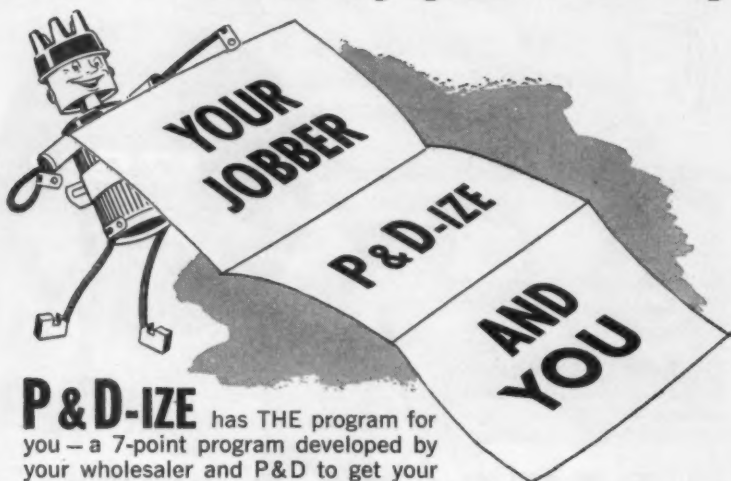
of the bowl. Adjustment is the same for both primary and secondary bowls.

Fuel level adjustment on the car: With the car on a level surface, the engine at normal operating temperature, remove the sight plug on the side of the float that you are working on. With the engine at idle speed, fuel should be visible, with the bottom of the

threads in the sight plug port.

If fuel should run out of the sight plug, it would indicate that the fuel level is too high. To correct the fuel level, loosen the lock nut, and turn the adjusting screw clockwise, to lower the fuel level, and counter clockwise to raise the

Make it a 3-way partnership



P&D-IZE has THE program for you—a 7-point program developed by your wholesaler and P&D to get your share of business—and PROFIT—for you from Ignition Tune-Ups—the most profitable part of all automotive service business.

With P&D and your automotive wholesaler backing you, you can't lose—the world's finest ignition parts and electrical equipment, trouble shooting manuals, clinics and schools, and the P&D-ize 12 months warranty. It's a deal you shouldn't miss! Get in touch with your P&D wholesaler for the complete P&D-ize Tune-Up Program or write us NOW!

There is a P&D cabinet and stock to fit all service station, garage and fleet owner needs.



P&D MANUFACTURING CO., INC.

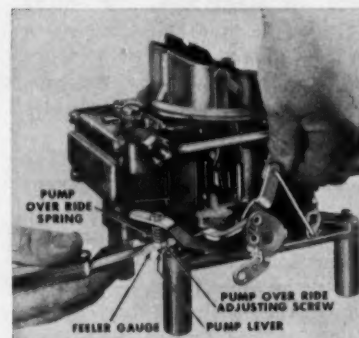
STARTING - LIGHTING - IGNITION

19-02 STEINWAY ST., LONG ISLAND CITY 5, N. Y.

Export Sales: Borg Warner International, 36 So. Wabash Ave., Chicago 3, Ill.



Adjusting fast idle speed, with screw on high step of cam: Adjust screw to open primary throttle plates—.014 to .016 inches.



Pump over ride spring adjustment with both throttles wide open: Hold pump down; then clearance should be .015 inches.

fuel level. $\frac{1}{16}$ of the adjusting screw equals approximately $\frac{1}{16}$ inches in fuel level.

Allow the engine to run for a few moments so that the level stabilizes in the bowl. Then check the proper fuel level. You are allowed plus or minus $\frac{1}{32}$ nd of an inch tolerance in the fuel level at the sight plug.

After satisfactory adjustment has been attained, lock the adjusting nut. Install the sight plug. This procedure can be used on both the primary and secondary fuel bowls, with externally adjusted float needle valves.

When adjusting the idle mixture screw start out by seating the screw lightly, and then backing out

Continued on page 154



Arvin

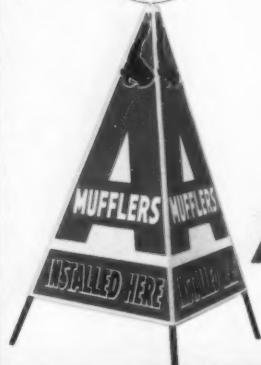
America's *most experienced* muffler manufacturer now puts you in a new position of strength for selling in today's highly-competitive after-market . . . with an all-new line of mufflers specifically planned for the replacement market.

Turn this page for the sales-making facts.





Check the facts! See why Arvin is your new Opportunity Line!



Arvin



an all-new line backed by the industry's greatest production experience . . . plus brand acceptance

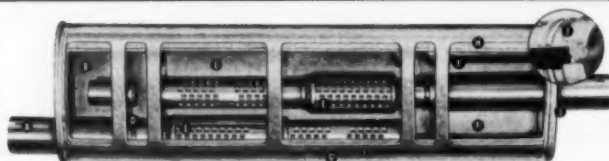
ARVIN
OFFERS THIS
NEW SALES
CONCEPT!

Arvin—a solidly-established, major manufacturer with the production experience of more than 115,000,000 mufflers, the production capacity of 8 automotive plants, the consumer acceptance of a nationally-known brand name—Arvin takes this important step forward to help you *get* the muffler business that *should* be yours . . . from prospects who are already your customers for other parts

and service! And Arvin has done it with a complete line of quality mufflers, competitively-priced and backed by complete promotion! Check the facts on the all-new, all *aluminized* or *zinc-coated steel* Arvin line, below. For further details on how you can gain extra sales, extra profit with the Arvin Muffler sign at *your* curb, write your jobber or direct to Arvin. No obligation. Write today!

- A Slotted connections for ease of service and leak prevention
- B Chamber and tuning tube to eliminate intermediate frequencies
- C Quadra-Crimped heavy duty heads Zinc-Coated

- D Aluminized or zinc-coated support or reversing baffles
- E Chambers for eliminating high level frequencies
- F Chamber and tuning tube to eliminate low level frequencies
- G Zinc-coated shell



- H Asbestos-wrapped for added life and better silencing
- I Enlarged view of lock-seamed head, shell, asbestos and outer wrap—an exclusive Arvin design feature
- J Nipple inlet and outlet seam-welded all around

ARVIN
OFFERS THESE
ADVANTAGES!

FULL LINE—Greater Inter-Changeability! The complete, new Arvin Muffler line gives you the greatest inter-changeability in the industry among models for 1940 to 1960 U.S. passenger cars and trucks! Your Arvin jobber maintains complete stocks at all times, so that he can handle your order for any particular muffler, pipe or accessory *promptly*—get the merchandise to you *fast* to help prevent “lost” sales.

PRE-SIZED TUBING—Faster, Easier Installation! All Arvin Mufflers come to you *pre-sized* for perfect muffler-to-pipe fit *without* crimping,

pinching or bending! And Arvin pipes come to you *Arvinized** with a special protective, anti-corroddent coating that keeps them factory-new in appearance, clean and easy to handle!

PRE-TUNED MUFFLERS—Perfect Performance! Every Arvin Muffler is pre-tuned for perfect performance on the make and model cars for which it is designed . . . without tuning or adjustment after installation! Your time per installation is reduced—and your customer is assured *maximum* sound suppression, *minimum* back-pressure!

AND ARVIN
GIVES YOU
ALL THIS, TOO!

LOCAL FOLLOW-THRU—Powerful National Advertising! Remember, too, that powerful Arvin national advertising tells-to-sell the Arvin story to prospects in *your* community . . . and free Arvin ad mats, radio announcements, curb signs, window

and wall banners and other promotion tools and plans help bring 'em to *your* place of business! Small wonder that the Big Swing is to Arvin . . . your *opportunity* line in mufflers! Check the advantages Arvin holds for you with your jobber today!



FREE!

You can get these famous Arvin Consumer Products as a gift! Free Gift Certificates for these products are packed at random into cartons of every bulk muffler shipment sent out from the Arvin plant. They're for you, the installer, exclusively!



Portable Radios



Outdoor Grills



Phonographs



Leisure Furniture



New Products

Continued from page 126

Hydraulic Lift

Will lift front or rear end of any car

Blackhawk Automotive Div.: A new one-end lift, designated SJ-26 is being marketed. It will lift



the front or rear end of any car—from smallest to largest—on the road, company states. Its new 6-inch diameter, wide-track wheels and 30-pound lighter body give it maneuverability. Lift height for the new model is 32 inches. Saddle spread is from 14 to 52 inches. A new, deepened throat reaches 14 inches under a car to grab inaccessible bumper brackets. Handle is removable for storage. Write: *Robert D. Barnard, Adv. Dept., Blackhawk Automotive Div., 5325 W. Rogers St., Milwaukee 46, Wis., or phone Orchard 1-4000.*

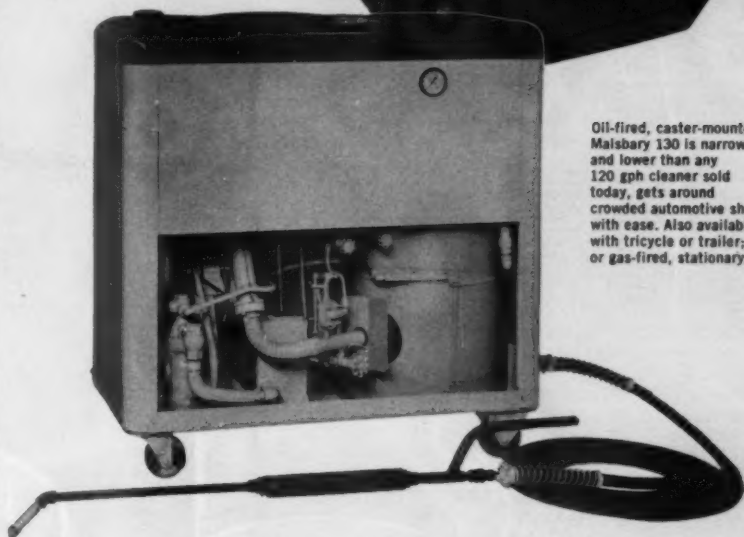
Cartridge Lamps

New 12.8 volt cartridge lamps
Tung-Sol Electric Inc.: Developed and registered as the 211 and 212, two new festoon-type cartridge lamps have been introduced. Similar to the fuse-clip-mounted festoon lamps used by many foreign car manufacturers, the company expects that new 211 and 212 will find their primary application in automotive dome and courtesy lights, radio and other panel lamps, and in license plate illumination.

Continued on page 148

*Get 20% more cleaning power
at no extra cost
from*

NEW Malsbary 130 STEAM CLEANER



Oil-fired, caster-mounted Malsbary 130 is narrower and lower than any 120 gph cleaner sold today, gets around crowded automotive shops with ease. Also available with tricycle or trailer; or gas-fired, stationary.

Here's a more powerful, more compact cleaner... built by Malsbary to sell at the same price as standard 120 gph cleaners. It offers you—

- 10 Extra Gallons Hourly
- 20% More Cleaning Efficiency
- New Nozzle Design

For the same money, you get 10 more gallons hourly—a full 130 gph...plus 20% more cleaning efficiency. This efficiency comes from a new orifice and nozzle design (exclusive with Malsbary), which releases more energy and produces larger droplets in a more uniform pattern. The stream hits dirt harder.

Like the famous Malsbary 110 (more than 9000 in use), the new 130 burns low or high grade fuels equally well, without smoke, soot or fumes. It has same large coils ($\frac{1}{2}$ " inside diameter) to reduce scaling.

Why wait? Call your Malsbary dealer now—ask about the 130 and special try-and-buy terms.

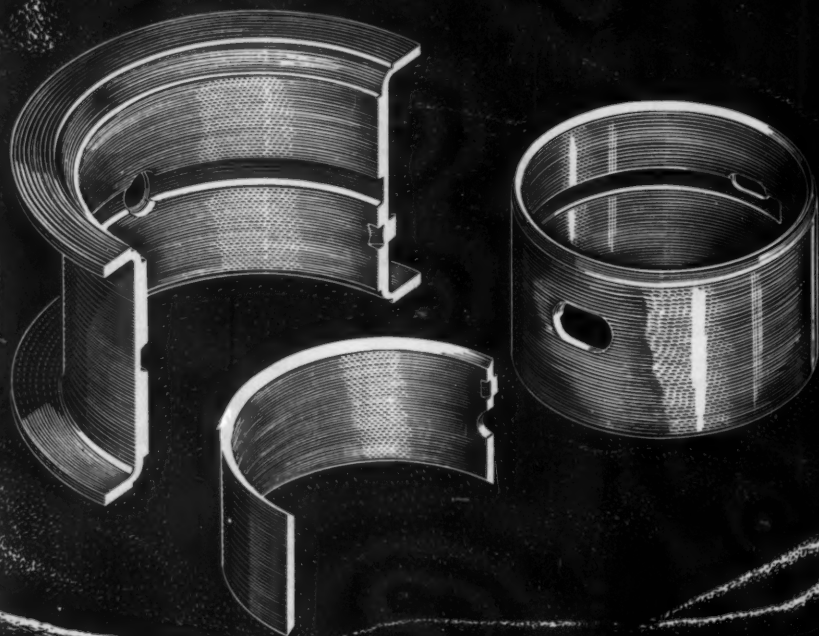
MALSBARY

Malsbary Manufacturing Co. • 845 92nd Ave., Oakland 3, Cal.



When the facts say why Aron is your new bearing...

Presenting...



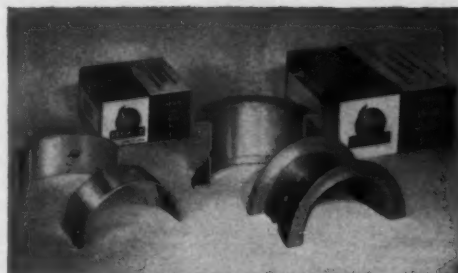
Cam Bearings
Connecting Rod Bearings
Main Bearings

A Complete Line of **ENGINE BEARINGS**

Used as Original Equipment - Accurate To .00025 Inch

When you supply Permite Preferred bearings to your customers, you are making friends—for these original equipment bearings fit perfectly, have a longer life expectancy, make money for the man who installs them and save money for owners of all types of vehicles.

Permite Preferred engine bearings are produced under the most modern conditions of quality control. Materials are accurately alloyed . . . multi-inspections guard against hidden flaws and surface imperfections . . . bearing walls must be accurate to .00025 of an inch. Aluminum Industries is an important supplier of bearings and engine parts for manufacturers of cars, trucks, buses and tractors throughout the country because of unfailing quality of Permite Preferred products.



NEW ALUMINUM-ON-STEEL BEARINGS FOR HIGH-COMPRESSION ENGINES OFFER A LIFE EXPECTANCY 2 TO 3 TIMES OTHER MATERIALS IN IDENTICAL ENGINES BY ACTUAL TEST!

20% cooler-running . . . carry loads up to 10,000 PSI on protected areas . . . show no wear after thousands of hours . . . endure rubbing speeds up to 20,000 FT/MIN . . . unaffected by oil additives or by-products of combustion . . . run on cast iron, forged steel or chrome plated shafts . . . completely interchangeable. More sizes and applications than any other line! "Get it from your jobber."

*The Most Complete Line of Engine and Chassis Parts Available
... Bearings, Pistons, Cylinder Sleeve Assemblies, Valves,
Water Pumps, King Bolt Sets, Tie Rod Ends, Drag Link Assemblies,
Spring Bolts and Shackles, Stabilizer Links, Ball Joints,
Front End Suspension Parts ... Today's Bigger and Better
Line of Permite Preferred Parts. Call Your Jobber!*

ALUMINUM INDUSTRIES, INC., Cincinnati 11, Ohio



Permite preferred

by vehicle manufacturers / car and truck dealers / garage and service men / fleet operators



KEM FUEL PUMPS

with the
Būnalon® Diaphragm

*Certified test results:

Continuous operation "1000 hours
at 80mph without breakdown —
equal to 4 times around the world"

Only Būnalon® — bonded
DuPont nylon and būna
rubber — will withstand
this punishment.

An exclusive feature of Kem
Fuel Pumps.



MA-4

Send for
FREE BOOKLET
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TESTING and
SPECIFICATIONS

My name
Firm name
Address
City State
☐ Gas station ☐ Repair shop ☐ Wholesaler
Your parts distributor

KEM



KEM Manufacturing Co., Inc.
Fair Lawn, New Jersey, U.S.A.



Leaders of Southeastern garagemen attending the Atlanta meeting of various convention planning committees included (l. to r.): W. R. Stone of Hapeville, Ga., first vice president of the IGO of Ga.; H. F. "Red" Reagin of Atlanta, president of IGOA; Bryan Davis of Raleigh, presi-

dent of the IGO of North Carolina; Lewis Allison of Greenville, president of the IGO of South Carolina; Harry Wright, president of the IGO of Greater Atlanta; Howard Viar of Savannah, president of the IGO of Georgia, and H. G. Stubblefield, president of the IGO of Nashville.

Circle of Safety Continued from page 37

ducted. Second, by cooperating in a Safety-Check program sponsored by the community.

Identify your place of business with this annual Vehicle Safety-Check. Use special displays during the months of May and June. Offer your customers a 10-point Vehicle Safety-Check. Alert the public by direct mail, personal calls, advertising and hand-out materials.

Remember, Safety-Checks Sell Service! Feature "Safety Specials" emphasizing lights, brakes, tires, and steering. Urge that all cars be serviced for safety at regular intervals. Call your customer's attention to the need for correcting unsafe items.

Here's how to cooperate in com-

Truck Brake Service

Continued from page 41

main fluid at low temperatures. If the improper fluid has entered the system, it will be necessary to drain the entire system. Then it should be thoroughly flushed with clean alcohol, preferably denatured, or a hydraulic brake cleaner.

Replace all rubber parts in the system, including the brake hoses. Refill the system with a recommended brake fluid of the right quality. On all brake relines, always check the axle shaft, oil seals and replace if necessary. Also pack the wheel bearings with the recommended type lubricant.

munity-wide Safety-Checks. Find out if your community plans to conduct a local Vehicle Safety-Check program. Then contact public officials, key civic leaders or other interested individuals and offer your help in organizing a community-wide Vehicle Safety-Check.

Planning materials available for community Safety-Checks can be obtained by writing to National Vehicle-Safety Check for Communities, 2000 K Street, N.W., Washington 6, D.C.

The Mechanic's Best Friend!

LIQUID WRENCH

**Loosens
Rusted Bolts
nuts, screws, "frozen" parts!**

The super-penetrating rust solvent that quickly loosens rust and corrosion.

YOUR JOBBER HAS IT!
RADIATOR SPECIALTY CO.
CHARLOTTE, N. C.

TELESCOPIC TOW BARS

**6
MODELS** for **ANY
size truck**

WELD BUILT Brooklyn 34, N.Y.
5903 Preston Court



**MONMOUTH
QUALITY...
IN ACTION!**

"Monmouth has always meant quality..."

... and in actual performance, this quality really pays off. I know because I always used them in my own stock cars that I raced. As a garage owner and engine rebuilder for 28 years, I've long since learned to rely on Monmouth for top quality engine bearings. What's more, my NAPA jobber makes it easy for me to use the best. He carries the complete line and gives me fast service."

If you want "performance that pays off," use Monmouth Bearings in all of your engine rebuilds. Your nearby NAPA jobber can give you instant service on your complete bearing requirements.

Low premium "customer insurance" is yours when you install...

MONMOUTH Engine Bearings

CLEVITE SERVICE: Cleveland Graphite Bronze • Division of Clevite Corporation • Cleveland 3, Ohio

says Clifton L. Taylor, independent garage owner of Delmar, Delaware



Simca Service Continued from page 45

bolts. What about cylinder heads?

Remove Cylinder Head

The Simca cylinder head is made of aluminum. For this reason you are **CAUTIONED** not to remove the head until it cools to room temperature. This prevents head distortion. Drain the cooling system by removing brass drain plug from

block. The plug is located behind distributor, below valve cover at rear of engine. Disconnect the battery ground strap at the cylinder head. Remove ground strap from battery. Remove air cleaner, carburetor and choke assembly.

On automatic choke assemblies, the carburetor choke heat tube must be removed as a unit with the carburetor. Now the plugs: Re-

move spark plugs. Disconnect heater hose and top radiator hose. Disconnect the exhaust pipe at manifold. Remove rocker cover, rocker arms and push rods. Place push rods in slotted tray on board. Rocker arm shaft is made in two sections, do not interchange these assemblies. Remove tappet cover plate. Extract tappets, and place in position on board with push rods.

Examine under side of all tappets for flaking or spalling. Check push rods for wear and straightness. Remove cylinder head, mounting bolts and washers. Remove cylinder head and gasket. Using valve spring compressor, compress springs and remove split keepers. After removing keepers, retainers and springs; remove valve stem safety rings. These rings should be renewed on every valve job. Clean cylinder head by soaking in a cleaning solution that will not harm aluminum castings.

If no cleaning solution is available use extreme care when cleaning carbon from cylinder head. Avoid the use of sharp tools or any tool that may groove the gasket surface of the soft aluminum head. Check the fit of the valve stem to guide.

Replace guides if excessively worn. Intake valve guides should project into the port .531 inches. Exhaust valve guides project into the port .295 inches. Reseat all steel seat inserts and valve faces to 45 degrees. Check seat concentric with valve guide center, by use of dial indicator. Spot in valves to seat lightly, to double check seating. Dress rocker arms at point of contact with valve stems.

Remove only enough metal to give a clean smooth curved surface. Valve refacing machines have adapters that perform this operation with great accuracy. Check valve springs on tester. Free length should be 1.968 inches minimum. Compressed length 1.54 inches at 48.5 pounds per square inch.

Reassembly Steps

Coat valve stems and seats with a light oil. Reassemble valves, springs, keepers, etc., into head. Remove carbon from tops of pistons, being careful not to remove carbon

Continued on page 154

What's up K-D's sleeve?



NEW RADIATOR SERVICE TOOL KIT. The most used tools in radiator service and repair. Contains a Radiator Cap Wrench, Top Tank Thermometer, Petcock Drain Tool, Hose Clamp Pliers. Durable plastic kit.



K-D 405
RADIATOR SERVICE TOOL KIT

\$8.25
Eastern Ready, higher West

OPERATION OF TOOLS IN NO. 405 KIT . . .



410 Radiator Cap Wrench. No more burned hands! Fits all standard sizes, plenty of leverage. Magnet holds tool to gas pump, car body, etc.



411 Radiator Top Tank Thermometer shows condition of thermostat, cooling system, merely by checking coolant.



412 Petcock Drain Tool. Prevents scalding while opening cooling system petcocks! Universal joint in handle.



429 Hose Clamp Pliers for all sizes wire clamps. New 4-position, automatic thumb latch. Jaws swivel thru 180°.

K-D TOOLS
Make Hard Jobs Easy

FREE CATALOG • K-D MFG. CO., LANCASTER, PA., U.S.A.

ZOOM your profits!

Be offering more than the next guy . . . in less time spent on the job and more accuracy in pinpointing troubles . . . your customer list and profits will grow and grow. Customer confidence in your work increases, your parts sales increase, the money in your pocket increases. Contact us for a free demonstration or for detailed literature.



DUMONT
EnginScope*

*Trade Mark

Automotive Test Equipment Division
ALLEN B. DUMONT LABORATORIES, INC.
744 Bloomfield Ave., Clifton, N. J.

Now! Faster Edging of AUTO GLASS

Plus 60% Increase
In Belt Life . . .

Sensational new Contact Rubber Roller Platen gives you longer belt life and faster edging without chipping.

**SOMACA 106-FB12RP WET
ABRASIVE BELT MACHINE with
New CONTACT RUBBER ROLLER
PLATEN**

Standard of the Glass Shop Trade
—Several Thousand in Operation

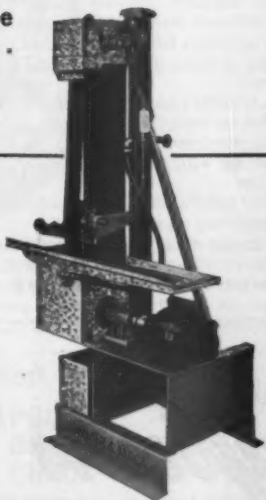
The flexing or fingering action of "cog-type" serrations in the rubber roller constantly present fresh, sharp cutting surfaces which gives the abrasive an aggressive cutting action. All this on the ever popular Somaca Wet Abrasive Belt Machines makes a combination hard to beat.

Write for full information and prices . . .

SOMMER & MACA Glass Machinery Co.

Automotive Department

5525 WEST OGDEN AVENUE, CHICAGO 50, ILLINOIS



SERVICE FOR YOUR B&D TOOLS



Check the Yellow Pages under "Tools-Electric" for the location of the nearest Black & Decker factory service branch or authorized service station.

Free tool inspection when requested • Genuine B&D parts used • Factory-trained technicians • Standard B&D Guarantee at completion of recommended repairs • Fast service, reasonable cost, always.

Or write for address of nearest of more than 50 branches and service stations to: THE BLACK & DECKER MFG. CO., Dept. 5104-S, Towson 4, Md.

Black & Decker



QUALITY TOOL SERVICE

FITZGERALD GASKETS

have the
answer

First with the best replacement gasket
Fitzgerald Fused-Aluminum Steel and As-
bestos Gasket with "built-in" SEALABILITY.



WITH
THE
ADDED
EXTRA

→ *Craftsmanship*

The Fitzgerald Manufacturing Co.

Torrington, Connecticut

Branches and Warehouses

Los Angeles, Calif.—Chicago, Ill.

Canadian FITZGERALD Ltd., Toronto, Canada

Boat Business

Continued from page 47

chandising job of boats, engines and automotive type trailers.

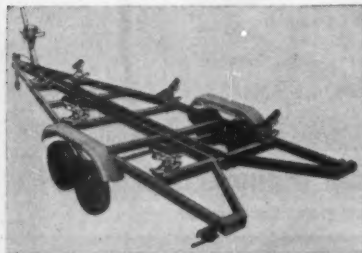
With the predictions of things to come, and the present trend upwards in the American economy and population, it would seem sound advice to consider your place in America's boating industry. To show the confidence that the leaders in this industry have in the future of the boating business, we report some statements by these men, forecasting for 1960. Laurence E. Nelson, Vice President, Marine and Industrial Engine Div., Chrysler Corp.; "Last year we predicted that 1959 would be an outstanding sales year for recreational power-boating. It is now apparent that this prediction has come true.

"We, at Chrysler Marine are highly pleased with the reception of our line by thousands of boat owners all over the world. We feel that the year 1960 will show an increase of between 28 and 32 per cent in consumer spending for recreational boating. We have a real confidence of the soundness of our entire economy for the next decade and feel that the year 1960, coming on the heels of a great 1959 sales year, will set the pace for a ten year period of outstanding prosperity."

Robert H. Scott, Director of Sales, Evinrude Motors; "We set all-time sales records in both dollar volume, and unit sales during the 1959 fiscal year, ended September 30th. The dollar volume of sales exceeded by 8 per cent the record established by the company in 1958. With a 50 per cent increase in horsepower in the leader of the Evinrude line, in 1960, the public interest in the firm's products have been greater than ever."

H. R. Nelson, Supervisor of Di-

Continued on next page



Above is a typical automotive hitch trailer with winch.



WHY?

Consider simply the brake lining. One type of lining, dry molded as an example, just cannot do a top-notch stopping job consistently for all cars.

For one thing, all cars don't have the same braking system. What's more, car weights vary tremendously. Heavy automobiles demand different friction materials than light ones. That's why Raybestos—and

WHY RANK RAYBESTOS LINED BRAKE SHOES ABOVE ALL THE REST? SEE FOR YOURSELF—MAKE THIS COMPARISON TEST!

	RAY- BESTOS	OTHER BRAND
1. Approved by the United States Government under lined brake shoe specification No. KKK-B-00640	X	
2. Produced by a basic manufacturer of brake shoes	X	
3. Produced by a basic manufacturer of bonding cements	X	
4. Produced by a basic manufacturer of brake lining	X	
5. Laboratory tested on dynamometers	X	
6. Proving ground tested on one of the largest company-owned test fleets	X	
7. Contain no drum-scoring wire backed linings	X	
8. Every shoe shear tested for bond strength	X	
9. Ground as follows to meet vehicle manufacturers' specifications: A. Standard Contour Ground B. Contour Anchor Ground C. Contour Cam Ground	X	
10. For use on power or manually operated brakes	X	
11. Linings made by 8 different manufacturing processes	X	
12. Right combination of linings for each make and model car	X	
13. Produced by supplier of brake lining to original equipment vehicle manufacturers	X	
14. Branded with manufacturer's name, number and shoe position	X	
15. Come in sealed boxes for customer protection	X	
16. Reduce your overall investment in cores	X	
17. Give maximum coverage with minimum inventory, yet have no improper substitutions	X	
18. Bonded under factory specifications and supervision	X	
19. Linings properly positioned on shoes	X	
20. Acclaimed the first choice of car owners and the trade in independent surveys	X	
21. Advertised to car owners in national magazines and network radio	X	

RELINE WITH
Raybestos
AMERICA'S BIGGEST SELLING FRICTION MATERIAL

RAYBESTOS DIVISION of Raybestos-Manhattan, Inc., BRIDGEPORT, CONN.

RAYBESTOS-MANHATTAN, INC., Brake Linings • Brake Blocks • Brake Fluid • Clutch Facings
Industrial Rubber • Mechanical Packings • Asbestos Textiles • Engineered Plastics • Sintered Metal
Products • Rubber Covered Equipment • Laundry Pads and Covers • Abrasive and Diamond Wheels
Industrial Adhesives • Bowling Balls





only Raybestos—makes 8 different types of linings and combines them in the precise combination in order to make certain that each car gets the best possible braking power.

With Raybestos Riveted or Bonded Contour Ground Lined Brake Shoes, you reline confidently, assured that your customers will have the brakes they need to stop swiftly and safely every time.

ANOTHER SALUTE TO RAYBESTOS QUALITY!



After exhaustive and coldly objective testing, Raybestos Brake Lining has been awarded the coveted *Motor Trend* Seal of Approval.



Powerful advertising and new curb sign—terrific together!



Raybestos' consistent, powerful consumer advertising — spearheaded by NBC-Radio's "Monitor"—gets action... sales action... by making motorists conscious of their need for top-quality Raybestos brake lining. And that's exactly where the new wind-tested curb sign fits in. With it in front of your station, car owners seeking a Raybestos dealer know you're their man. So put it up and make the most of our strong pre-selling campaign.

SURE WAY TO MAKE WHEELS OFF PAY OFF!



1. Pull front wheels, inspect linings
2. Check brake drums
3. Inspect front wheel bearings
4. Clean brake assembly
5. Check hydraulic system
6. Adjust brakes or recommend reline
7. Road test brakes

Corvair, Falcon, Valiant brake service featured in new Raybestos Clinic!

How to service brakes properly on the Corvair, Falcon, Valiant and other cars is highlighted in the brand-new motion-picture, "Total Brake Service with Raybestos." This sound color film shows brake work on 16 real cars—not brake models. See your Raybestos jobber about a showing.

rect Sales, West Bend Aluminum Co.; "In 1959, our company did 34 per cent more business volume than we did in 1958. We have set a projection of 25 per cent for 1960, over our '59 sales. At the present date we are ahead of schedule, and anticipate to make it easily throughout the year."

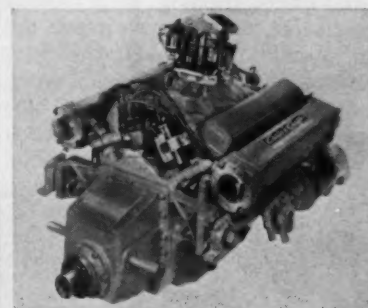


West Bend electric-starting "25" 2-cylinder, 37.71 cu. in. displ. rated at 25 horsepower at 4000 rpm.

Thomas B. Kalbfus, Director of Sales and Advertising, Johnson Motors; "Recreational boating will produce it's largest dollar sales volume in history during the 1960 season. This forecast is based on the generally healthy tone of the nation's economy, and a steadily mounting boom in family boating. In the first three months of the introduction of the 1960 Johnson line, dealers have placed 50 per cent more orders than they had at this time last year."

MOTOR AGE's editors attended the 50th National Motor Boat Show, held at New York's Coliseum. Ob-

Continued on page 144



Chris-Craft Model 283 V-8, 283 cu. in. displ. rated at 185 shaft hp.

Be sure
it's
a genuine
CHANNELLOCK



**ASK YOUR
TOOL SUPPLIER
FOR
CHANNELLOCK
PLIERS—
BY NAME**

Why? Only Channellock has undercut, interlocking channels that can't slip no matter how tight you grip. Only Channellock has the reinforced tension edge to withstand maximum gripping force. These exclusive features have made the Channellock No. 420 the first choice of skilled mechanics everywhere. So be sure you get only genuine Channellock pliers. Look for the trademark on the handle.



CHAMPION DEARMENT TOOL COMPANY
MEADVILLE, PENNSYLVANIA

NEW LITERATURE DESCRIPTIONS

Cord Line Catalog

*Offers line of replacement
and extension cords*

Belden Mfg. Co.: Publication of a catalog supplement A-60-S,



which contains information on Cord Reel Lights, Cordlites, accessory items, and extension cards, has been announced. The new Belden Cord Reel Light is available in two and three conductor sizes with neoprene jacket. The Cord Reel Light also features lengths of 20, 30, and 40 feet to give added selection. Write: Belden Mfg. Co., 415 S. Kilpatrick St., Chicago 80, Ill.

New AC Manual

*Explains new spark plug tester
and gives tune-up information*

AC Spark plug Division of GM:
Published recently is a new man-



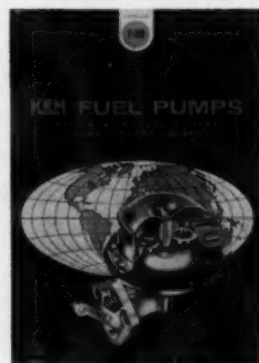
ual explaining operation of AC's new ACilloscope spark plug tester and giving instructions for engine tune-up work. The 44 page manual contains more than 100 illustrations and is divided into five sections. (1) Instructions for operating the ACilloscope; (2) Inspecting and servicing spark plugs; (3) Removing and replacing spark plugs, in-

cluding pictures and a discussion of the special problems encountered in some makes of automobiles; (4) Seven steps to minor tune-ups; (5) Special AC service aids and tools. Write: W. J. Oldfield, Dir. of Adv. & Promotion, AC Spark Plug Div. of GM Corp., 1300 N. Dort Highway, Flint 2, Mich., or Phone CE 4-5611.

Fuel Pump Catalog

*Lists fuel pumps, repair
kits, filters and parts*

Kem Mfg. Co.: New Catalog has just been published. It is an up-to-date listing of the company's

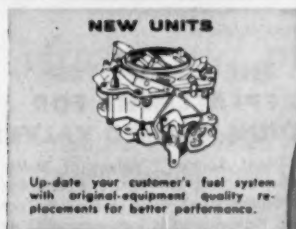


fuel pumps, repair kits, filters and parts for all makes of cars, trucks, marine and stationary engines. Catalog includes both alphabetical and numerical listings. A feature of this 3 color-catalog is a cross reference listing of interchangeable pump numbers designed to help prevent stock duplications. Write: Syd Frier, Adv Mgr., Kem Mfg. Co., Inc., 20-21 Wagaraw Road, Fair Lawn, N.J.

Solenoid Catalog

Units for most 1942-59 models
Arrow Armatures Company: A complete 1959-60 catalog of new and rebuilt Solenoids is now available. This catalog lists unit for most models from 1942 to 1959 including passenger cars; trucks, buses and taxis; and tractors. Write: Arrow Armatures Company, 15 Fordham Road, Boston 34, Mass.

QUICK, SATISFYING CARBURETOR SERVICE IS PROFITABLE SERVICE!



THAT'S WHY MORE AND MORE SHOPS DEPEND ON THE ROCHESTER-GM SERVICE LINE—THE LINE THAT'S GOING PLACES FAST!

Everything to facilitate carburetor service and selling has been included in the fast-moving Rochester-GM Service Line. More important—the materials and methods have been thoroughly tested by specialists so that you never have to "guess" or "make-do." You'll get customer-satisfying results—the one sure way to profitable service operation. Get into the Rochester-GM Service Line right away. Call your nearby UMS distributor. *Rochester Products Division of General Motors, Rochester, New York.*

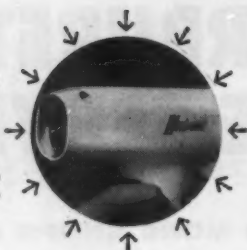
FREE CARBURETOR TRAINING is available across the country at GM Training Centers and UMS Distributor-sponsored Field Schools. Be a Rochester-GM Carburetor Specialist... you'll find it really pays! Write for details.



**America's
number one
original equipment
carburetors**

ROCHESTER CARBURETORS

here's
the
secret
to the



first and only
controlled output
timing light

new inside and out!



PATENT PENDING

HT-800
DEALER
NET

\$39⁹⁵

Only the completely new HT-800 Timing Light has a voltage matching switch that *locks out* the "wrong" voltage. *Nothing burns out!* The bright light through the optically ground lens flashes *only* when the battery voltage and switch position match—and it is equally as brilliant on 6 or 12 volt systems.

For twice the normal service life, the HT-800 also boasts an exclusive under-load circuit. For safety, it has a tough, high-impact plastic case that is electrically *non-conductive* . . . easy to keep clean and new-looking. It's your best buy for long-time, trouble-free service—backed by a one-year guarantee. Ask your jobber.

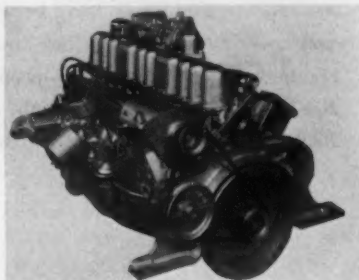
Herbrand Equipment

HERBRAND DIVISION
THE BINGHAM-HERBRAND CORPORATION
FREMONT, OHIO

Boat Industry

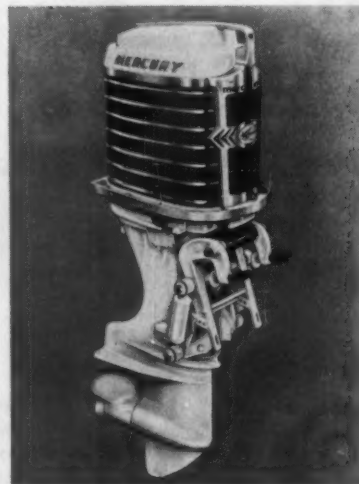
Continued from page 141

served were refinements in outboard motors, new styling, smaller, streamlined, compact engines, both for the inboard and the outboard. Introduction of "V" type inboards, showing a trend towards more power in compact packages were featured at the show. Diesels were much in evidence, and in the electrical systems, the alternators to provide the required high capacity outputs for running lights, ship to shore radio, etc.



Dearborn Golden Interceptor 6-cyl. eng. rated at 85 hp. at 4000 rpm.

Also there were several other innovations, one of which is the depth finding equipment, it was demonstrated. Fresh water evaporators that used the waste heat from the exhaust of the Diesel engine. All of the engines showed an increase in the usage of aluminum and heavy interest in increased efficiency, higher compression ratios, and improved carburetion.



Above is shown Mercury 80 hp. Merc 800, 6-cyl. with piston displacement of 76 cu. in. direct reversing.



RMC HEAT BAND
68,000 MILES

Note good condition of RMC Heat Band around margin and seat even though surface of head is badly eroded.



NON-BANDED VALVE
20,000 MILES

Note Failure caused by seat burning. Compare with illustration above. RMC Heat Band prevented this type of failure.

THE SOLID STEM REPLACEMENT FOR SODIUM COOLED VALVES

RMC "Heat Banded" Valves of Bi-Metal (2 piece) construction feature Stellite faces with a Stellite band fused around the outer edge of their Austenitic (non-magnetic) steel heads. Stem diameters match sodium cooled Valves—need no special guides or installation—give maximum heat dissipation—will not seize or bind.

LOWER COST—BETTER PERFORMANCE

Heat Bands cost less than sodium Valves. Yet, they give greater performance. A trial will show you.

Actual results achieved in a Reo Gold Comet Engine by Goethals Automotive Supply, Inc., Grand Rapids, Michigan.

COMPLETE COVERAGE VALVE TRAIN PARTS

VALVE SPRINGS	ROTATOR VALVE
VALVE LOCKS	KITS
VALVE GUIDES	VALVE SPRING
VALVE SEATS	INSERTS

Warehoused in all principal cities. Sold by leading Replacement Parts Wholesalers everywhere.

FOR INFORMATION WRITE TO
RICH MANUFACTURING CORP.
BATTLE CREEK, MICH.

EVERY CAR NEEDS IT EVERY 5000 MILES!



Made and priced for fast sales...easy profits!

Another Fram First! Here is the first and only nylon gasoline filter! It's now installed as original equipment on many 1960 cars.

Installed in minutes—right in the line, between fuel pump and carburetor.

No brackets, bolts or screws. Tubing and clamps with each filter make installation fast and easy. And replacement after 5,000 miles is easier still!

Perfect extra tune-up sale! Protects against carburetor wear and plugging to insure gas savings, instant starts, full power, no stalling! Each sale pays big profits! Order from your supplier today!



Order this colorful counter display! Includes six Fram Nylon Gas Filters packaged with tubing and clamps! Handy tube cutter also available. FRAM CORPORATION, Providence 16, R. I.

Clearing House. . .

Continued from page 96

runs quiet on the hydraulic valves, but could this be due to the lifters causing the valves to be too tight.

George E. Taylor
Schuyler Lake, N.Y.

IF the lifters were holding the valves off their seats, the compression would be low, and

the engine should turn over faster. Usually when this trouble is encountered it is due to a faulty starter, or a poor ground. Also make sure the ignition timing is set correctly.

Annoying Thud Is Heard When Car Goes Over Road

A persistent "humping" (the effect of hitting bottom) has been a complaint of three of our regular customers. The action comes

from the rear of the automobile and travels through the car giving a very apparent thud. This occurs when the car encounters a slight dip or raised area in the road. It doesn't matter how fast or slow the car is traveling. Our first thought was of course, bad shock absorbers. They were checked and doubled checked when the condition persisted. It is not the shocks. Next we checked the springs, repacked them, still no good. Spleens were filed. Still no good.

The car is a 1957 Ford Custom 300 V-8 automatic transmission. Incidentally, one of our customers advises that he had an extra leaf added to his springs, but this did not help the situation. Can you help me?

Michael E. Terita
Bridgeport, Conn.

THIS particular thud, which is annoying is happening on a lot of these jobs. From what we gather the noise comes from the drive shaft, where it slides into the transmission tail shaft. Some mechanics have been drilling a hole in the washer on the end of the joint. Others pack the spline with special compounds and some have installed a small coil spring inside the joint for a cushion. I have seen caster shims installed at the rear spring "U" bolts. All these remedies have worked out on some cars. I would suggest considering them in your case.

\$300⁰⁰
A WEEK
SERVICING
RADIATORS!

Inland Trained
Radiator
Repairman
Soldering
Radiator



"Our INLAND Radiator Department brings us an average of \$300.00 A WEEK! (Over \$15,000 a year!) Wish we had installed the equipment years ago!" — Douthitt-Carroll-San Chez Co., Memphis, Tenn.

\$10,000 to \$20,000 A YEAR ADDITIONAL VOLUME IS COMMON: "\$16,750 in one year!" — McRill's Auto Repair & Radiator Service, Twin Falls, Idaho. "\$18,000 in one year!" — Clough Auto Parts, Storm Lake, Iowa. Radiator servicing brings more profit per sq. ft. than any other activity in the service area!

20 to 30 MILLION RADIATORS NEED SERVICING YEARLY! Tests prove 83% of all radiators over a year old are partly plugged. Inland equipment shows customer his radiator needs cleaning — is designed for fast easy production methods — stays neat and clean.

Inland, world's largest radiator equipment manufacturer, offers the complete package — Equipment, Training, Merchandising, "Pays-For-Itself" Purchase Plan.

FREE BOOK!

New free 48-page book "Blueprint for Profits" shows equipment, training course, "Pays-for-itself" purchase plan and experiences of other operators. Take a minute and mail the coupon now.

INLAND MFG. CO.

1108 Jackson St.

Dept. MA-4, Omaha 2, Nebr.

MAIL COUPON TODAY

INLAND MFG. CO., Dept. MA-4
1108 Jackson St., Omaha 2, Nebr.
Please send now free book, "Blueprint for Profits."

FIRM _____ (PLEASE PRINT)

ADDRESS _____

CITY _____ ZONE _____ STATE _____

BY _____ TITLE _____

If dealer, make of car sold _____
Are you now operating a radiator Dept. ☐ Yes ☐ No

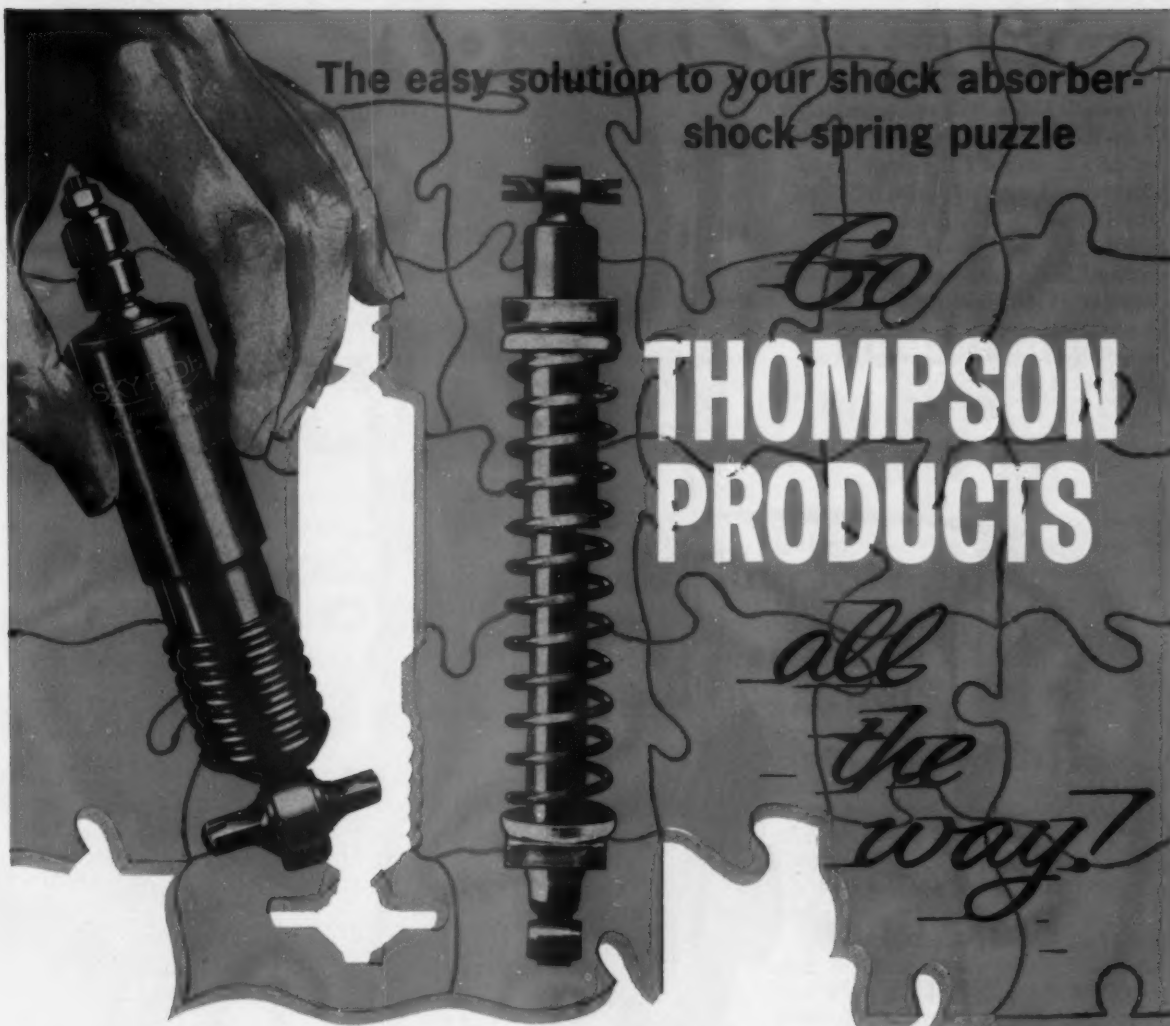
JONES
MOTORS
SEE OUR
ECONOMY CAR!

Smith
MOTOR CO.



"I had to do Something!"

The easy solution to your shock absorber-
shock spring puzzle



Thompson Products has solved the shock absorber-shock spring puzzle for you with the best low-cost package deal in the industry. You can now stock and install Sky-Ride shocks and shock springs as a compatible unit . . . or install the shock springs right over the car's existing shock absorbers.

Thompson Products Sky-Rides bring back new car riding comfort and steering ease. Sky-Ride's "Hydraulic Brain" construction includes:

- "Six-Way" Valving
- Sealed Piston Design
- Self-Adjusting Spring-Loaded Piston Rod Seal
- "Aeration-Free" Fluid Reserve Chamber
- Greater Oil Capacity
- Rubber Stone Shield
- All-Purpose Multi-Viscosity Fluid

Sky-Ride shock springs sold as a unit with Sky-Ride shocks cost less than competitive makes. When the spring only is installed over existing shocks—the price is but a small fraction of competitive units. What's more, Thompson Products shock springs are rubber coated for quiet operation and corrosion protection.

Talk to a Thompson Products jobber today! He's your direct line to full line chassis and front end parts.



Thompson Products

Replacement Division

Thompson Ramo Wooldridge Inc.
Cleveland 3, Ohio

Sold thru the world's finest jobbers

New Products

Continued from page 133

Types 211 and 212 are both 12.8 amp lamps having a light output of 15 CP and 6 CP respectively. Externally similar, each is rated for 100 hours of continuous operation, is one and three-quarter inches long and has a bulb diameter of .600 inch. For more information write: Tung-Sol Electric Inc., 1 Summer Ave., Newark 4, N.J.

POINT OF PURCHASE PROFIT MAKERS

Filter Merchandiser

Features nylon in-line fuel filter
Fram Corp.: A point-of-purchase merchandiser called the D-123 for its nylon in-line fuel filter has been announced. It consists of



one PG2-1PL fuel filter, four PG2-2PL filters, and one PG2-3PL filter. Also available is a tube cutter to assist dealers in the installation of the new nylon in-line fuel filters. The new type filters are now used as original equipment by car manufacturers. Write: Fram Corp., Providence, R.I.

Muffler Sign

Designed to mount three ways
Maremont Muffler Division: All new 39 inch sign with all the fea-



tures of the division's four-color, giant seven foot curb sign is available. The sign has been designed to mount three ways: (1) as a driveway sign set in a "no-tip" stand, (2) as a wall sign bolted to any wall, inside or out, and (3) as a pole sign which mounts perpendicular to any pole. Write: Maremont Muffler Division, 168 No. Michigan Ave., Chicago 1, Ill.

Automotive Lamp Display

Consolidates lamp stocks and provides inventory control

Union Carbide Consumer Prod-
Continued on page 150

"The most profitable
2 square feet
in my shop...
Thanks to my
Van Norman
301 Re-Tru
Brake Drum
Lathe."

*—says a Hartford
brake specialist*



The Van Norman 301 Re-Tru brake drum lathe requires less than 2 square feet of floor space. Performance-wise it features automatic feed, automatic shut off, waist level mounting, and handy controls for positioning the tool bar into the drum. And the tool bar swings out of the way for maximum clearance.

Exclusive Van Norman built-in drum micrometer standard equipment—no extra cost. You always know immediately the diameter of the drum before and after precision machining.

The sturdy Van Norman 301... designed to increase your profits... is a product of a company which has been a precision builder of automatic service machines since 1888. Get detailed information on the 301 Re-Tru today.

Remember, it pays to Van Normanize.

VAN NORMAN
Springfield 7, Massachusetts
A DIVISION OF VAN NORMAN INDUSTRIES, INC.



2295

SEALED POWER *Stainless Steel.*

OIL RING

does things
no other ring
can do!*

- * Stainless steel resists corrosion—makes it virtually impossible for troublesome sludge and carbon deposits to build up. Overcomes oil ring plugging—oil pumping.
- * Stainless steel maintains its original, built-in tension indefinitely so it delivers better oil control far longer than ordinary rings.

The unusually high number of spring tension points assures positive oil control even in tapered and out-of-round bores.

Side seals in piston groove—stops oil going around in back of ring—eliminates smoking.

End-abutment design produces ring tension independently of contact with bottom of piston groove—eliminates groove depth problems.

Chrome-plated, factory-seated side rails give instant oil control. Sealed Power Corporation, Muskegon, Michigan.



Sealed Power KROMEX PISTON RING SETS

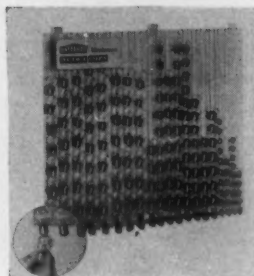
PISTONS • • PINS • • SLEEVES AND
SLEEVE ASSEMBLIES • • VALVES
• • WATER PUMPS • • TAPPETS

STAINLESS STEEL OIL RING
U. S. PAT. NO. 2,789,872

Point of Purchase

Continued from page 148

ucts Co., Div. of Union Carbide Corp.: A new display rack for "Eveready" automotive lamps is now being offered. The new Clear-View lamp dispenser is a compact clear-plastic wall unit which holds 300 miniature automotive and marine type lamps in automatic gravity-feed tubes.



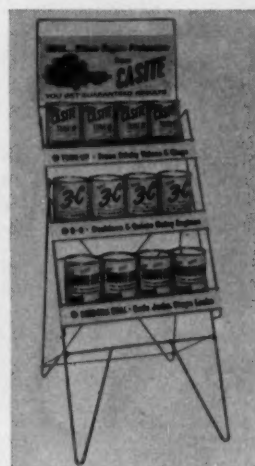
Designed to consolidate lamp stocks and provide easy inven-

tory control, the 20 x 17 x 2 inch displayer is dust and dirt-proof, states company. Write: Union Carbide Consumer Prod. Co., Div. of Union Carbide Corp., 30 E. 42nd St., New York 17, N.Y.

Island Merchandiser

Sales making aid to sell engine additives

Hastings Mfg. Co., Casite Div.: A specially-designed island mer-



chandiser which holds a ready working stock of improved Casite Tune-Up, 3-C crankcase concentrate and Smooth-Seal for automatic transmissions is featured. The merchandiser is light weight and can quickly be carried inside the station for overnight storage. It is available at no charge with the purchase of Casite engine additives. Write: Hasting Mfg. Co., Casite Div., Hanover St., Hastings, Mich.

TOP NOTCH PERFORMANCE!

It takes a quality product to satisfy every customer . . . and a complete line to service every job. L & S offers both! That's why top notch sales performance is guaranteed . . . and product performance, too. Replace with L & S Bearings . . . the QUALITY line . . . the PROFIT line!

L&S BEARINGS

L & S BEARING CO. P. O. BOX 995 OKLAHOMA CITY, OKLAHOMA

Match-Rite Assortment

Lets mechanic select distributor leads by sight

Standard Motor Products, Inc.: A wall chart containing actual-size illustrations of each number in the assortment is the chief feature of company's new "Match-Rite" distributor lead service stock. The chart enables the mechanic to select replacement leads without referring to a catalog page to find the right

number. He simply matches the worn lead against the illustra-



tions on the chart. The new service stock, designated DLA-3, contains 107 leads and covers all Delco-Remy, Autolite and Ford distributors. It comes in a sturdy display merchandiser which can be wall-mounted to save space. Write: *Standard Motor Products, Inc.*, 37-18 Northern Blvd., L.I.C. 1, N.Y.

Car Care Display

Displays waxes, polishes and washes

Union Carbide Consumer Products Co., Div. of Union Carbide players for "Prestone" Car Care Products are now offered. Largest of the new displays is the Americana Market Center. It is a car care department offering 12 square feet of visible shelf space in an all-steel floor unit



requiring only a fraction more than 4 square feet of floor space. It displays polishes, waxes, metal protectors and cleaners. In fern green and natural wood

grain, the unit is styled to attract customer attention to a self-contained car care center. Write: *Union Carbide Consumer Prod. Co., Div. of Union Carbide Corp.*, 30 E. 42nd St., New York 17, N.Y.

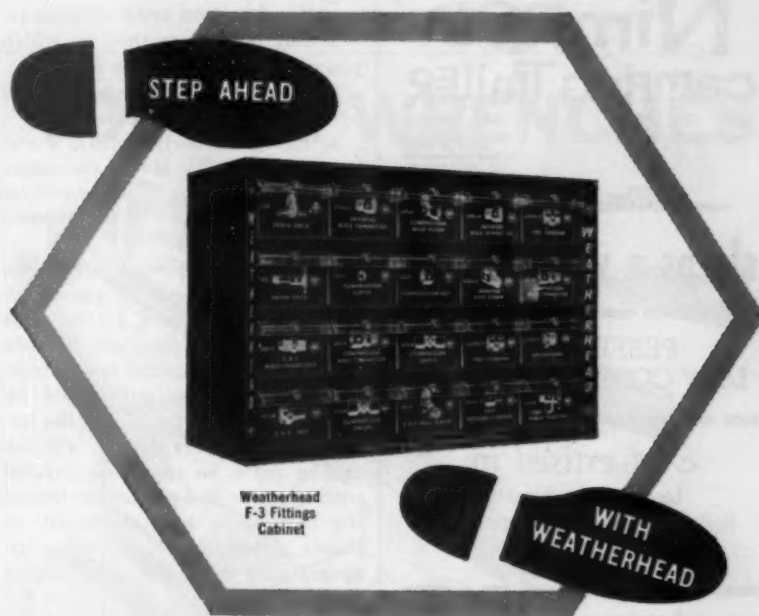
Display Stand

Displays automobile seat cover miniatures

Volland Industries, Inc.: A display stand for in-store display of automobile seat cover miniatures

is available. The stand holds either six 16-inch miniature automobile seat mock-ups or four 22-inch miniatures which are not included with the stand. The stand itself is 45 inches high and the same in width. Rising above this is an oval shaped sign bearing the legend, "Good Car-Keeper begins with beautiful seat covers!" Write: *Volland Industries Inc.*, 1711 Pratt Blvd., Chicago 26, Ill.

Continued on page 162



FREE! Fittings cabinet

when you buy \$25 worth of Weatherhead brass fittings

During Weatherhead's famous Cabinet Carnival you get this twenty-drawer stock cabinet free with every twenty-five dollar order—whether you take the standard assortment that fits most needs, or make up your own selection. Your Weatherhead jobber salesman will be

glad to help you choose. And you'll get more fittings for your money than ever before, as a result of our recent revolutionary price reduction announcement. There's no reduction in your profit margin, though—so get in on this Cabinet Carnival deal while jobber's stocks last.



WEATHERHEAD

the original equipment line

THE WEATHERHEAD COMPANY • Ft. Wayne Division
Dept. 608 • 128 West Washington Blvd. • Ft. Wayne, Indiana

DEALERS WANTED

*newest,
hottest item in camping*

50 Million Campers This Year

THE NimROD CAMPING TRAILER



sleeps a whole family

PERFECT FOR
LOW-COST VACATIONS

Advertised in

Look, Holiday, Parents,
National Geographic, Sports Afield,
Sunset, Sports Illustrated.

**aggressive
dealer promotion**



NIMROD Equipment Division,
Ward Manufacturing, Inc.
2550 Spring Grove Avenue, Cincinnati 14, Ohio
Please send complete dealer information on
Nimrod Camper.

Name _____

Address _____

City _____ Zone _____ State _____

Body Shop

Continued from page 51

ness that results in almost a complete absence of conventional body noises and rattles.

3. Body damage in a collision is seldom extensive because of the shock absorbing ability of this type structure.

4. Insurance companies have recognized them as good risks, resulting in some cases of a 10 per cent reduction in premium costs.

The normal steps to take in attempting to make repairs are as follows:

1. Examine the vehicle thoroughly to pinpoint areas of damage.

2. Lay out the method by which you expect to correct the damage.

3. Assemble the correct tools and hydraulic aligning fixtures.

4. Using the correct metal working techniques, rough out the major damage and check work measurement as you proceed to the finished job.

Remember, body work and frame work can no longer be separated. Both are accomplished at the same time. The basic operations consists of pulling and aligning body structure metal. These pulls must be made along the same line as the impact damage. Very seldom will the pulling force be in a line parallel with the fore and aft center line of the car. Pulls and alignment of panels generally will be on a diagonal axis from the car's center line.

Most damage due to impact results in crumbling, wadding, buckling and distortion of the metal in the immediate area of the impact. In extremely rare cases is the impact damage carried beyond the middle part of the car. (Such as an impact on the right front fender at the head lamp.) Damage generally will be confined to the area such as fender, bumper, grille, inner fender well, wheel and suspension if hit low.

It would be extremely rare and possibly unheard of for the left rear door to be sprung out of alignment. So basically, the job consists of working a localized area.

The force of the impact compacted the body metal. All that remains is to pull the crumpled metal, and form it back into alignment.

FOR MECHANICS ONLY...

win a
complete workshop
enter

Herbrand Tools

INDIANAPOLIS

"500" 1960 CONTEST

easy to enter!

nothing to buy!

**SIMPLY GUESS
THE SPEED OF THE 1960
SPEEDWAY WINNER**

THREE BIG PRIZES

1. Mechanic's workshop—complete with tools.
2. 3-drawer roller cabinet, 5-drawer chest and 85 piece tool set.
3. Tool chest and 57 piece tool set.



To obtain an entry blank, ask your Herbrand jobber. And don't delay—in case of a tie the earliest postmark wins! To qualify, entries must be postmarked May 25 or earlier.

Each prize is a genuine Herbrand product—the official brand of the "500" mechanic. Since 1949, every winning mechanic at the Speedway has used and endorsed Herbrand Tools.

Herbrand Tools

HERBRAND DIVISION
THE BINGHAM-HERBRAND CORPORATION
FREMONT, OHIO

Convention Plans For IGO Meeting

Ira Saks of Cleveland, Ohio, long prominent in the service market, will be among the speakers to be heard by the approximately 500 persons expected for the fifth annual convention of the Independent Garage Owners of America in Atlanta July 7-9.

Saks is chairman of the board of Unit Service Exchange Co., Inc., Atlanta, and executive director of the Ignition Manufacturers Institute, Cleveland, Ohio. He organized Accurate Parts Mfg. Co. in 1923 and in 1953 sold both it and the ReNu Co., a subsidiary of Accurate, to the Maremont Automotive Products Co.

During World War II and during the Korean War he represented the industry on the industry advisory board of the War Production Board and the Office of Price Administration. He still represents the industry on the business and defense service administration advisory committee of the United States Department of Commerce. He is chairman of the Industry-Wide Excise Tax Committee.

H. F. "Red" Reagin of Atlanta, president of the IGOA, announced that Saks' subject would be "The Road for Garagemen." It will pinpoint the problems of garagemen as seen through the eyes of this veteran manufacturer and look ahead toward solutions.

Bryan G. Davis of Raleigh, president of the IGO of North Carolina, will tell the convention how that group sprang into being with 23 units in less than a year.

Garagemen, parts wholesalers and factory representatives from several Southeastern states attended a meeting in Atlanta last month at which further plans for the convention were discussed.

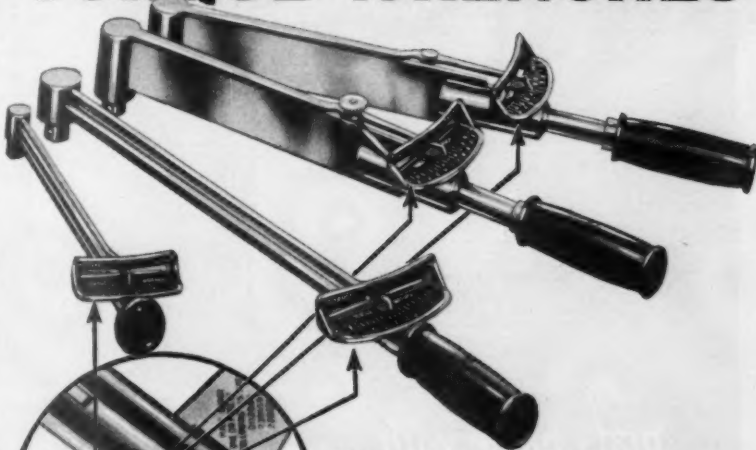
The first two days will be devoted primarily to sessions of the board of directors, with interruptions for a "genuine" Southern barbecue the first day and a tour the second day of the John Rogers engine-rebuilding plant.

A panel discussion of garagemen's problems is planned, among other things, for the last day, which will be concluded with a banquet. New officers will be installed that evening. The Dinkler Plaza is the convention hotel.



1960 Crown Imperial Limousine introduced recently by Chrysler Corporation is example of fine automotive craftsmanship. Styled and engineered by Chrysler, the coachwork, exterior and interior trim are hand-fashioned by Carrozzeria Ghia of Torino, Italy. The corporation said only 25 of the luxury models will be built now.

TORQUE WRENCHES



This Mark* Guarantees:

1. Guaranteed accurate forever within 2% of maximum scale reading.
2. The only wrench that permits accurate use of adapter & extensions. Patented pivoted handle permits concentrated load position—the only way to obtain accuracy with adapters and extensions.
3. Rugged construction. Can be stored in a tool box with other tools.
4. Easy to use as any socket wrench.
5. Signalling models and direct reading styles to choose from.
6. A complete range to choose from to meet every service application.

*These are the same torque wrenches used and recommended by leading engine and equipment builders.



Valve and Clutch Spring Tester

Test new as well as used springs. Match sets of valve springs for top engine performance. Check clutch springs to prolong clutch life.

FREE

Torque Specification Book for over 130 makes and more than 1200 models of automobiles, (U.S. and foreign) trucks, tractors, outboards, motorcycles, diesel, aircraft, marine and small air cooled engines. Spark plug, wheel bearing, valve spring data and many helpful torque tips sent free upon request, write Dept. 618



TORQUE WRENCH
ADDISON • ILLINOIS

Holley Carburetor Continued from page 130

one full turn. After the carburetor is installed, this screw can be adjusted finally for the conditions that exist in that particular engine. Turning the screw in or out, to increase or decrease the amount of the mixture feeding the idle current.

This model carburetor also has an idle air by-pass adjustment screw. The screw is located under

the air cleaner anchor screw hole or stud. The static adjustment on the idle air by-pass screw is to turn the idle air by-pass adjusting screw in until it seats lightly, and then back it off one full turn.

After the carburetor is on the engine and the engine is at normal operating temperature, additional adjustment will be required. Adjust in order to compensate for var-

iations in different engines and atmospheric conditions.

Service Hints

Always center the throttle plates in the bore to prevent excessive amount of air leakage. When installing fuel bowls, install the primary bowl first. Lubricate all O-ring seals to insure easy installation.

Check that all gaskets match the holes on the casting to which it is fitted. Make sure that the idle air by-pass screw is installed flush with the base of the main casting before assembling to the throttle body to prevent damage to the screw. Check that the choke plate drops wide open of its own free weight before installing the choke rod. Always check your catalog specification sheets before attempting any physical adjustments.

Simca Service

Continued from page 138

ridge at piston top and cylinder edge.

Disturbing this carbon accumulation without replacing piston rings could destroy its compression sealing effect on pistons and cylinders. Removing the carbon could result in excessive oil consumption after assembly.

New Head Gasket

Install new head gasket (free thickness .047 inches). The letters on head gasket "AV" should be facing up and towards the front of engine. Install cylinder head. Install mounting bolts and washers. Torque cylinder to 43 foot pounds of torque.

Installing Rocker Shafts

Install rocker shafts and arms. Set valves cold. Intake valves .004 inches—exhaust valves at .006 inches. Reassemble all other components, and fill the cooling system. Start engine and make final adjustments to carburetor and ignition timing.

Ignition timing set a 4 degrees B.T.D.C. Re-torque cylinder head and relash valves after 500 miles of operation.



FOUND!
a new standard of quality...

...mats made with **POLENE***

outdates all mats without this miracle material

**IT PAYS
TO CHECK
MOTOR
MOUNTS**

*Don't wait
'til it's too
late! Check
mounts with
every motor
or with each
transmission
repair job.*



Next time you order protector mats, insist on this new miracle material...the new standard of quality. It has a soft luster-sheen finish that adds sparkle to any car interior...new or old. A complete family of mats in five colors for front and rear floors.

*Trade Mark

360

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1725 London Road • Cleveland 12, Ohio
A DIVISION OF ANCHOR INDUSTRIES, INC.



STUCK VALVE CORE HEADACHES CURED FOREVER!

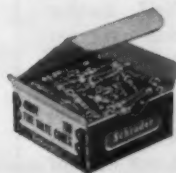
**New Swivel-T Valve Cores come out clean and easy...
yet assure the most positive air seal you've ever made**

If you haven't taken advantage of Schrader's Swivel-T Valve Cores, you're behind in important daily service. The Swivel-T can't stick! Makes the best seal! Won't ever break off in the valve stem! Assures faster, better service for customers and profits for you.

Look for the white washer. This Swivel-T sealing gasket is made of Teflon—slipperiest substance known. Its built-in lubrication makes it self-swivelling, and it shrugs off oil, water, acids. Temperatures from -100°F . to 500°F . don't faze it. Slides in and out like magic, leaving valve walls clean.

Yet Schrader Swivel-T Valve Cores cost no more than previous Schrader cores. Your supplier has them in stock. See for yourself how you can make a better seal.

*A duPont Trade Mark



Use **4000SV Bulk**
boxes of 100 in
the tire room

Display
4000MS
packages of five



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FIRST NAME IN TIRE VALVES

FOR ORIGINAL EQUIPMENT AND REPLACEMENT

Made in America to American Standards of Quality

Engineering Highlights



by Joseph Geschelin
Engineering Editor

Widespread interest in the use of aluminum in engines and other structural elements of vehicles abounds. Thus it has been enlightening to review a recent report prepared by J. H. Dunn and James M. Smith of the Aluminum Company of America. This report was based upon an extensive trip in Europe. There these specialists visited some of the major motor car producers.

One general impression was the European manufacturers are not ahead of practice in the USA either in product or process. On the other hand, European producers do have the advantage of more extensive use of aluminum; and a longer and more varied experience in its applications in motor cars. From this standpoint there is much that we can profit by.

For example, while current passenger cars in the USA employ about 56 pounds of aluminum per car, on the average, Fiat has about 100 to 120 pounds per car or about four per cent of car weight. Lancia has approximately 250 pounds of aluminum or around 11 per cent of car weight; while Citroen has about 200 pounds of aluminum. These are examples of smaller, lighter cars, employing aluminum to a large degree and thus gaining the maximum advantage from its use both in lower total car weight and correspondingly smaller powerplants.

As a general rule, European cars are low powered. Engines are required to operate at much higher speeds, capable of delivering maxi-

Continued on page 159

SAFE BRAKES PROTECT LIVES!



**For Safety's Sake...PULL A WHEEL
...Check the brake system!**

**If replacements are needed, use
Wagner® Lockheed
BRAKE PARTS... FLUID... LINING**

Do your part during May—National Vehicle Safety Check Month. Every time you put a car up on a lift, ask the owner to let you pull a wheel... check the brake system.

Don't guess. *Be sure* that the braking system—parts, fluid and lining—are in safe operating condition. You'll help save lives... and build profitable business, too.

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helps you develop more
profitable brake service business

Save time... save effort... "How-To-Do-It" Installation Instruction Sheets (another Wagner first) are included with Wagner Lined Brake Shoe Sets—and in each Wagner Lockheed Power Brake Repair Kit. The easy-to-read... easy-to-follow... service information, provided by Wagner, enables any mechanic to turn out better, safer brake jobs—profitably.

Wagner Lockheed POWER BRAKE REPAIR KITS—complete with "How-To-Do-It" Installation Instructions—are available for all makes of passenger cars, and popular light trucks. Depending upon the type of unit being serviced, you have a selection of single packaged units, major kits, minor kits, packing kits and poppet valve kits.

Wagner Lockheed LINED BRAKE SHOES—are contour ground. They are easy to install with a minimum of effort. Shoes come to you one axle set to a box, complete with Wagner originated "How-To-Do-It" Instruction Sheet and "thank you" Dash Tag. You have two choices to meet your needs: "WEB" Sets, for general use; or "WP" Sets, for high horsepower cars or trucks with or without automatic transmissions and power brakes.



with **Wagner Lockheed**
the Quality line--
you've got what it takes!

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MAIL COUPON NOW!**

FOR DETAILS—on all Wagner Lock-
heed Brake Service Products consult
your nearest Wagner Distributor, or
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6400 Plymouth Avenue, St. Louis 33, Missouri, U.S.A.
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Please send:

- ☐ FREE 1960 Catalog AU-1 covering complete line.
- ☐ Details on "How-To-Do-It" Program.
- ☐ Details on Franchised Dealer Program.
- ☐ Bulletin AU-1212A on TRADE HELPS.

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BRAKE SERVICE PRODUCTS

enable you to turn out better, safer brake jobs

The safety of your customers can depend upon the quality of the brake products you use. So, for safety's sake standardize on Wagner Lockheed Brake Parts... Brake Fluid... Brake Lining. You'll turn out better, safer brake service jobs—and you'll be time and money ahead.

The Wagner Lockheed line of brake service products is complete. It is of the highest quality,—reflecting Wagner's 36 years of experience as a leading manufacturer of brakes, brake parts and brake fluid for original equipment, and for service needs.

WAGNER LOCKHEED REPLACEMENT BRAKE PARTS—are manufactured by the same machinery—to the same specifications—as parts used for original equipment. Line includes master and wheel cylinders, repair kits, pistons, springs, washers, cups and hose for every make and model vehicle... Line also includes POWER BRAKE REPAIR KITS for all cars and popular trucks. Each kit is complete with "How-To-Do-It" Installation Instruction Sheet.

WAGNER LOCKHEED BRAKE FLUID is the world's largest selling brand. It is compounded of finest ingredients—chemically balanced to function efficiently under all driving conditions. Wagner Lockheed Heavy Duty Brake

Fluid surpasses S.A.E. specifications 70R1, meets state laws, conforms to Federal specifications, and is recommended for general use... No. 21-B Super Heavy Duty Fluid (*the finest on the market*) surpasses S.A.E. specifications 70R1 and 70R3, meets state laws, and conforms to Federal specifications.

WAGNER CoMaX BRAKE LINING—provides more miles of quick, safe, smooth stops... fewer brake adjustments... less drum wear. Uniform in density, composition and frictional quality. Available in sets, blocks, rolls, slabs, cut segments, and on brake shoes.

WAGNER LINED BRAKE SHOES—are supplied with lining contour ground. This feature saves you time. "How-To-Do-It" Installation Instructions—and a Dash Tag—are included with each set.

FOR DETAILS consult your regular supplier, or write us for 1960 Catalog AU-1.



ONE CALL GETS ALL

Your brake service needs from one source — your Distributor of Wagner Products.

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You will profit with Wagner's great SAFETY-CHECK PROMOTION

Millions of motorists will hear about
Wagner Lockheed Brake Products over



MONITOR

and

NEWS ON THE HOUR

Look at this Concentrated Schedule!

MONITOR NEWS

✓ 22 announcements each week-end... one on every single MONITOR Newscast!

April						
10	11	12	13	14	15	16
17	18	19	20	21	22	23
24	25	26	27	28	29	30

✓ NBC NEWS ON THE HOUR
85 announcements during these two weeks of NATIONAL VEHICLE SAFETY CHECK MONTH.

April						
10	11	12	13	14	15	16
17	18	19	20	21	22	23
24	25	26	27	28	29	30

NBC NEWS ON THE HOUR
39 announcements during these seven week days.

May						
1	2	3	4	5	6	7
8	9	10	11	12	13	14
15	16	17	18	19	20	21

and

Millions of car owners will see this
half page ad in the POST... April 30!

(Shown in greatly reduced size)

This great Wagner Lockheed Safety Check Promotion lets you tie in solidly with NATIONAL VEHICLE SAFETY CHECK MONTH in May.

It will build business for you. Check your stocks of Wagner Lockheed Brake Products. And... stock up in time to meet the demand for Wagner Lockheed parts, fluid and lining.

Ask your Wagner Distributor for details on how you can utilize and build more profits with this Wagner Program "SAFE BRAKES PROTECT LIVES."

Wagner Lockheed

the best known name in brake service

LOCKHEED BRAKE PARTS, FLUID, LINING and LINED SHOES • AIR HORNS • AIR BRAKES
TACHOGRAPHS • ELECTRIC MOTORS • TRANSFORMERS • INDUSTRIAL BRAKES
WAG-20



Engr. Highlights. . .

Continued from page 156

mum output for longer periods of time without giving trouble. Since Europeans must keep their cars running for many years, they expect to rebuild engines as a matter of course. Consequently, it is normal to employ separable and replaceable cylinder liners and sleeves. In the USA, on the other hand, engine designers are prone to shun cylinder liners because of their higher cost. That is one of the reasons for the lag in the adoption of all-aluminum engines. Also the reason for the attention that has been given to the utilization of high-silicon alloys for cylinder blocks and heads.

Due to the difficulties involved in the development of hypereutectic aluminum alloys, coupled with economic foundry procedures, there is considerable interest in the USA in the development of surface coatings of various kinds for the cylinder bores or, conversely, a finish for aluminum pistons suitable for operation in an aluminum cylinder bore. If a satisfactory solution can be found the adoption of all-aluminum engines will be greatly accelerated.

It is interesting to note the observers' comments that light weight means less in motor trucks in Europe than it does in the USA. This contrast is quite striking. Here we have made strenuous efforts to reduce the weight of motor trucks by the use of light-weight materials but have paid little attention to motor cars from this standpoint. By contrast European manufacturers have gone all the way with aluminum in motor cars but have paid little attention to light weight construction in trucks.

The Alcoa observers found that few European vehicle manufacturers produce their own pistons. Consequently, there is considerable specialization in pistons by independent concerns who do design work as well as testing; and jealously guard their position as piston suppliers. In general, piston design follows about the same practice as in the USA.

Many pistons are made in high-silicon alloy (18-25 per cent silicon), particularly for two-cycle en-

Continued on page 162



Gaining suburban customers full attention by a unique Kiddie Romper Room, is an idea of an aggressive Chevrolet dealer. Delaware County, Pa., dealer Dick Barone Chevrolet uses his Kiddie Romper Room to its fullest in presenting sales argument to the relaxed parents.

Chrysler Sales Appointment

Appointment of Charles P. Noonan as director-sales development, for Chrysler Corporation's sales divisions group was announced by E. C. Quinn, vice president-sales divisions. Prior to his new position, Mr. Noonan was director of the Company's Eastern marketing area.

As director of sales development, Mr. Noonan will be responsible for the company's sales training and sales development programs and dealer financing as well as new car and truck sales in Hawaii. He will also be responsible for coordinating activities of the company's area marketing directors.

At the same time Mr. Quinn announced appointment of J. E. Murphy to succeed Mr. Noonan as director-Eastern marketing area. Mr. Murphy has been director of the company's Central marketing area since August, 1959.

Calendar of Events

Apr. 16-24—International Auto Show, Coliseum, New York, N.Y.

Apr. 23—Automotive Wholesalers of Okla., annual convention, Skirvin Hotel, Oklahoma City, Okla.

May 1-3—Michigan Automotive Wholesalers Assn., annual Convention, Pantlind Hotel, Grand Rapids, Mich.

May 8-10—Fort Worth Auto Show, Will Rogers Exhibit Bldg., Fort Worth.

May 8-13—ASIA management institute, University of Illinois, Campaign.

May 20-22 New Jersey Automotive Jobbers Assn., convention, Colony Motel, Atlantic City, N.J.

May 22-27—ASIA sales management institute, Syracuse (N.Y.) University.

June 20-22—Pennsylvania Automotive Assn., Bedford Springs Hotel, Bedford, Pa.

July 7-9—Independent Garage Owners of America 5th Annual Convention, Dinkler Plaza Hotel, Atlanta, Ga.

July 21-23—Automotive Trade Assn. Managers summer meeting, Grand Hotel, Mackinac Island, Mich.

Oct. 10-12—Automotive Parts Rebuilders Assn., annual convention, Conrad Hilton Hotel, Chicago.

Oct. 15-23—National Automobile Show, Cobo Hall, Detroit.

Oct. 16-21—American Trucking Assn., annual convention, Waldorf Astoria Hotel, N.Y.C.

Oct. 23-25—New Jersey Automotive Trade Assn., Chalfonte-Haddon Hall, Atlantic City, N.J.

Oct. 28-Nov. 2—Florida Automobile Dealers Assn., Cruise to Montego Bay and Port-au-Prince.

AND NOW A FREE BOOKLET ON ROD RECONDITIONING FOR YOU

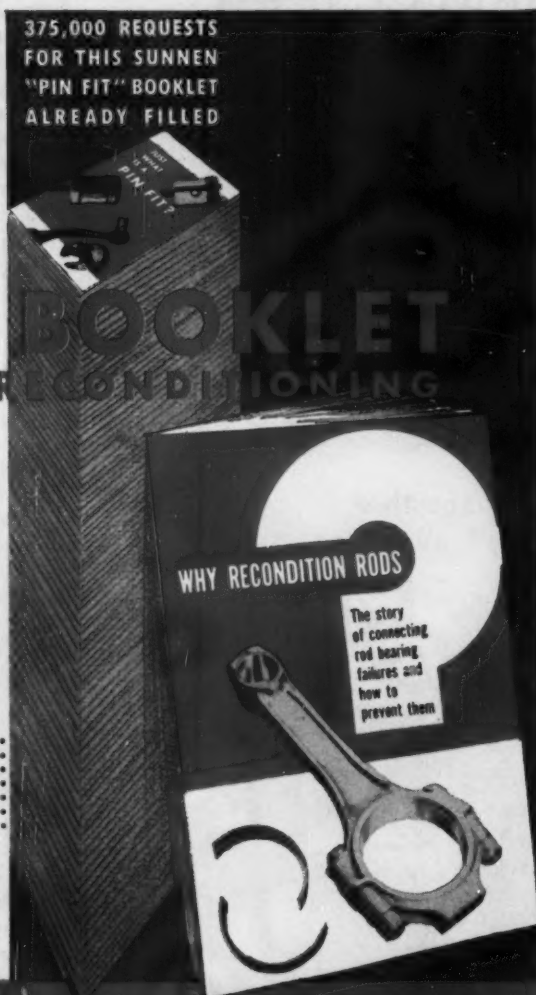
Full of valuable information for every mechanic and automotive machinist, this new booklet explains the causes of con-rod bearing failures and how to cure them — describes in detail the why and how of rod reconditioning.

- Handy, easy-to-read
- 24-page pocket-size,
- with many illustrations

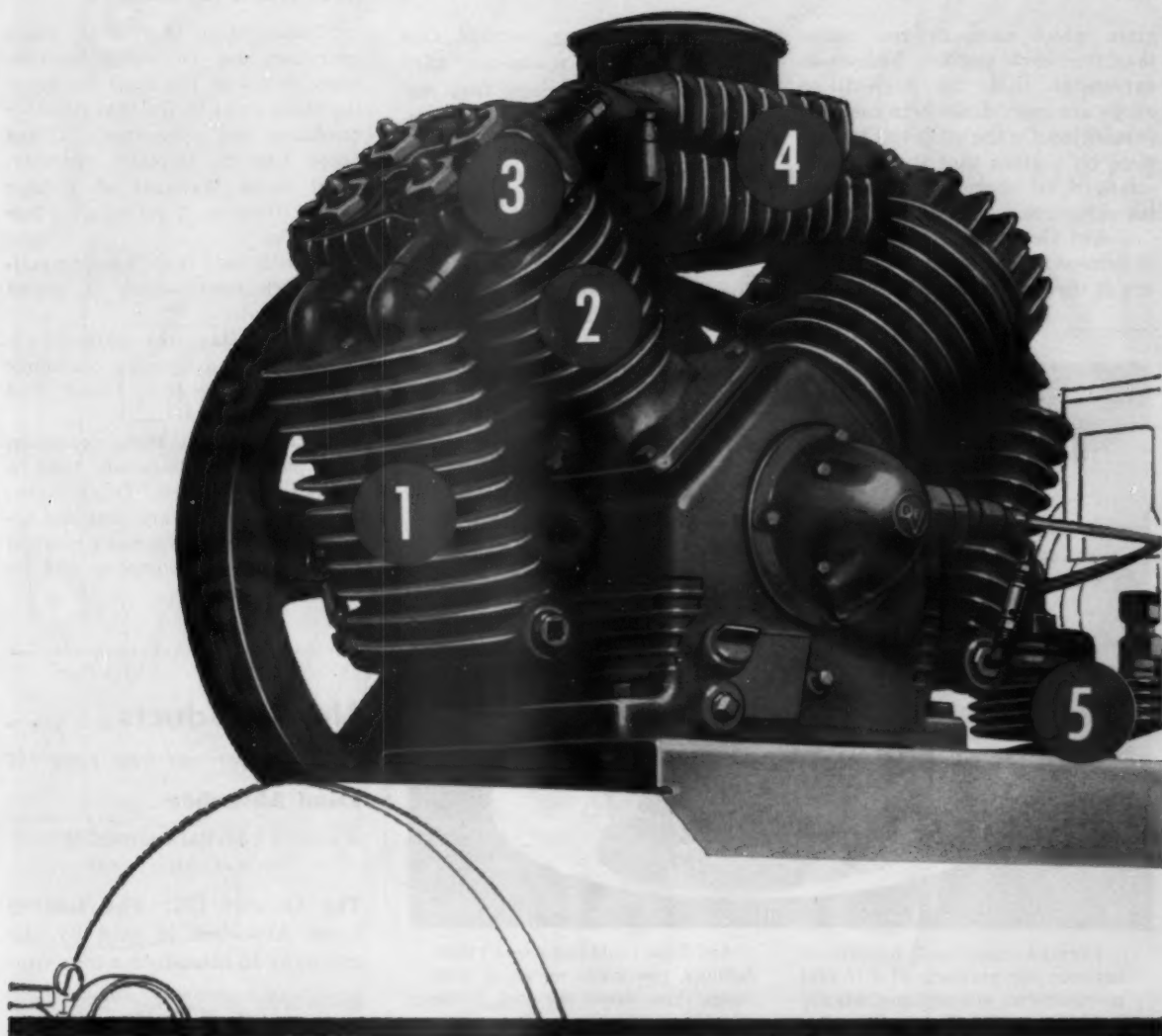
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"PIN FIT" BOOKLET
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SUNNEN PRODUCT COMPANY
Satisfied Customers Are The Foundation Of Our Business



Fewer stops and starts!

THANKS TO GREATER FINNED COOLING AREA DeVilbiss air compressors require fewer running cycles and actually help stretch your power dollars. Hot air, delivered into your tank, shrinks as it cools, requiring more frequent running cycles to keep tank full. Cool-running DeVilbiss compressors deliver low-temperature, less expanded air to your tank. Results: less shrinkage, fewer running cycles, significantly lower power costs.

Notice that the finned cast-iron intercooler(1) is extra large to permit a longer cooling cycle between compressing stages. Finned cylinder wall(2) and head(3) allow rapid, thorough heat dissipation. Finned aftercooler(4) lowers temperature of the air still further before it enters the tank. Even the check-valve manifold(5) is finned for maximum aftercooling. For complete details on efficient DeVilbiss air compressors, call your DeVilbiss jobber. The DeVilbiss Company, Toledo 1, Ohio.

FOR TOTAL SERVICE, CALL

DeVILBISS



Engineering Highlights Continued from page 159

gines which normally run hotter than four-cycle engines. There was agreement that the high-silicon alloys are more difficult to cast and to machine. On the other hand, there were indications that their fatigue strength is improved at elevated temperatures.

Use of aluminum engine bearings in Europe is quite widespread. Some are of the conventional 6-7 per cent

tin alloys, made as castings. One company is using a lead-zinc alloy and machining bearings from extruded stock. Another is using impact extrusions. Also used is a combination of the tin-aluminum bearing alloy bonded to a high strength aluminum-copper-alloy backing; as well as the tin-aluminum alloy binded to a steel back.



How do you recognize an "honest product"?

Even an expert can't distinguish between the products of different manufacturers without completely disassembling and testing each component. Because this is impractical, buyers have learned to accept a company's reputation as the most reliable guarantee of an "honest product".

However, if you actually did take apart an "honest product", you would find materials and components which meet specifications and engineering requirements in every respect.

If you could look into the way the product was made, you would see modern precision machinery, conveyerized lines, quality control stations and final operating tests of the finished product.

And if you could visit Arrow's three facilities, you would see all of these things. You would see that Arrow rebuilds automotive electrical components only and has developed specialized machines and methods for every important operation.

Arrow's specialization and standards of quality have earned our company a reputation for building honest value into every product.

For more information, write for Arrow's 12-page booklet, "How Good is a Rebuilt Generator?" This booklet is available without charge to jobbers and service shops east of the Mississippi River.



ARROW ARMATURES COMPANY
BOSTON 34, MASS., SPARTANBURG, S. C.

Ditzler's McNally Urges Neat Trucks for Good PR

"Today, more than ever, truck operators are becoming increasingly aware of the need for keeping their units in the best possible condition and appearance." Thus noted Lee H. McNally recently, Fleet Sales Manager of Ditzler Color Division, Pittsburgh Plate Glass Co.

McNally said that "keeping rolling stock neat-looking is sound public relations."

He said this was particularly true of fleets delivering consumer food products—like bread and milk.

He added that rust, corrosion and paint deterioration take a heavy toll in dollars. "Truck operators are highly aware that the appearance of a vehicle has a marked reflection on the company and its sales."

New Products

Continued from page 151

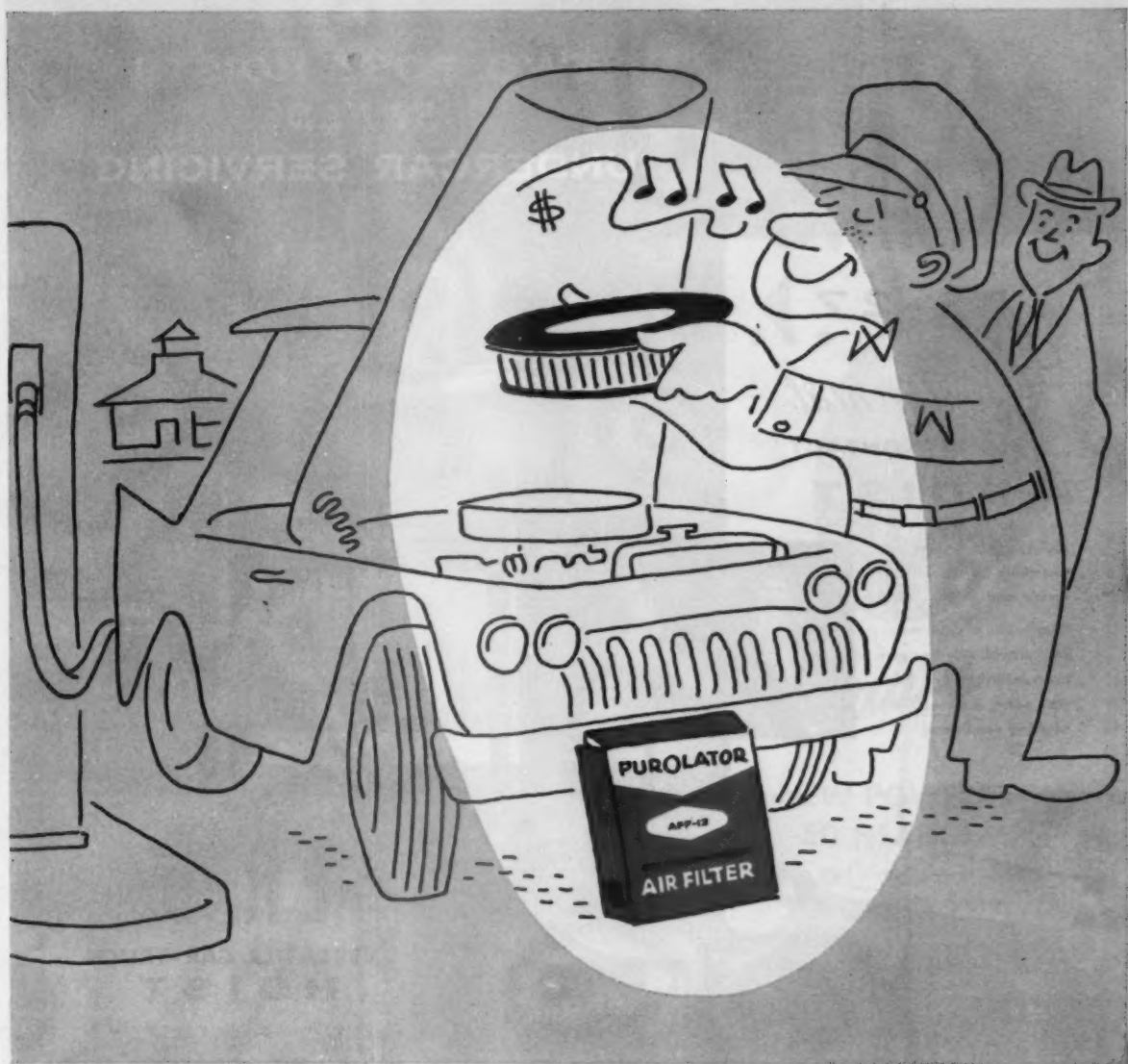
Load Absorber

Floats a hydraulic damping unit on a cushion of air

The Gabriel Co.: The Gabriel Load Absorber is said by the company to introduce a new con-



cept in suspension: air and oil. The Load-Absorber floats a hydraulic damping unit on a cushion of air. It is variable in that it automatically compensates for heavier loads without impairing the "luxury ride" under normal loads. And it is adjustable in that, if the need should ever arise, it can be inflated to handle extremely heavy loads, company states. Write: The Gabriel Co., 1148 Euclid Ave., Cleveland 15, Ohio.



**In the time it takes to fill a gas tank
Make an extra \$4.00 sale!**

In less than two minutes you can make a fast \$4.00 sale. That's all the time it takes to change a Purolator Air Filter! And you couldn't ask for better volume. Today, 15-million cars are equipped with dry-type air filters that are as

easy to change as the blade in your razor. So make filter changing a sales habit . . . it's easy with Purolator's FC-58 light tester! And be sure to stock Purolator. For quality . . . for profit . . . there's none better.

"Purolator" Reg. U. S. Pat. Off.

THE STANDARD EQUIPMENT LINE

PUROLATOR

OIL, AIR AND FUEL FILTERS

PUROLATOR PRODUCTS INC., RAHWAY, N. J.; TORONTO, ONTARIO, CANADA

GLOBE

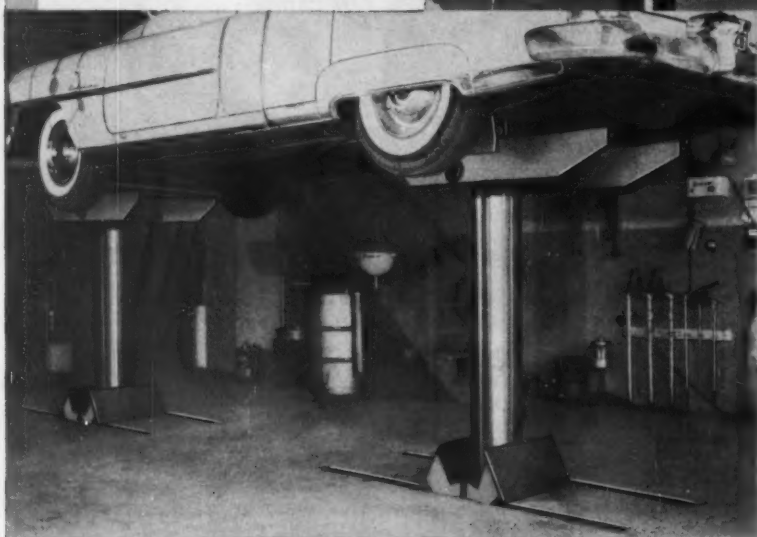
TWO POST HOISTS SPEED UNDERCAR SERVICING

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Continental "FRAME-KONTACT" HOIST

Quick, easy pre-set adjustments handle all domestic and foreign cars.

Elimination of cross member provides wide-open accessibility to every repair point, more efficient working conditions.



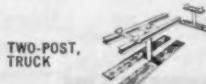
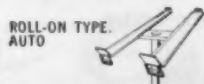
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VERSATILE CAR-TRUCK HOIST

Builds business volume and profits by servicing all vehicles.
Three models having lifting capacities from 10,000 to 18,000 lbs., accommodate wheel bases from 70 to 231".



For complete information on the full Globe Hoist line, write to Globe Hoist Company, East Mermaid Lane at Queen Street, Philadelphia 18, Penna.



THE WORLD'S MOST COMPLETE LINE OF AUTOMOTIVE AND HEAVY-DUTY TRUCK HOISTS!



GLOBE

THE BODY LIFT

HOISTS

PLANTS: PHILADELPHIA • LONG BEACH, CALIF.



SELLING SLANT OF THE MONTH!

MONEY-MAKING FACTS FOR DEALERS

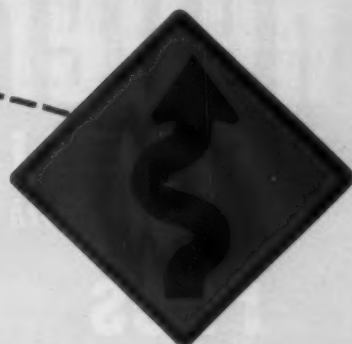
Help 41 Million Car Owners see **THE** Light...



Guide
T-3

**NEW POWERBEAM
50 HEADLAMPS**

help spot a road sign or obstacle
in time to avert an accident.



Sell Drivers on **SAFETY...** Stock *Guides* for greater Sales!

41 million drivers—driving cars with two headlamps—are assured greater nighttime driving safety, when you help them see the advantages of Guide T-3 Powerbeam 50 Headlamps.

Guide headlamps give two-headlamp cars more lower-beam throw... like the newest four-headlamp cars. Powerbeam 50's improved lower beam stretches a bright pattern of light 50 feet farther down the right side of the road, precisely where it's needed most.

And Guide T-3 Powerbeam 50 headlamps offer *more light... aimed right*. Three precision ground guide points on every Guide Headlamp insure perfect aiming... in minutes... day or night.

Tap this vast market for your extra profit. Stock up on Guide and you sell the headlamp that is original equipment on more new cars than any other brand.

**Call your
regular**



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AC SPARK PLUG  **THE ELECTRONICS DIVISION OF GENERAL MOTORS**

Chilton's MOTOR AGE • April 1960

**DESIGNED
TO DUPLICATE
THE ORIGINAL FINISH
OF ANY CAR!**

Heller VIXEN *Milled Curved Tooth* Files

FACTORY FINISHES COME NATURALLY when Heller VIXEN Files are used on body repair jobs ... because they're the same files the leading car makers use to produce paintable surfaces without extra filing or polishing.

THERE'S NOTHING FASTER THAN A VIXEN! Exclusive undercut teeth chew off metal in time-saving bites. The forward curve, deep gullets and wide pitch prevent loading and tracking.

SEE A DEMONSTRATION! Your nearby Heller distributor has Heller Vixen Files that fit any flat or curved body surface you'll ever repair. Make a date for a presentation today.



HELLER TOOL CO.



**America's Oldest File Manufacturer
NEWCOMERTOWN, OHIO**
Subsidiary of Simonds Saw and Steel Co.

Branch Offices and Warehouses: Newark, N. J. • Detroit • Chicago • Shreveport • Los Angeles • Portland, Oregon

BODY SHOP TIPS

Simple Way To Remove Dent In The Cowl Section Of Car

Whenever a door on a truck or an old model car swings open too far, because of a broken door check and dents the cowl section between the front end of the door and rear end of the front fender, it is usually very difficult to rough out the dent. A simple way to remove most of the damage is by opening the door all the way so the front edge of the door rests against the crease in the cowl. Then tack weld along the crease a couple of inches apart. The simple action of closing the door will remove most of the dent. The rest can be metal finished or leaded. *Eugene King, 2949 Elmwood Avenue, Kenmore 17, N.Y.*

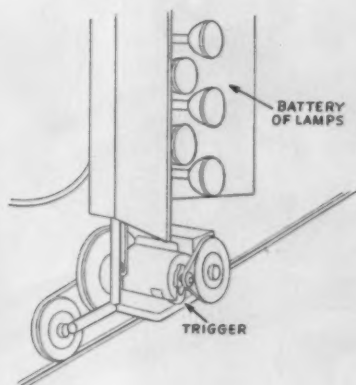
Preventing Liquid Adhesive Cans From Drying Out

Here's how to prevent liquid adhesive cans from drying out and building up around the threads of the can. Also from building up around the top which contains the brush. Simply apply a coating of silicone to these areas. *Boris Andrican, 1621—13th Avenue East, Hibbing, Minn.*

Moving Platform of Heat Lamps Used In Bake Oven

In our paint room the baking oven consists of a series of heat lamps mounted on a movable steel platform in the shape of an inverted U. This rides on tracks and spans the roof of the car.

The lamps at each side provide the heat as the unit moves alongside the painted car. We added a worn transmission unit so that the electric motor driving the battery of lamps can be left un-



attended. When the lamp carrying the unit has progressed the length of the room the trigger strikes a bolt in the track and this reverses the direction of the lamps. Similarly there's a bolt at the far end of the track to reverse the travel direction backwards. With this device a man is not needed to supervise the baking operation. *Harry J. Miller, 991 42nd St., Sarasota, Fla.*

Cleaning Paint Hose Leading From Tank to Gun

In painting operation where remote pressure tank equipment is used there exists a real problem in cleaning the paint hose leading from the tank to the gun. Failure to remove all paint residue from this hose will invari-

ably mean spoiled work sooner or later. The accumulation of paint grows thicker with each new layer allowed to dry inside, until the paint supply becomes inadequate or it peels out in small flakes. This either restricts, to some extent, or entirely stops the flow of paint. If by chance these flakes do go through the gun, they are deposited on the work. A simple recirculating tank may be used to clean this hose economically and quickly. We use a small gear type pump powered by a ¼ hp electric motor. A large juice can fitted with the proper hose fitting serves as our tank. We use approximately one pint of the thinning agent which is circulated through the paint hose for about 4 or 5 minutes while the gun is being disassembled in preparation for cleaning. At the end of this period the thinning agent is removed and stored for reuse. The tank is then filled with a mixture of 10 per cent liquid detergent, with water, which is circulated for 3 or 4 minutes and then discarded. The final step is the removal of the detergent with clean water. This procedure results in good work and trouble free equipment operation. *George A. Abner, No. 5 Woodcrest Dr., Alton, Ill.*

BODY SHOP TIPS are worth

\$7.50

If you've developed an Idea that has helped you to do body and fender work or painting better or faster, it may be worth money. Jot down the idea and, if necessary, make a rough sketch. Sometimes a snapshot will help. Just make the description of your BODY SHOP TIP clear, and if it is used, you'll receive a check for \$7.50.

5/8" OR 3/4" DRIVE at no extra cost

Just specify which Drive you want and it's yours at no extra cost . . . but only from Ingersoll-Rand. No other manufacturer offers you this choice in a tool that's the smallest, lightest, fastest and most powerful in its class on the market. Look for red ● ball extras like this before you buy.



**ELECTRIC
IMPACTOOL**
Size 8U-12
5/8" or 3/4" Drives



another red ● ball extra

- "Cuss-Proof" Socket Retainer. Sockets can't fall off accidentally, but strong, spring-held retainer can be easily replaced. Another I-R exclusive.
- Life Guard Commutator. Exclusive I-R construction prevents motor damage, permits easy dressing, provides long brush life.
- Sealed Nose Bumper. Exclusive I-R rubber guard seals out dirt in addition to protecting impact mechanism.
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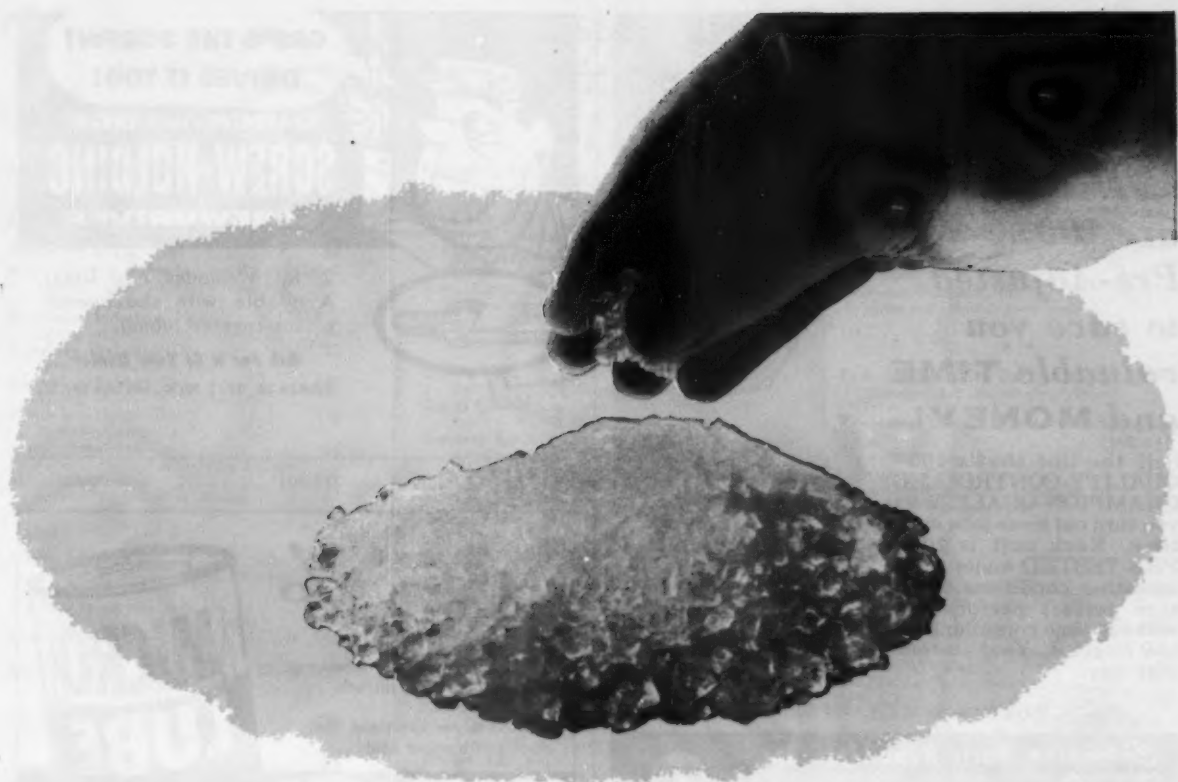
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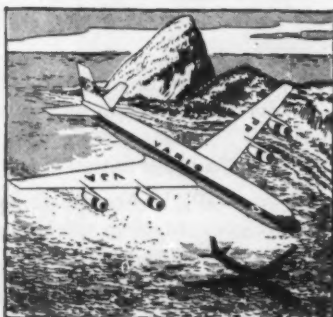
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ADVERTISERS' INDEX

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A	
AC Spark Plug (Electronics Div.)	18-19, 165
Agency—D. P. Brother & Co., Inc.	
AP Parts Corp.	20-21
Agency—Gray & Rogers Advg.	
Albertson & Co., Inc.	26
Allen Electric & Equipment Co.	8-9
Agency—Schoonmaker, Williams & Moss, Inc.	
Aluminum Industries, Inc.	134-135
Agency—Applegate Advertising, Inc.	
American Brakeblok Div.	22
Agency—Allman Co., Inc.	
Armstrong Rubber Co.	105
Agency—Lennen & Newell, Inc.	
Armstrong-Victor Div.	6
Agency—Buchen Co.	
Arrow Armatures Co.	162
Agency—Richard Thorndike Agency	
Arvin Industries	131-132
Agency—Ruben Advg. Agency	

B	
Belden Mfg. Co.	106
Agency—Frensholt Advg. Agency	
Big Four Industries, Inc.	129
Agency—Lewis W. Selmeier Co.	
Bingham-Herbrand Corp.	144, 152
Agency—Slayton-Racine, Inc.	
Black & Decker Mfg. Co.	97-98, 139
Agency—Van Sant, Dugdale & Co., Inc.	
Branick Mfg. Co.	170
Agency—Manufacturers' Advg. Agency	
Breeze Corp., Inc.	107
Agency—Adams & Keyes, Inc.	

C	
Carter Carburetor Div., ACF Industries, Inc.	125
Agency—D'Arcy Advertising Co.	
Champion De Arment Tool Co.	142
Agency—Davies & McKinney Advg.	
Champion Parts Rebuilders, Inc.	172
Agency—Stern, Walters & Simons, Inc.	
Champion Spark Plug Co.	24-25
Agency—J. Walter Thompson Co.	
Chevrolet Div., G. M. Corp.	16, 109
Agency—Campbell-Ewald Co.	
Chrysler Motors Corp., Mopar Div.	10-11
Agency—N. W. Ayer & Son, Inc.	
Classified Advertisements	172
Clevite Corp.	137
Agency—Duffy, McClure & Wilder, Inc.	
Commercial Credit Corp.	2
Agency—Van Sant, Dugdale & Co., Inc.	

D	
Delco-Moraine Div., G. M. Corp.	14
Agency—Campbell-Ewald Co.	
Delco Remy Div., G. M. Corp.	28-29
Agency—Campbell-Ewald Co.	
De Vilbiss Co.	161
Agency—Brooke, Smith, French, & Dorrance, Inc.	
Doan Mfg. Div.	154
Agency—William Glazer Advertising	
Dow Corning Corp.	126
Agency—Church & Guisewite Advg., Inc.	
Du Mont Laboratories, Inc., Allen B.	139
Agency—Lescarboura Advg., Inc.	
duPont de Nemours Co., Inc.	12-13
Agency—Batten, Barton, Durstine & Osborn, Inc.	

E	
Eis Automotive Corp.	169
Agency—The Furman Co., Inc.	
Electric Autolite Co.	94-95, 102-103
Agency—Grant Advg., Inc.	
Electric Storage Battery Co.	110-111
Agency—Batten, Barton, Durstine & Osborn, Inc.	

F	
Fitzgerald Mfg. Co.	139
Agency—Wilson, Haight, Welch & Grover, Inc.	
Ford Motor Co.	
Part & Service Div.	100-101
Ford Motorcraft Div.	113 thru 116
Agency—J. Walter Thompson Co.	
Fram Corp.	145
Agency—Marshall & Pratt	

G	
Gabriel Co.	104, 119
Agency—Gray & Rogers Advg.	
Gates Rubber Co.	3rd Cover
Agency—Harold Walter Clark, Inc.	
General Electric Co.	99
Agency—Batten, Barton, Durstine & Osborn, Inc.	
General Motors Corp.	23
Agency—Campbell-Ewald Co.	
Globe Hoist Co.	164
Agency—Eldridge Company	
Golden Glide Shock Absorbers Co.	17
Agency—Gray & Rogers Advg.	
Gray-Rock Div., Raybestos-Manhattan, Inc.	27
Agency—Gray & Rogers Advg.	
Guide Lamp Div.	165
Agency—D. P. Brother & Co., Inc.	

H	
Heli-Coil Corp.	170
Agency—O. S. Tyson & Co., Inc.	
Heller Tool Co.	166
Agency—Chambers, Wiswell, Shatuck, Clifford & McMillan, Inc.	
Homestead Valve Mfg. Co.	15
Agency—Van Sant, Dugdale & Co., Inc.	

I	
Ingersoll-Rand Co.	168
Agency—Beaumont, Heller & Sperling	
Inland Mfg. Co.	146
Agency—Buchanan-Thomas, Inc.	

K	
K-D Mfg. Co.	138
Agency—Andes Advg. Agency	
Kedman Co.	172
Agency—Gardiner Advg. Agency	
Kem Mfg. Co., Inc.	136
Agency—Conti Advg. Agency, Inc.	

L	
L & S Bearing Co.	150
Agency—Sargo Advg.	
Latham Time Recorder Co.	4
Agency—Barke Douling Adams, Inc.	
Lee Filter Corp.	112
Agency—The Furman Co., Inc.	
Lucas Electrical Services, Inc.	122
Agency—McCarthy Co.	

M	
Malsbary Mfg. Co.	133
Agency—Geo. C. McNitt Advg.	
Micro-Lube Sales	172
Agency—Grant Advertising, Inc.	
Mopar Div., Chrysler Motors Corp.	10-11
Agency—N. W. Ayer & Son, Inc.	

N	
Nimrod Equipment Co.	152
Agency—Farson, Huff & Northrich, Inc.	
Novi Sales & Service Co., Inc.	128

P	
P & D Mfg. Co.	130
Agency—La Porte & Austin, Inc.	
Pennzoll	123
Agency—Fuller, Smith & Ross, Inc.	

Perfect Circle Corp.	2nd Cover
Agency—Compton Advertising Agency	
Permatex Co., Inc.	3
Agency—G. M. Basford Co.	
Porter Co., Inc., H. K., Thermoid Div.	Back Cover
Agency—Erwin Wasey, Ruthrauff & Ryan, Inc.	
Purolator Products, Inc.	163, 170, 172
Agency—J. Walter Thompson Co.	

R	
Radiator Specialty Co.	136
Agency—Alan Advertising Agency, Inc.	
Raybestos Div., Raybestos-Manhattan, Inc.	140-141
Agency—Gray & Rogers Advg.	
Rich Mfg. Co.	144
Agency—Van Wart Associates, Inc.	
Rochester Products Div.	143
Agency—D. P. Brother & Co., Inc.	

S	
Schrader's Son, A.	155
Agency—G. M. Basford Co.	
Sealed Power Corp.	149
Agency—Roche, Richerd & Cleary, Inc.	
Sherwin-Williams Co.	171
Agency—Fuller, Smith & Ross, Inc.	
Skil Corp.	127
Agency—Fuller, Smith & Ross, Inc.	
Sommer & Maca Glass Machinery Co.	139
Agency—Merchandising Advertisers, Inc.	
Standard Motor Products, Inc.	1
Agency—Friend, Reis Advg., Inc.	
Stewart Warner Corp.	
Instrument Div.	124
Agency—MacFarland, Aveyard & Co.	
Sunnen Products Co.	160
Agency—H. George Bloch, Inc.	

T	
Texaco, Inc.	120-121
Agency—Cunningham & Walsh, Inc.	
Thermoid Div., H. K. Porter Co., Inc.	Back Cover
Agency—Erwin Wasey, Ruthrauff & Ryan, Inc.	
Thompson Products Co., Replacement Div.	147
Agency—Grinwald-Eshleman Co.	
Timken Roller Bearing Co.	108
Agency—Batten, Barton, Durstine & Osborn, Inc.	
Torque Wrench	153
Agency—Ross Lewellyn, Inc.	
Tung-Sol Electric, Inc., Automotive Products Div.	30
Agency—E. M. Freystadt Assoc., Inc.	

V	
Vaco Products, Inc.	170
Agency—O'Grady - Anderson - Gray Inc.	
Van Norman Machine Co.	148
Agency—O. S. Tyson & Co., Inc.	
Victor Mfg. & Gasket Co.	6
Agency—Buchen Co.	

W	
Wagner Electric Corp.	156 thru 159
Agency—Arthur R. Mogge, Inc.	
Wayne Pump Div., Symington Wayne Corp.	7, 117
Agency—Gray & Rogers Advg.	
Weatherhead Co.	151
Agency—Carlson & Co., Inc.	
Weld Built Body Co., Inc.	136, 172
Westinghouse Electric Corp.	118
Agency—McCann-Erickson, Inc.	
Wilkening Mfg. Co.	32
Agency—Aitkin-Kynett Co.	

THE LAST

LAFF



"By golly, boss. This sure beats coffee breaks!"

An admirer came up to a senatorial candidate after his speech and said: "Cengratulations. That was a fine speech. I like the straightforward way you dodged those issues."

Diner: "I want this meal put on the cuff."
Waiter: "I'll throw the whole thing in your lap if you want me to."

"I'm wondering what to buy for my wife's birthday."
"Ask her."
"Good Heavens, I couldn't afford that!"

"Is she a good cook?"
"She serves so many cocktails before dinner that nobody knows."



"Miss LaVerne is part of our incentive system!"

About the only thing worse than having your wife find a letter you forgot to mail, is having her find one you forgot to BURN.

It's a great pity that things weren't arranged so that an empty head, like an empty stomach, would not let its owner rest until he put something in it.

A professor commented about a student, "He's got a mind like a blotter. He soaks up everything, but he gets it all backwards."

Don't fear you'll be called a miser if you save money nowadays. You'll be called a miracle man instead.



"That Miss Snookums is awfully nice about leaving a tip!"

It's easy to explain what makes modern man tick. Some woman has given him the works.



"Before we put that up we couldn't keep the place clean."

**"We sell 360 belts a year
because we *CHECK* every *BELT*!"**



**says Alick Barlow, Owner
Barlow's Automotive Service
284 San Gabriel Blvd.
San Gabriel, California**

"I've been in the automotive business now for over 39 years. For 37 of those years I've handled Gates Belts, so I know first hand about their quality and dependable performance!

"I've found out also through the years that the minute or two it takes to check every belt, really pays off in satisfied customers and increased profits. We average 360 belt sales a year!"

"Gates sales tools, such as the Dial-Finder and Display Racks, make it quick and easy to find and install the right belt in a matter of minutes. For us, Gates V-Belts are a top-profit line."

Phone today for YOUR Gates V-Belt Display Rack. Supplied by your Gates Jobber, this attractive fixture puts the right belt for any popular make of car at your fingertips.



The Gates Rubber Company, Denver, Colorado



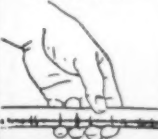
World's Largest Maker of V-Belts

**To find belt wear always
turn belt over — The underside
of the belt... not the top... tells
the true condition of the belt.**

**REPLACE BELTS
LIKE THESE:**



CRACKED



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GLAZED



PEELING



SPLIT



TPA 488

Gates Vulco V-Belts

Flashing Safety Light FREE!

with
each of
these great



COMPLETE WITH BATTERY,
\$6.14 VALUE!
KEEP IT OR RETAIL IT FOR
\$4.19

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(WITH BATTERY)
WITH ANY 12 AXLE SETS OF
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THERMOBOND
BONDED
BRAKE SHOES

Thermoid Deal No. 2

FREE SAFETY LIGHT
(WITH BATTERY)
WITH ANY 18 AXLE SETS OF
THERMOID
PORTOBOND
BONDED
BRAKE SHOES

Thermoid Deal No. 3

FREE SAFETY LIGHT
(WITH BATTERY)
WITH ANY 50 PIECE SELECTION OF
THERMOID
FAN BELTS
AND/OR
"REDI-CURV" HOSE

GET SET TO SELL BRAKES, BELTS AND HOSE TO LOOK READERS WHO COME IN FOR THEIR CAR CHECKS.
SEE YOUR THERMOID DISTRIBUTOR OR JOBBER SALESMAN FOR ALL THE DETAILS OF THIS TREMENDOUS PROMOTION.

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